

REDEVELOP Journal Square City Center

A Redevelopment Plan for Jersey City's Central Business District

Prepared by A. Nelessen Associates and Dean Marchetto Architects

Prepared for the Jersey City Redevelopment Agency
and the City of Jersey City

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Journal Square Redevelopment Plan

The City of Jersey City
Hudson County, New Jersey
3 February 2009

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How to Use this Document

The Journal Square Redevelopment Plan establishes a legal framework for the effective redevelopment of Journal Square by setting forth standards and making recommendations designed to foster the revitalization of the Rehabilitation Area. The document is divided into nine sections, many of which deal with a specific functional or design topics such as land use and transportation. Because of the interrelated nature of these topics, some repetition may be found across various sections of the Plan as central concepts are reinforced. Some recommendations are already underway and will continue to be executed in upcoming years. Others will have to wait until conditions allow their implementation. Wherever possible, the document tries to indicate how current conditions will have to change in order to facilitate the implementation of this Plan.

The document is a compilation of text and a variety of graphic elements that include maps, diagrams, tables, and photographs. The Plan refers to these graphic elements as Exhibits. Exhibits followed by a number represent graphics that shall be regulatory in nature. These include exhibits such as Exhibit 6 Land Use Plan and Exhibit 17 Bicycle Plan. Exhibits followed by a letter are intended to illustrate more general concepts or describe implementation alternatives but are not intended to mandate development in accordance with the graphic depicted.

There are many ways to utilize this plan and it is expected that different audiences will read it for different purposes. One critical group of readers will be developers interested in projects within the Rehabilitation Area. Many of a developer's major questions can be addressed by following these recommended steps:

1. **Locate your lot(s):** use Exhibit 3 Block and Lot Map to determine if your lot falls within the jurisdiction of the Journal Square Redevelopment Plan.
2. **Establish the build-to-lines for your lot:** Build-to-lines are determined according to the street or streets onto which your property fronts. Street typologies are designated on Exhibit 19 Street Typology Plan. Street sections depicting specific regulations such as build-to-lines begin on page 112 and are located throughout the Streets Section. For example, a developer interested in a property fronting onto Van Reipen Avenue sees that Van Reipen Avenue is classified as a One-way Tertiary C Street according to the Street Typology Plan. The Tertiary Street C' Section on page 126 illustrates the street configuration for Van Reipen Avenue. In this case, the build-to-line is found 24' from the centerline. This distance includes half of the one-way travel lane, an on-street parking space, and the sidewalk. Exact locations of centerlines should be checked with the relevant governing agency for that street. In this case, Van Reipen Avenue is a City street and the centerline should be established in coordination with the City of Jersey City. It is worth noting that John F. Kennedy Boulevard is a Hudson County Road.
3. **Determine land use and required frontages:** The Rehabilitation Area is divided into four land use districts as designated by Exhibit 6 – Land Use Plan. The majority of the Rehabilitation Area is classified as the Central Business District or a Residential/Hotel/Mixed-Use District. Permitted uses for each of the Rehabilitation Area land use districts are detailed Section 3.3 Land Use Regulations. Exhibit 7 – Retail Frontages illustrates the locations where ground floor retail uses are mandatory or recommended.
4. **Determine Modified Floor Area Ratio (MFAR) and height regulations:** The Redevelopment Plan uses a combined system of modified floor area ratios and height limits to regulate the mass of buildings within the Rehabilitation Area. Exhibit 9 – Base MFAR Plan and Exhibit 12 – Base Heights Plan establish the standards for 'as of right' development and largely reflect the existing zoning that regulated the Area prior to the Redevelopment Plan.

Some developers may wish to develop above the base maximum heights and modified floor area ratios. Exhibit 10 – Bonus MFAR Plan and Exhibit 13 – Bonus Heights Plan represent levels of MFAR and height that are available at various locations throughout the Rehabilitation Area. In order to develop at the intensities shown in Exhibits 10 and 13, developers must contribute to a Journal Square District Improvement Bonus Fund to be administered by the Jersey City Redevelopment Authority. The Journal Square DIB is discussed in more detail in Section 3.6.

The Redevelopment Plan also allows for additional MFAR and building height in select locations in the event that transit is extended to the Rehabilitation Area along the Bergen Arches. Exhibit 11 – Transit Overlay Bonus MFAR Plan and Exhibit 14 – Transit Heights Overlay Plan show the locations near proposed transit stations that are eligible for these bonuses.

5. **Understand parking standards:** There are no parking minimums for any development within the Rehabilitation Area; however, Section 5 Parking establishes parking maximums for all new projects. This section also describes a long-term parking strategy that proposes a mix of on-site and remote parking to accommodate the future parking needs of Journal Square.
6. **Determine applicable building design regulations:** Section 8 Building Regulations contains a series of standards pertaining to building form and architectural regulations. This section also establishes a system of building classes that will govern all new buildings in the Rehabilitation Area. This section will need to be read closely to ensure that any proposed development meets the goals and objectives of the Redevelopment Plan.

Redevelop Journal Square

Section 1 Introduction



Journal Square, Jersey City's historic downtown, has extraordinary potential. This document, The Journal Square Redevelopment Plan, was prepared by A. Nelessen Associates, Inc. (ANA) and Dean Marchetto Architects, PC (DMA) in coordination with the Jersey City Redevelopment Agency (JCRA) for the City of Jersey City. The Area was deemed in "need of rehabilitation" pursuant to the State's Local Redevelopment and Housing Law (LRHL) on November 25, 2008. The Journal Square Redevelopment Plan is the direct result of months of public input, planning, and urban design and will serve as a blueprint and legal framework for the revitalization of Journal Square.

The Journal Square Rehabilitation Area is roughly 244 acres and contains the Journal Square Transportation Center at its center. Although the Plan focuses its regulations and recommendations on the Area within the Rehabilitation Boundary, a comprehensive approach to planning for the City's future necessitated consideration of conditions and future policy decisions beyond the Area. Where appropriate, recommendations regarding areas outside the Rehabilitation Area are presented in this Plan.

The Journal Square Redevelopment Plan is the culmination of an ambitious planning effort that has included extensive public participation, an analysis of existing conditions, a parking study and the Journal Square Vision Plan. The Plan builds upon the previous efforts of the City of Jersey City, the Jersey City Redevelopment Agency, Special Improvement District members, and residents of Jersey City who have contributed to the continued improvement of Journal Square.



1.2 Purpose of the Plan

The purpose of the Journal Square Redevelopment Plan is to set forth the terms and conditions under which the Area may be redeveloped. The document is divided into sections such as Land Use, Transportation, Parking and Utilities, Streets, Landscaping, and Design Standards. Each section contains detailed standards and guidelines as they pertain to the Area. For example, the Land Use Plan details the placement of mandatory and recommended land uses while the Mobility Regulating Plan describes the layout and design of streets and multi-modal mobility system. Each of these regulations and recommendations is intended to foster the effective revitalization of Journal Square.

The Journal Square Redevelopment Plan is characterized by transit and pedestrian oriented, sustainable development that exceeds Smart Growth standards. The Plan seeks to promote redevelopment that has a long term positive fiscal impact on the City and enhances the aesthetic appeal of Journal Square. The Redevelopment Plan focuses on making the downtown more pedestrian friendly and greener. The Plan seeks to create the opportunity for more people to live, work, shop, play and recreate in close proximity to Journal Square. Furthermore, the Plan will contribute to generating more customers, visitors, tourists, retail, and businesses while accommodating existing and projected parking needs.

The Redevelopment Plan is a hybrid of traditional zoning practices and a form-based code. Form-based codes create a predictable public realm by controlling physical form; they address the relationship between building facades and the public realm, the form and mass of buildings in relation to one another and the scale and types of streets and blocks. The Journal Square Redevelopment Plan provides the framework for high quality redevelopment in Journal Square that will complement and enhance the existing character of the surrounding neighborhoods. The Redevelopment Plan also provides a large degree of flexibility which will allow the Redeveloper(s) to adequately respond to future market conditions and create development projects that are financially feasible while offering creative opportunities for architects and engineers.

This Redevelopment Plan identifies a series of goals and objectives for the Rehabilitation Area. Many of these goals emerged during the Journal Square visioning process and were first encapsulated in the Journal Square Vision Plan. The standards and the strategies recommended to achieve these goals and objectives resulted from the efforts of the consultants, the Jersey City Redevelopment Agency, Mayor Jerramiah T. Healy, the Jersey City Planning Department and the people who live, work and visit Journal Square. The feedback obtained from four public presentations and two visioning workshops were invaluable to the planning process.

The Journal Square Rehabilitation Area is situated in the Central Business District of Jersey City, NJ. With a total area of approximately 21 square miles, the City of Jersey City is the largest municipality in Hudson County and serves as the county seat. The regional context map (opposite page, top) shows the relationship of the Rehabilitation Area and City to neighboring communities in the region. Jersey City is bordered to the east by the Hudson River, to the north by Union City, Hoboken, North Bergen, and Secaucus, to the west by Newark, Kearny and the Hackensack River, and to the south by Bayonne.

Jersey City is a fully developed urban center with diverse land uses and a compact pattern of development. Jersey City has long been a commercial and industrial center that has served as a port of entry and manufacturing center while containing well-established residential neighborhoods. With 11 miles of waterfront and significant rail connections, the City is an important transportation terminus and distribution center.

In a broader regional context, the Rehabilitation Area is included as part of the New York-Northern New Jersey-Long Island, NY-NJ-PA Metropolitan Statistical Area (MSA). Also referred to as the Tri-State Region, the New York metropolitan area is the most populous metropolitan area in the United States and one of the most populous in the world. Jersey City plays an integral role in this region and enjoys several advantages based on its location along the northeast corridor and proximity to New York City. The area is served by four major airports including Newark Liberty International Airport which is located roughly 9 miles from the Rehabilitation Area. Several of the region's most important highways pass through or near Jersey City including Interstates 95 and 78 and U.S. Routes 1 and 9. The region possesses an extensive transit system with several services connecting to Jersey City and the Rehabilitation Area. These include the Hudson-Bergen Light Rail, Port Authority Trans-Hudson rail (PATH), New Jersey Transit, and various ferry connections.

Jersey City is a community of distinct neighborhoods that reflect its early development from several townships in the 19th century. Today, Jersey City consists of six districts or Wards which are subdivided into a series of diverse neighborhoods. Many of the principal neighborhoods are annotated on the map of Jersey City Districts and Neighborhoods on the opposing page. Each of these neighborhoods has its own unique character, land use pattern and development history.

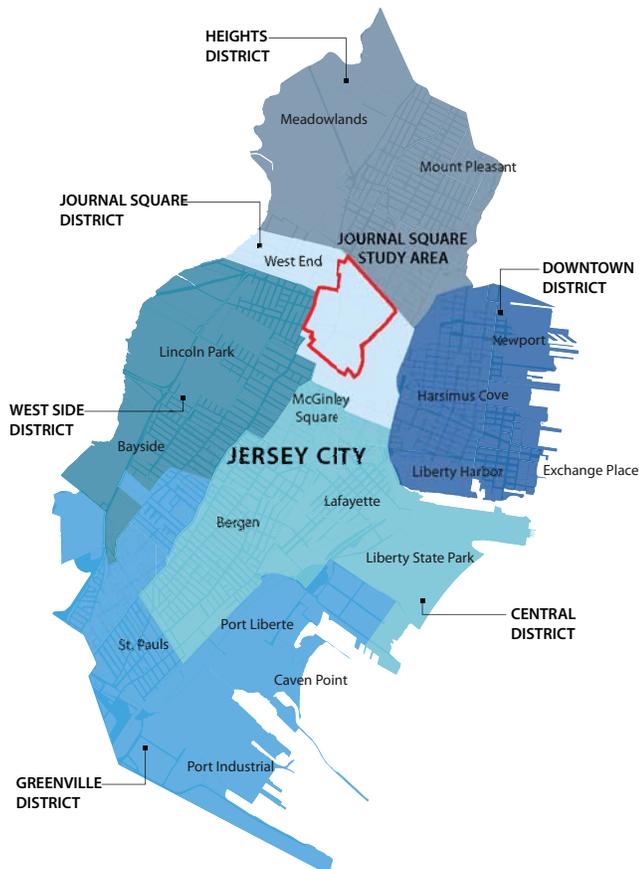
The Heights District, north of the Area, includes the Central Avenue Shopping corridor and is composed primarily of two- and three-family houses including many elegant Victorian and Edwardian homes.

Downtown Jersey City encompasses the area from the Hudson River to the Interstate 78. This area includes the historic downtown as well as the revitalized waterfront area which has seen dynamic growth in recent years. West of the waterfront, the district is home to historic brownstone neighborhoods such as Hamilton Park, Van Vorst Park, Harsimus Cove and Paulus Hook. The central district is also known as Bergen/Lafayette and includes Liberty State Park. Liberty State Park incorporates ferries to the Statue of Liberty, Ellis Island and Manhattan, the Liberty Science Center and the historic Central Railroad Terminal of New Jersey. West of the park, Bergen/Lafayette is a community of row houses, brownstones, and corner stores.

Greenville is primarily a residential district with a principal commercial corridor along Danforth Avenue. The district is home to both Port Liberte, a luxury gated residential community on the Hudson River waterfront, as well as some of the most depressed areas of the City. The West Side is an ethnically diverse district bordering the Hackensack River. West Side Avenue is the district's primary retail corridor while Lincoln Park is one of the largest public recreational areas in the City. The district is also home to the West Side campus of New Jersey City University.



EXHIBIT A - REGIONAL CONTEXT OF THE AREA



Jersey City Districts and Neighborhoods

Jersey City is a community of distinct neighborhoods. The Journal Square Rehabilitation Area is located in the Journal Square District of the City and represents approximately 1.8% of the land area of the City of Jersey City.



The area that would eventually become Jersey City was settled by the Dutch and English in the 1660s. By the time it was ultimately founded in 1838, Jersey City had already secured a key transportation role for the region with the simultaneous arrival of the railroads such as the New Jersey Central and Lehigh Valley Railroads and the Morris Canal. Access to transportation systems and Pennsylvania coalmines along with proximity to New York City led to a booming manufacturing industry.

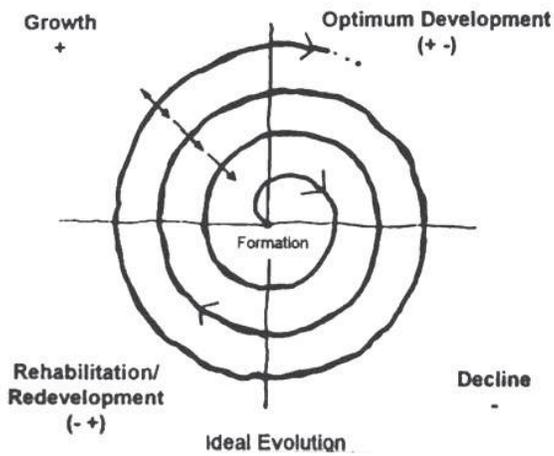
The Journal Square station opened on April 14, 1912 and was originally referred to as the Summit Avenue station. The development of Journal Square neighborhood began in the early 1920s under the direction of Mayor Frank Hague, when the area was built to serve as the city's transportation center. Journal Square was also envisioned as a potential downtown that combined retail, restaurants, and entertainment uses. Even at that time, the area served as a regional crossroads with a stop on the Hudson and Manhattan Railroad that ran between New York and Newark. Journal Square was also a destination and transfer point for many of the region's bus and Streetcar lines. Ultimately, the addition of the Stanley, the State, the Loews Theaters along with a multitude of restaurants and stores made Journal Square one of the premiere shopping and nightlife destinations in the region.

Once the commercial heart of Jersey City, Journal Square, like many urban areas in New Jersey, began to deteriorate in the late 1960s. In Journal Square, this deterioration was symbolized by the loss of retail establishments to surrounding suburban areas and the subsequent closings of the Stanley Theater in 1978 and Loews Theater in 1986.

Despite these changes, the 1970s construction of the Port Authority's Transportation center ensured that the neighborhood would still be anchored by its central square. Today, Journal Square serves as the headquarters for the County's daily newspaper, the Jersey Journal, from which the Square got its name. Provident Bank, a major New Jersey based bank, and one of the principal offices of the Port Authority of New York and New Jersey are based in Journal Square. The Rehabilitation Area is home to a variety of national, regional, and local retailers and is also in close proximity to Hudson County government offices and many Hudson County Community College facilities.

Journal Square's history and development have resulted in one of the most interesting and complex street structures in the City. The existing buildings, railroad trenches, sense of enclosure, and visual terminations give Journal Square its unique form. It is home to several historical and architectural jewels including several significant commercial buildings from the turn of the 20th Century along Newark and Kennedy Avenues and the majestic Loews and Stanley Theaters. Bergen Square, at the intersection of Bergen Avenue and Academy Streets, is the site of what is considered to be the longest continually inhabited European settlement in the state of New Jersey. The outline of the original fort has been maintained over the years by the streetwall at Bergen Square. The square contains an eclectic array of architectural styles including 19th century row homes, art deco retail and office buildings as well as Dr. Martin Luther King, Jr. Public School #11. The Apple Tree House, a colonial stone farmhouse where General Lafayette conducted meetings during the American Revolutionary War, is also located at Bergen Square.

In recent years, Jersey City has experienced a renaissance and is becoming known for more than its ideal location and proximity to New York. Today, Jersey City, one of the state's fastest growing cities, is developing its own impressive skyline and economic engine that is underpinned by an integrated transportation system, aggressive development efforts, an impressive collection of schools, and one of the most diverse populations in the United States. Over the last 25 years, the City has gained about 30,000 residents, 27,000 jobs and over 18 million square feet of prime office space. A revamped Liberty Science Center and State Park have created an enhanced the City's gateway to the Statue of Liberty and Ellis Island. Journal Square is poised for its own revitalization that capitalizes on many of its existing assets. Redevelopment efforts coupled with the neighborhood's strategic location and transportation infrastructure can help the Area regain its status as a regional hub of commercial and cultural activity. Rehabilitation efforts have already begun and are being led by the Journal Square Restoration Corporation (JSRC) and the Jersey City Economic Development Corporation (JCEDC).



The Built Form Evolutionary Spiral

Journal Square, like all places, has evolved over time. After a long period of decline, Journal Square is poised for significant rehabilitation and redevelopment. The Journal Square Redevelopment Plan will help guide the City as it makes critical land use and design decisions in the future.

Journal Square Yesterday and Today



Journal Square as depicted in a postcard from 1938. Below, Journal Square as it looks today. While many of the familiar landmarks remain, much of Journal Square is suffering from retail vacancies, underutilization and an undesirable pedestrian environment. The Redevelopment Plan strives to enhance the pedestrian network and create development opportunities while respecting Journal Square's rich history.





The Journal Square Rehabilitation Area is a mixed-use urban environment composed primarily of office space, a range of housing types, a transportation hub, and a diverse range of neighborhood retail. The 244 acre site is shown in the Journal Square Rehabilitation Area Map on the opposite page. The Rehabilitation Area boundary was determined by the JCRA in conjunction with the State Office of Smart Growth. The Area centers on the Journal Square Transportation Center (JSTC) which houses the Journal Square PATH station and the city's largest bus terminal. The Journal Square Transportation Center is a multi-mode transportation hub that is owned and operated by the Port Authority of New York and New Jersey. In addition to the PATH station and bus terminal, the JSTC includes a 10-story tower, retail plaza and a two-level parking facility. The PATH Station serves as the terminus of the Journal Square-33rd Street route. This station connects to Hoboken (weekends only), Newark Penn Station, Harrison, 33rd Street, and the World Trade Center PATH Station in New York City, approximately 10 minutes from Journal Square by PATH.

From the JSTC, the Area extends in an irregular fashion nearly ½ mile north and south of the plaza and over ¼ mile east and west of the station. The Area is bounded primarily by the Pulaski Skyway interchange Route 139 on its northern edge; Route 139 and Baldwin Avenue on its eastern edge; Vroom Street on its southern edge; and Garrison Avenue and Tonnelle Avenue on its western edge. Major highway access to Journal Square is afforded by US-1 & 9 and NJ Route 139 to the north and Interstate 78 to the east of the site. One road within the Rehabilitation Area is a Hudson County Road: Route 501/John F. Kennedy Boulevard.

The existing railroad trenches play a critical role in shaping the form and character of the Area. These railroad cuts run diagonally through the site and under the Journal Square Transportation Center from Tonnelle Avenue to Baldwin Avenue. These cuts were identified during the visioning process as significant voids that restrict pedestrian and vehicular flow through the Area and detract from its aesthetic appeal.

The Rehabilitation Area falls entirely in the Journal Square district and encompasses a large part of the Journal Square neighborhood and portions of the St. Aedens and St. Joes Neighborhood. In addition to the JSTC, the Area is the home of several historic and cultural resources as well as Hudson County Community College facilities. Journal Square has a diverse population includes areas such India Square which is a cultural and commercial center for the region's Indian community. Additionally, St. Peter's College is located on a 15 acre campus just outside the southern boundary of the site.

A small portion of the Rehabilitation Area, blocks 1880 and 1879 near the southeast border of the Area, are found within the Palisades Preservation Overlay District (PPOD). This district is subject to special procedural and performance standards as detailed in the City's Land Development Ordinance. These regulations will continue to apply to the portion of the PPOD that is located within the Journal Square Rehabilitation Area.

Rehabilitation Area Basics

- The Area is approximately .38 square miles in area (roughly 244 acres)
- The Area contains approximately 49 acres of off-street parking (roughly 20% of developable land)
- The Area represents an important part of Jersey City's history. The Area contains six historic buildings: The William F. Brennan Courthouse, the Stanley Theater, Loews Jersey Theater, the Labor Bank Building, the Apple Tree House, and the Newkirk House. In addition, the Area is home to several significant buildings which may soon become eligible for the State and/or National Register of Historic Places. Exhibit 49 highlights historic buildings, neighborhood assets and significant facades found in the Area.
- As of 2000, the Area contained 7,888 housing units of which 75% were renter occupied
- The Area has a diverse population with no single racial majority. The largest segments of the population are White (34%), African American (28.3%) Hispanic or Latino (28%).
- Retail/Trade and Health Care and Social Assistance is the most common type of business found in the Area



EXHIBIT 1 - JOURNAL SQUARE REHABILITATION AREA



STUDY AREA BOUNDARY





Public participation was critical to the formation of the Journal Square Redevelopment Plan. No one knows a community better than the people who live and work there. A unique and inclusive public visioning process was created for Journal Square to gather as much public input into the Plan as possible. By sponsoring this process, the City of Jersey City and the Jersey City Redevelopment Agency gave residents, visitors, business operators, developers and land owners an opportunity to participate in the Redevelopment of Journal Square. The public visioning process comprised of a series of stakeholder and public meetings and was successful due to the extraordinary civic interest demonstrated by all those who participated.

Obtaining the community's input is a hallmark of good planning. The ANA/DMA Team was partially selected due to its use of innovative public involvement. Three primary techniques were used to gather information from the public: the Visual Preference Survey™ (VPS™), a Demographic, Market and Policy Questionnaire, and the Vision Translation Workshop.

The outreach process began with a series of meetings between the Consultant Team, the JCRA and property owners and developers. The Team interviewed these stakeholders and examined their plans for Area to gain a better understanding of the attitudes and perceptions of the development community. These meetings and interviews identified a series of properties with serious development potential within the Area. These properties and their existing uses were highlighted in the *Journal Square Existing Conditions Report* (June 2008). Feedback from these stakeholders was incorporated into the visioning process which informed both the Vision Plan and the design guidelines included in this Redevelopment Plan.

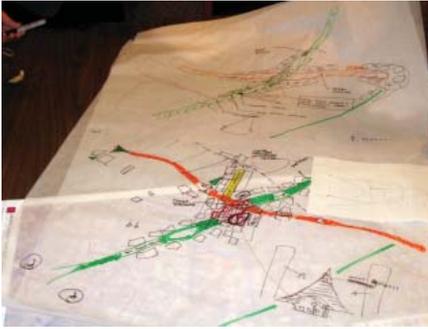
The Journal Square Visioning Process included two intensive workshops – one for stakeholders and one for the general public. In preparation for these meetings, the consultant team met with officials from Jersey City Planning Department in May 2008. This meeting was used to discuss the history of the Rehabilitation Area and the planning issues that Journal Square faces today. During the meeting, City staff also participated in planning exercises dealing with traffic and circulation, infrastructure improvements, potential transit connections and design guidelines. The purpose of this meeting was not only to gather critical input from the staff but also to test and refine the visioning process before the full-scale public presentation. The staff provided valuable feedback, particularly regarding the Vision Translation Workshop overlays, that helped make the subsequent workshops a success.

A Stakeholder Visioning Workshop, facilitated by A. Nelessen Associates, was held in June 2008. The following month, a full-scale public Community Visioning Workshop was held. Both events were held at the Culinary Institute of Hudson County Community College. Nearly 150 people participated in the workshops which helped to define the visual and spatial characteristics desired for Journal Square. In total, the input from 24 design teams was incorporated into the Plan. The results from both the Stakeholder and Public Workshops, as well as their cumulative results, are discussed and compiled in the Journal Square Vision Plan. Every aspect of the public's participation was integral to the formation of this plan.



Stakeholder Workshop

Public Workshop





The Journal Square Redevelopment Plan and attending controls herein have been prepared pursuant to the State's Local Redevelopment and Housing Law (LRHL) (N.J.S.A. 40A: 12A-14) The Area was deemed in "need of rehabilitation" per the LRHL and Resolution 08-879 as adopted by the City Council on November 25, 2008.

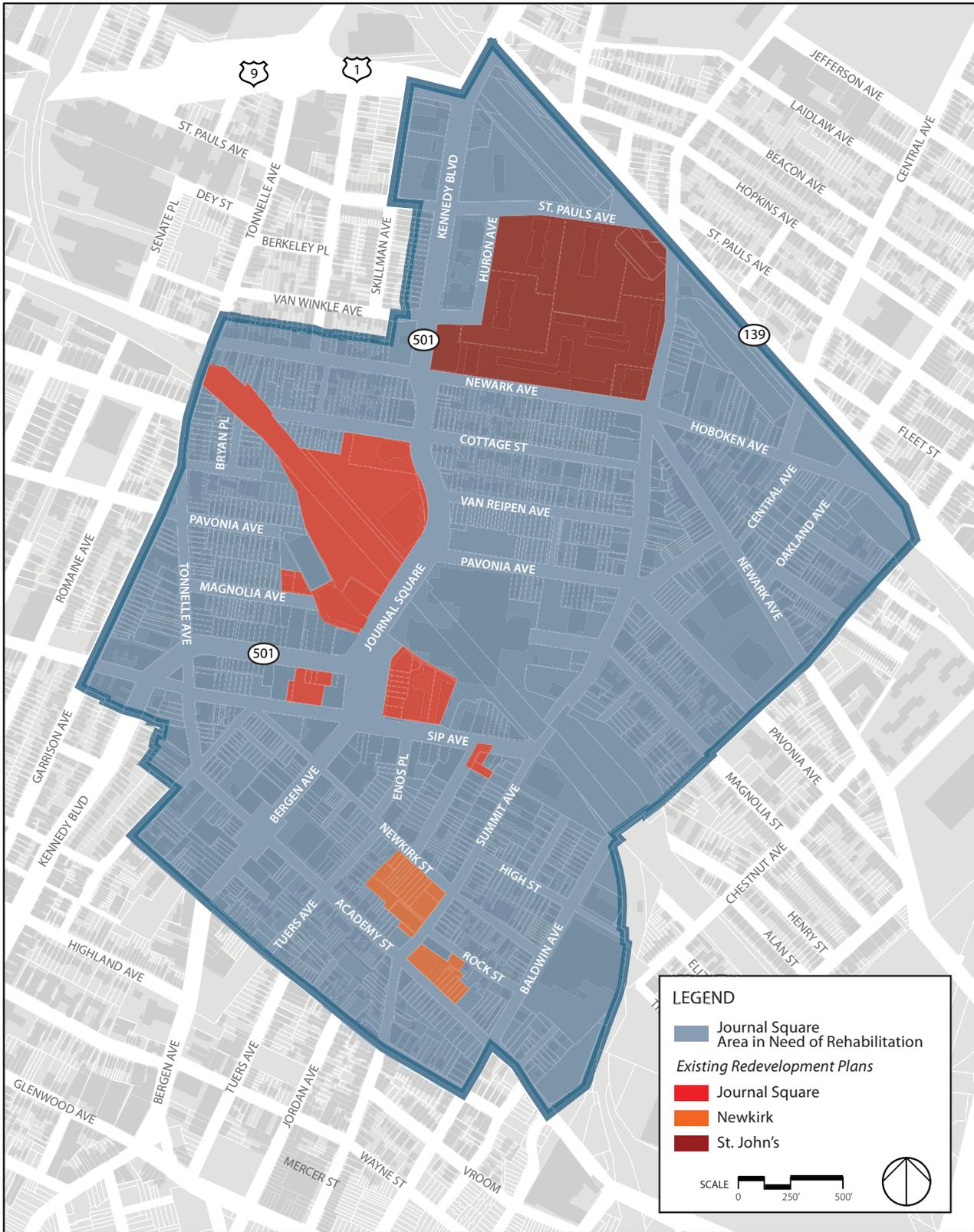
Exhibit 2 – Existing Planning Context identifies the three existing redevelopment areas that are found within the Journal Square Rehabilitation Area: Journal Square, Newkirk, and St. John's. The Journal Square Rehabilitation Area encompasses the entire area indicated in Exhibit 1 including the redevelopment areas identified in Exhibit 2.

The Journal Square Redevelopment Area was declared in 1989. Since that time, the Redevelopment Plan was been updated ten times, most recently on August 22, 2007. Over the years, this Redevelopment Plan has been one component of the City's efforts to improve Journal Square and reinforce its role as a central business and shopping destination. The Plan itself divides Journal Square into seven districts which are spread over six blocks. In total, the preexisting Journal Square Redevelopment Area encompasses 28 lots.

An Area in Need of Rehabilitation designation permits limited tax abatements but does not allow for the use of eminent domain for private development. The State of New Jersey is currently reviewing new financial provisions for areas in need of rehabilitation. Any new regulations regarding rehabilitation areas in New Jersey will apply to the Jersey City Rehabilitation Area.



EXHIBIT 2 - EXISTING PLANNING CONTEXT





The Journal Square Rehabilitation Area is comprised of approximately 244 acres. While the Area is roughly defined by several major highways and roads, its complex shape makes a simple definition of the Area difficult.

The boundary of the Rehabilitation Area is depicted on the foldout map that is included in this section. Any land area within the boundary that is not listed in the tax lot enumeration below is intended to be included within the Rehabilitation Area. Due to potential errors in tax lot enumeration, the map takes precedence to determine which properties are in the Area that is the subject of this Plan.

The Area is comprised of 69 total tax blocks. Five of these blocks (595, 596, 618, 1846, 1847) are only partially included in the Area while the remaining blocks are entirely included.

The Journal Square Rehabilitation Area consists of the following tax blocks and lots:

- Block 519 Lots 3A, 4A, 1, 2A, 19A, C, B, 18A, A, 16A, 7, 8, 15A, 9, 10, 11, 12, 13, 14
- Block 520 Lots 1, 2, 3, 4, 18, 5, 17, 6, 16, 7, 15, 8, 14, 9A, 10A, 12B, 13
- Block 520.5 Lots A3, A2, B, 22, C
- Block 521 Lots 4D, 6P, 6Q, 5, 6R, 6H, 6S, 6T
- Block 522 Lots 19, 5, 18, 6, 17, 7, 16K, E, F, 27E, 27D, 29F, 29E, 26, 30D, 25, 31B, 24, G, W1, H, J, 8A, L1, 8B, 9, 14A, 10, 13A, 11, M, 12, N, 13, P, 14, 9, 14-15, 8, 15, 16, 7, 16-17, 6, 17, 18, 5, 19, 4, 20, 1, 21, 2, 22, 3, 23, A1, 24, B1, C2, C1, S, D
- Block 523 Lots 50A
- Block 524 Lots S1, D1, 8, 7, 6, 5, 4, 3, 2
- Block 524.1 Lots 16G, 16F, 16E, A5, A11, A10
- Block 525 Lots E, B2, S, 10F, 11, 12, 13, D, 22A
- Block 525.1 Lots 13D, 13E, 13B, 13A, 12C, 12D, 10C, 11A, 10B, 8C, 8D, 9D, 7D, A, C, 14A, 6A, 1, 2, 3, 15E, 4, 15F, 5, 6, C8, C7, C6, C5, C2
- Block 526 Lots 1B, 1A, 7, 9, 12A, 2, 12B, 12C, 3, 21, 4, 23, 5, 28, 6, 10, 32, 11A, 1B, 36, 11C, 11D, 20, 5N, 6N5, 12N, 11R, 22B, 26A, 26B, 22A, 11P, 30, 8N1, 8N2, G, 1, F, 6N4, G1, E, F1, D, C1, 6N3, E1, C, 6N2, D2, B, 6N1, C2, A, 1N, B2, A2, 4N, 3N
- Block 527 Lots 10A, 10B, 11C, 11D, 11E, 12, 13G, 13H, 10F, 11F, 2C, 1D, 2D, 9A, 13J, 9B, 13K, 8, 13L, 14, 7, 15, 6, 16, 5C, 17, 5E, 4, 18B, 3A, 18C, 3B, 1F, 2F 1E, 2E
- Block 528 Lots 1C, 1D, 1E, 1F, 1B, 1G, 1A, 16, 6.5, 18, 20, 22, 24, 26, 28, 2, 30, 32, 3, 34, 36, 4, 38, 5, 6, 8D, 8C
- Block 529 Lots 9, 10, 3, 11A, 12B, 2, 7, 6, 5, 8, 4, 13B, 1, 30, 14A, 29, 15B, 28, 16B, 27, 26, 18B, 25, 24, 23, 19C, 20A, 22A
- Block 530 Lots 48, 57C, 57D, 58, 59, 60A, 62A, C, 63A, 1, 66, 64A, 67A, 67B, 2, 1, 50, 49
- Block 533 Lots B1, M, A, 45A, 44A, 43A, 42A, 41A, 40A, 35A, B1, 36, 37C, 37D, A, 37B, 37E
- Block 566.1 Lot 1A



Block 571	Lots E1, 23, D3, 77A, 76A, 67A, 75, 60A, 70A, 70B, 58, 57, 23A, 56, 5A, 54A, F4, M1, C2, 27A, C3, 26D, D1, 25C, C2, 25A, 24, D2, 51A, C4, M2, 52, 50A, 2B, 42A
Block 581	Lots 3D, A, B1, C5, 1C, 33B
Block 582	Lots 90C, 90B, 88A, 87, 86, 85, PL A, 80, 79, 78, 77, 76, 75, 74B, 72B, 71B
Block 583	Lots 13A, 12A, 10J, 11D, 11E, S1, S1, A, B, C, 4, 5, 6, 7A, 8A, 9A, 10L, 10K, S2, 20, 19, 18, 21
Block 584	Lots 37, 38A, 39A, 40, L1, L2, M, A, B, C, D, E, F, G, H, J, 9, 10, 11, 12, 13, 14, 15, 16, S3, 36, S2, M, L, K, 20, 19, 18, 17, S4
Block 586	Lots 41, 40, 35, 39, 34, 38, 33, 37, 32A, 36, 31A, 30, 28B, 28A, 24E, 24D, 24C, 24B, 24A, 22B, 22A, 21A, 83, 82, 13B, 13E, 9C, 9D, 7C, 6B, B1, A1, 1B, 2, 3, 4, 5, 33, 1A, 34, 72A, 72B, 73A, 73B, 74A, 74B, 75A, 75C, 75D, 76A, 76B, 77E, 77F, 77B, 77C, 78, 79A, 79B, 79C, 80A, 80B, 81J, 81H, 81G, 81F,
Block 586.5	Lots B, 69, 68, 67, 66, 65, 64, 63, 62, 61-62, 61, 60, 59, 58, PL A, PL B, C2, A2, D21, A3
Block 588	Lots C1, D1, E1, E2, 30A, 29A, 28, 27, 26, 25, 24, 23, 22, 21, 20A, 19A, B2, 18, 17, 16, 15, B1, 14, 13A, 12, X1, 11, 10, A2, 9, 8, 7A, 6A, 5, 46, 4, 47, 3, 48, 2, 49, 1, W, 80B, V2, 89B, 52A, 53, 54, 55, U, 57, T, 58, 59, 61, 60, 63, 62, S1, R2, P1, M2, L, K, 69A, 11C, 70A, 71, 72, 73, J, H, G, F, 77, 78, 84A
Block 588.5	Lot X
Block 589	Lots 31C, 31B, 31A, 29, 28A, 26D, 26C, 24D, 24C, 23, 22A, 21A, 20B, 19A, 18, 17B, 16, 15, 14, 13, 19B, 13B, 21C, 17C, 13C, 20A, 17A, 16A, 13D, 15A, 14B, 13E
Block 589.5	Lots 11, 10, 9, 8D, 7D, 6, 5, 4, 3, 2, 1, L, K1, H1, G, F2, 11A, 10A, 9A, C, 6B, 4B, 13, 12, 1B, 8B, 7B, M, 7C, F1, N, O, 14, E2, A1
Block 593.1	Lots A4, A3, C, G, B, H, F
Block 595*	Lots A5, A4, 22A, 20, 18, 16, 14, 12, 10, 8, 6, 54, 4, A3, 27, 25, 21, 23, 19, 17, 15, 13, 11, 9, 7, 5, 3
Block 596*	Lots 61, 52, 50, 48, 46, 60, 44, 42, 40, 38, 36, 34, 59, 32, 30, 58, 57, 56, 55, 54, 53, 51, 49, 47, 45, 43, 41, 39, 37, 35, 33, 31, 29
Block 618*	Lots 2, 35A, 3, 37A, 1, 36A, 34A, 4, 33, 5, 32, 6, 31, 7, 30, 8, 29, 9, 28B, 28A, 10, 27, 11, 26, 12, 25, 13, 24, 14, 23, 15, 22, 16, 21, 17, 20A, 18A, 19
Block 619	Lots A
Block 628.1	Lots S1, 3D, 3C, 2D, 2C, M, S3, S2
Block 631.1	Lots C6, C2, M, S4, M1, P4, P2, S3, C4, P3, C5, D, 53A
Block 633	Lots 152, B, A2, A1



Block 1846*	Lots 30, A, 29A, 28A, 20C, 27, 26, 25, 21B, D18, 22, 23, S, 24, R, Y, D19, D14, D20, N, D21, M, 4B, 6, 7, 8, 13, 9, 12, 10, 31, D22, K2, D23, D24, J2, D25, D26, H1, D27, D28, G1, D29, 7A, Y, D30, 6A, D31, 5, D32, 4, E4, E3, 3, E2, 2, 1, F1, F2
Block 1847*	Lots A1, 31, 30, 29, B1, 28, B2, 27, C1, 26, C2, 25, D, 24, 16, 23, 17, 22, 18, 21A, 19, 21B, 20
Block 1848	Lots A10, 1A, 3A, 4A, 5A, 6, 8B, 10A, 49, 48, 12A, 47, 46, 14, 45, 15, C, D2, 17, D1, 41A, 18, 40A, 19B, 19A, 39A, 20, 38B, 21, 38A, B1, A8, A7
Block 1848.5	Lot C
Block 1856.1	Lot 1
Block 1860.1	Lots 1O, 1P, 1G, 1M, 1H, 1J, 1D, 1A, 16A, 2, 15A, 3, 14, 4, 13, 9, 10, 12, 13, 5, 7, 8, 6
Block 1860.2	Lots 1B, 1, 2, 3, 9, 4, 10, 5, 6, 7, 8, 11, 12, S
Block 1861	Lots 1A, 43, 42, 41A, 40B, 40A, 39A, 2A, 38A, 37A, 37B, 36A, 35A, 35B, 34B, 33B, 31A, 30, 3A, 29, 28, 27, 5A, 9, 10, 11, 6, 12, 13, 14, 15, 16, 17, 18, 7, 19, 20, 21, 22, 23, 24, 25, 26, 8, S
Block 1862	Lots 5, 37, 36, 35A, 4, 10, 11, 12, 13, 14, 9, 15, 16A, 17A, 18, 19, 20, 21, 8, 22A, 23A, N10, 7, 6, 34A, 33, 32, 31, 30, 29, 28, 3B, 27, 26, 25, 3A, N27, 2A, N26, 1
Block 1863	Lots N25, N24, 4A, 4B, 5, 6, 7, 8A, 8B, 8C, 10, 11, 12, 13, 14, 15, 16, 17, N26, N27, 1, D2, D1, 46, 42, 41, 40, 45, 39, 38B, 38A, 37, 36, 47, 35, 34, 44, 33, 32A, 31B, 30A, 27A, 19A, 48, N23, 43
Block 1864	Lots 1A, 57, 56, 55, 54, 53, 52, 51, 50, 49, 48, 58, R1, 3, B2, 38D, 7, 38E, 8, 36A, 9, 4A, 10, 34A, 11, 33D, 12, 13, 59, 22
Block 1866	Lots C1, 25J, 25H, 27C, 20, 19, 18A, 17A, 16, B3, B4
Block 1866.1	Lots 29A, 28B, 30A, 31A, 32A, 33A, 34A
Block 1867	Lots 16, 35, 17, 18, 34, 19, 1B, 3B, 4B, 45E, 45F, 45B, 5, 44, 6, 43, 7, 42, 8, 41, 9, 40, 39, 46, 38, 11, 12, 13, 37, 14, 36, 15, 20, 33, 21, 22, 32, 23, 31, 24, 25, 30, 29, 26A, 26B, 28, 27
Block 1868	Lots 17B, 19A, 20A, 36, 21A, 22A, 23A, 12A, 26, 8, 27, 7, 28, 6, 29, 5, 30, 4, 31, 3, 32, 2, 33, 1, 34, 35
Block 1868.5	Lots 7B, 1A, 2B, 3A, 4C, 42J, 42L, 42H, C, D1, E1, 11, 12, 13, 14, 15, 16
Block 1869	Lots 1B, 15C
Block 1870	Lots 8C, 1, 2, 3B, 3A, 4, 5, 6, 7, 8F, 8A, 8G, 8B, 8D
Block 1871	Lots 6, 5, 14, 13, 12, 11, 10, 9, 8, 7
Block 1871.1	Lots 18, 15, 16, 17, 19, 25A, A, 4G, 20, 27A, 4H, 21, 4J, 28A, 22, 4R, 29, 23, 4F, 30, 24, 4Q, 3D, 31A, 4P, 4O, 32B, 4N,



Block 1872	3C, 1C, 1B, 34 Lots 24E, 16, 25, 15, 26, 27, A4, C, D, E, 4B, F, G, 3B, H, 1B, 11, 19, 20, 10, S1, 21, C, B, 22, A, 23B, 18, 17, 24F, 28, 28-29, 29, 3H, 3G, 3F, 3C, 3D, 3E, 3B, 3J
Block 1873	Lots 17K, 17D, 17M, 17H, 17G, 17F, 17E, 4, 5, 6A, 6, 3, 7, 2, 8, 1, 7A, 9B, 10, 16C, 11, 12, 16A, 13A, A, 13B, B, 13C, 16C, C, 14A, D, 15A, E, 17P, F, 17O, 17N, 17R, 15B, 17Q, 17M, 16D, 17L
Block 1874	Lots 9, 10, 11A, 11B, 8D, 1C, 1B, 8C, 1D, 8B, U1, 1E, 8A, 1F, 7D, 7E, 1G, 7F, 7A, N, 7B, A1, T1, L, S1, M, R1, K2, J, H, K1, C, D, E, F, G, S
Block 1875	Lots 46, 47, A1, 2, 3, 5, 24, 40A, 45, 39A, 25, 38C, 38B, 26, 41, 50, 27, 42A, 42B, 37F, 36B, 28, 43, 37J, 36C, 29, 44, 31G, 30, 35C, 31F, 35D, 31E, 34, 31D, 33F, 31.5, 33E, 31C, 33D, 32, 48, 49
Block 1876	Lots 24A, 17U, 16N, 27, 16M, 19X4, 19X3, 19X2, 19W, 18V, 19X1, 18U, 18T, 20, 17S, 17R, 17Q, 21, 17T, 22A, 23C, 16T, 23A, 16S, 24, 28, 16C, 1, 16L, 26, 16, 16K, 16J, 25, 16R, 2, 16P, 3, 4, 7C, 5B, 5A, 16E, 7B, 6B, 16H, 6A, 8B, 16D, 8C, 16C, 12H, 10, 16B, 9A, 8D, 11, 12J, 12K, 12F, 15A, 12C, 12B, 12G, 14, B, A
Block 1877	Lots 12A, 16, 20A, 19A, 14D, 18A, 14C, 13B, 18B, 17, 10A, 15, 9, 3A, 6D, 2A, 6C, 6A, 1A, 4A, 5A, 8, 7
Block 1877.5	Lot 1
Block 1878	Lots A, 6, B, 7, C1, 5A, D1, 5B, E, 4A, F, G, 4B, H, 3A, J, 3B, 12C, 2A, 2B, 13, 12D, 1B, 1A
Block 1879	Lots 3A, 2A, 1A
Block 1880	Lots 12B, 1A, 4A, 7A, 13A, 6A, 3C, 13C, 5, 7A, 17, 16B, 5A, 16D, 3B, 16C, 4, 13D, 3, 2, 1, 8B, 8C, 13B, 13A, 14
Block 1881	Lots 31A, 22, D, E, F, 24, 26, 32A, 28, A, B, C, 1, 2, 3, 9B, 9C, 4, 11A, 13, 15, 5, 17, 6B, 19, 21, 6A, 23, 8, 7A, 25, 10, 27, 12, 29, 14, 7B, 33A, 16, 30A, 18, 20, G, 38
Block 1882	Lots 2, 24, 7, 23, 22A, 8A, 22B, 3, A, 27, 4, 26, 5, 25, 6, 1, 21C, 8B, 21B, 34, 20C, 20B, 19A, 18A, 11, 28A, F, 13A, 12A, 29A, 14C, 14B, 30, 14A, 15, 31, 32, 33
Block 1883	Lots 27A, PL B, 22A, 10B, 21, 10A, 20, 11A, B2, 12, 19B, 13A, A, 13E, 14B, 13F, 15B, 16C, 16B, 16A, 17, 18
Block 1884	Lots C2, D, E, F, G, B, H, I, J, K, A1, L3, 2A, L2, T1, M1, N, 6A, T2, O, 6B, P, 7, 6C, 8A, 11, 9A, Q1, 12, 10, Q, 13, 14, 19A, 18A, 16A, 15A

* indicates blocks that are only partially included in the Rehabilitation Area





Exhibit 3 Block and Lot Map





1.9 Superimposed Plan

Exhibit 4 – Superimposed Plan shows the location of proposed new blocks on top of the existing block and lots. The approximate locations of curb lines for the proposed blocks are shown in red on the map. New blocks created by the addition of new streets throughout the Rehabilitation Area are identified in Exhibit 3 – Block Identification Plan. Details on specific infrastructure improvements are contained in Section 2.5 Development Plan and Section 6 Streets.





Exhibit 4 Superimposed Plan

Redevelop Journal Square

Section 2 Overview of the Plan

VISION STATEMENT - This Redevelopment Plan strives to create a national icon of sustainable mixed-use urban design that is pedestrian and transit-oriented with great streets, parks, and neighborhoods that contain a great diversity of people, businesses, and culture.

The **goals** of the Journal Square Redevelopment Plan were developed in consultation with city staff and stakeholders, as well as through the involvement of the public. By adoption of this Redevelopment Plan for Journal Square, the City of Jersey City seeks to accomplish the following goals:

1. Make sustainability a theme of future development and redevelopment that guides land use and transportation decisions.
2. Develop a balanced transportation system that provides multimodal travel options to and within the Rehabilitation Area.
3. Establish a coherent and positive identity for Journal Square that restores the image of Journal Square.
4. Integrate open and green space into the Area by incorporating a system of parks, plazas, natural amenities, and a continuous green streetscape.
5. Enhance the economic vitality of the Rehabilitation Area and the City by expanding the level of residential and commercial activity in Journal Square, thereby increasing the potential for economic activity and job creation.
6. Promote a pattern of mixed and multiple-use development within the Square. New buildings within the Area should appropriately combine residential, commercial, and entertainment uses and encourage a balance of jobs-to-housing.
7. Make walking easy, safe, desirable, and convenient.
8. Promote remote parking to limit the impact of cars on the desired pedestrian priority of the Square.
9. Establish a system of District Improvement Bonuses (DIB) that funds the continued improvement of the Square.
10. Preserve and protect the historical resources and buildings within the Greater Journal Square area for future generations.





2.2 Redevelopment Objectives



*The Journal Square Redevelopment Plan addresses the need for redevelopment and rehabilitation in the Rehabilitation Area. The following **objectives** must be met in order to achieve the goals described on the previous page:*

- Reduce automobile dependency by implementing alternative modes of travel such as streetcar, heavy and light rail, bicycle and buses.
- Encourage walking to jobs, transit, shopping, recreation, civic and religious functions.
- Infill surface parking lots with development that creates a streetwall. Site buildings along the sidewalk edge and place parking in mixed-use structures to unify the appearance of neighborhood streets.
- Establish an interconnected block and street network that accommodates vehicular and pedestrian circulation while promoting effective redevelopment and infill projects.
- Allow for more efficient use of land and expand the city's tax base by encouraging high quality mixed-use development.
- Design the urban amenities such as transit, housing variety, park space, and entertainment that will attract new employers and a range of new residents to the Area while sustaining existing neighborhoods.
- Maximize the investment of private developers while minimizing the cost to the public sector.
- Add a system of crosswalks and traffic lights that create safe zones for pedestrians to cross streets at appropriate locations.
- Establish a variety of parking options including on-street, in-building, neighborhood, local interceptor, and regional interceptor.
- Create a community fund that developers can contribute to in exchange for the ability to develop at a higher intensity at select locations within the Area.
- Create a system of open and green space that will integrate nature into the urban fabric.
- Encourage buildings to meet or exceed the US Green Building Council's LEED (Leadership in Energy and Environmental Design) Certification.



The Journal Square Redevelopment Plan contains a number of distinctive features designed to achieve the goals and objectives described above. The following list details key elements of the Plan that will be discussed throughout this document.

- The Plan proposes a balanced street network that accommodates the needs of pedestrians, transit, vehicles, and non-motorized movement. The Plan identifies strategic locations where pedestrian and bicycle movements have priority, incorporates a new streetcar along Kennedy Boulevard, and creates a new pedestrian and bus only commercial street in the heart of the Square which will be phased in over time.
- The Plan proposes that the existing rail road cuts be decked over to create developable blocks and enhance circulation within the Rehabilitation Area
- A significant portion of this platform will be comprised of a linear network of parks and public open space that will extend from Tonnele Avenue to Baldwin Avenue with future connections along the 6th Street Embankment to the Waterfront
- A revitalized Journal Square Plaza becomes the centerpiece of this open space network and a year round 24/7 destination. The Plaza will include a newly day-lit transit station, reflection pool, expansion of the farmer's market and the opportunity for new housing and commercial uses
- Existing surface parking lots are infilled with mixed-use buildings and municipal or private mixed-use parking structures
- Land and building uses are structured to create a sense of community. Revitalized neighborhoods will provide the vigor, diversity and conveniences needed to support dynamic places to live and work
- The Plan provides a variety of housing types that will be attractive and affordable to a wide segment of the population
- The Plan provides a range of commercial spaces for diverse employment requirements. From large floor plate office buildings to in-home offices, the Redevelopment Plan promotes Jersey City as a serious regional office market
- Neighborhood amenities such as small parks, plazas and shopping will be located within a short walk of new housing and employment centers to balance private spaces with public gathering spaces
- Retail will be sited at locations convenient to transit and within walking distances of residences and employment
- Regional and local parking 'intercept' facilities will reduce congestion and enhance the pedestrian realm within the Area by receiving cars that would normally attempt to park in Journal Square. Instead, motorists and those who only occasionally use their cars will have the opportunity to travel via transit from these parking facilities to their Journal Square destinations
- A comprehensive transit system will allow people to travel within Journal Square while linking the Rehabilitation Area to the larger region. This transit system combines an expansion of the Hudson Bergen Light Rail and a new streetcar along John F. Kennedy Boulevard with a vastly improved Journal Square Transportation Center.
- An extensive landscaping plan will green the streets within the Area and provide appropriately scaled sidewalks.
- A network of bicycle paths and lanes that allow convenient transportation through the Rehabilitation Area and provide access to important regional connections.

Exhibit B – The Illustrative Site Plan is a valuable tool to visualize the location and character of buildings, open space, the pedestrian realm and the streetscape. Actual building forms may vary, however, key elements such as the new street network, enhanced pedestrian environment, the land devoted to parks and plazas, and the careful massing of structures should substantially conform to this plan.

The renderings on the following pages are illustrative and intended to suggest the character of development that is possible by following the standards and recommendations in the Redevelopment Plan.



EXHIBIT B - ILLUSTRATIVE SITE PLAN

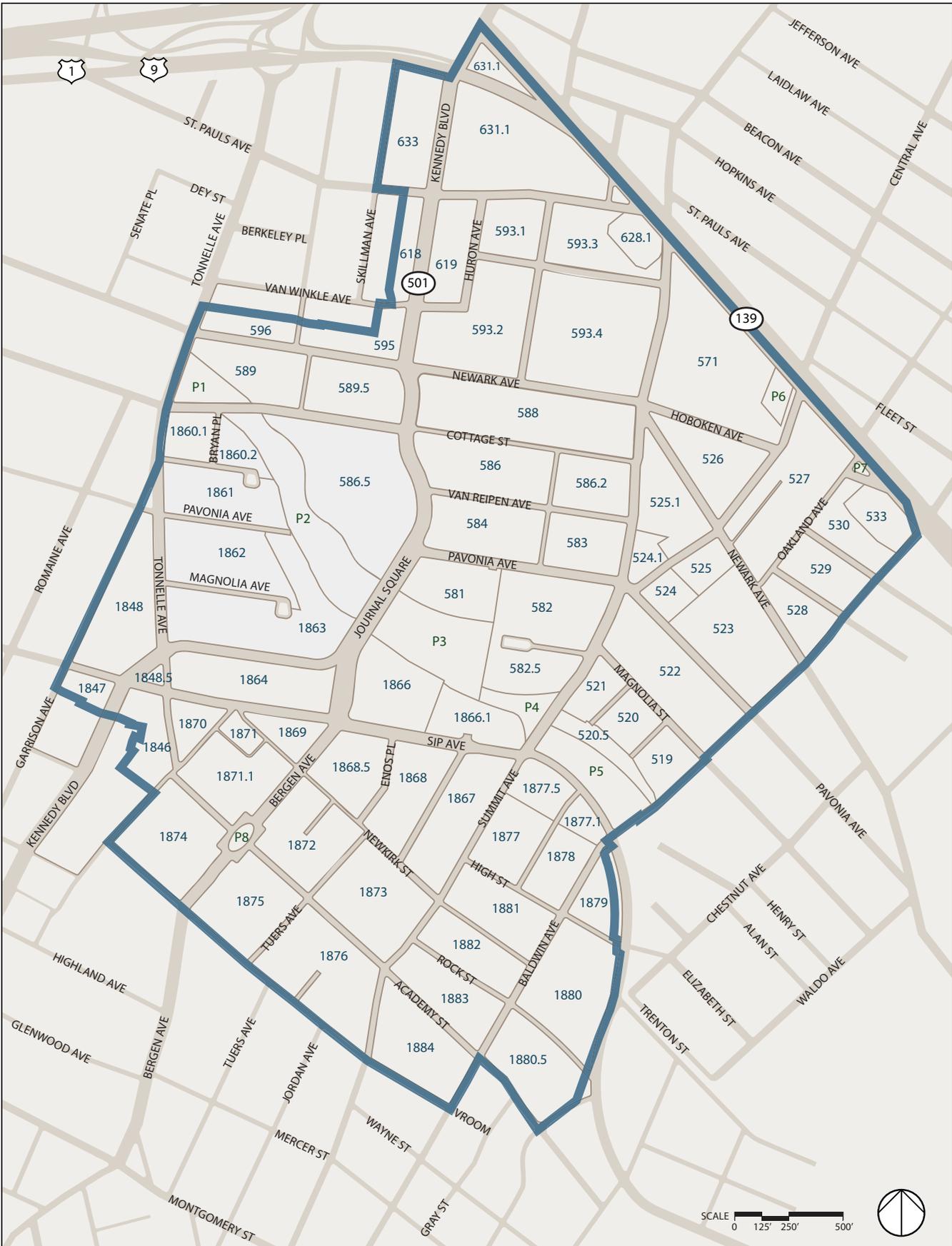




For the purpose of describing the standards and recommendations for the Journal Square Rehabilitation Area, the Area has been divided into 84 blocks. While the block numbers shown on the Block Identification Plan are based on the existing tax block numbering system, they are presented for reference only. The addition of new streets and the removal of other necessitated the creation blocks which heretofore did not exist. In these cases, new block identification numbers are derived from adjacent existing block numbers. Seven blocks have been reserved for park and plaza spaces. These blocks are identified with a P and a number. The exact width of streets and sidewalks is detailed in Section 6 – Streets, while the dimensions of the Park Blocks are presented Section 7 – Landscaping.



EXHIBIT 5 - BLOCK IDENTIFICATION PLAN



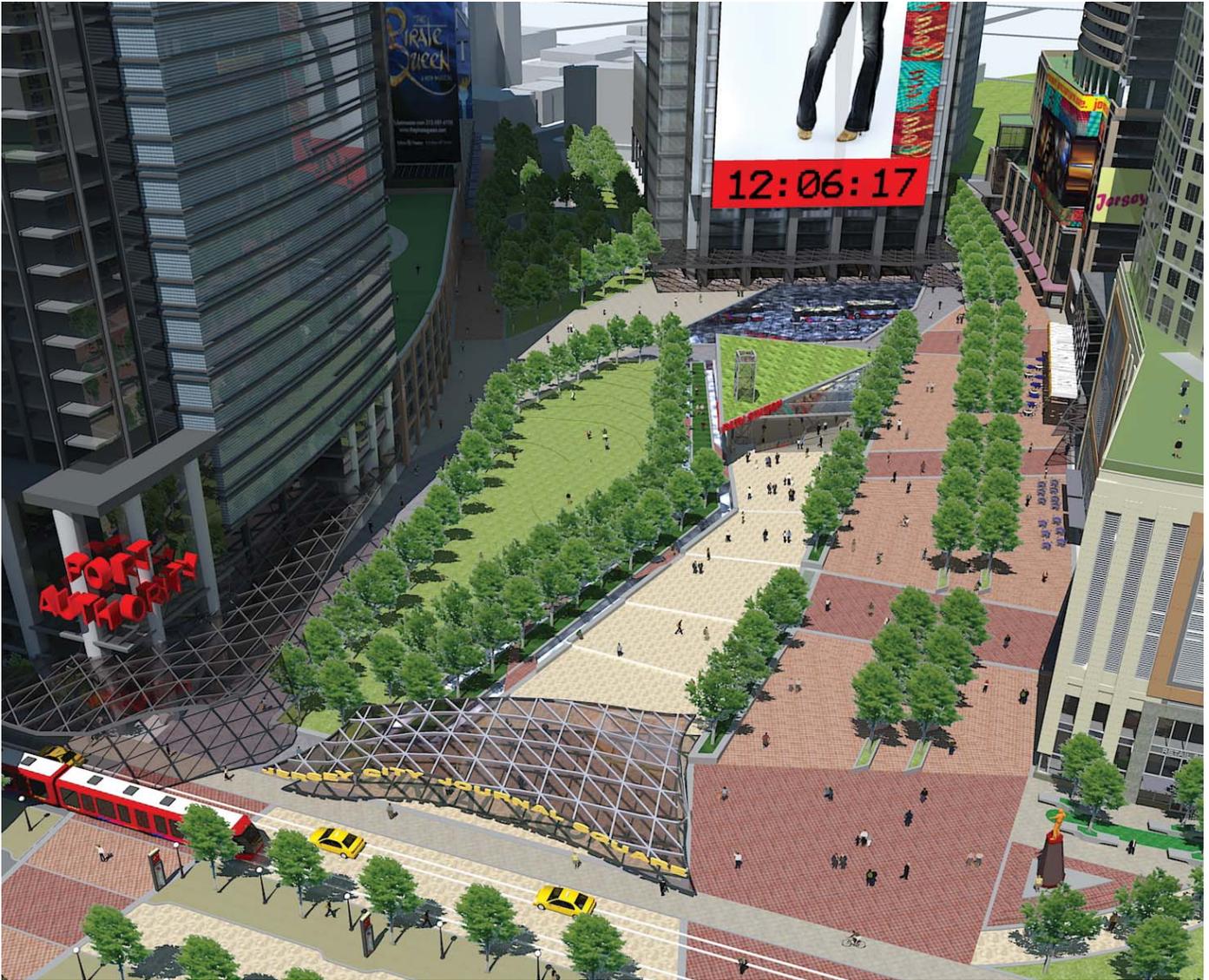
The Development Plan illustrates the proposed street network and recommended infrastructure improvements for the Area. This Plan requires that the Rail Cuts that currently divide the Study Area be decked over. The area of proposed decking is shown on the adjacent map. These investments will allow for better connectivity and circulation as well as create developable land for new buildings and much needed park space.

Orange lines indicate where existing streets must be widened to accommodate increased vehicular traffic within the Area. Portions of Sip Avenue, Summit Avenue, and Tonnelle Avenue must be widened in accordance with the specified street sections in Section 6 – Streets. It is strongly recommended that widening and streetscape improvements along Tonnelle Avenue be continued outside of the Area to the intersection with Routes 1 and 9.

The green dashed lines indicate where new streets must be developed. These streets will provide important connections and enhance vehicular circulation. These new streets/extensions must be built according to the specified street sections in Section 6 – Streets. Hoboken Avenue and Cook Street have been marked with a red dashed line to indicate their reconfiguration at some point in the future. As redevelopment occurs on the blocks adjacent to Hoboken Avenue and Cook Street, Cook Street will serve as an access mews. A portion of Hoboken Street will be used to accommodate a new local intercept public parking facility on this block.

Journal Square Plaza

Journal Square Plaza remains the centerpiece of the Area. This rendering shows how a reconfigured plaza can create a dynamic public urban space. The plaza will contain a covered waiting area along Journal Square, iconic entrance to a combined PATH Station and Bus Terminal, as well as a reflecting pool that allows the transit station to be partially day lit.





View of Walkway through Journal Square Plaza

Pedestrians are the lifeblood of Journal Square. Already one of the most walkable places in New Jersey, the Redevelopment Plan ensures that the pedestrian experience will be the focus as Journal Square is redeveloped. This rendering highlights a walkway along the southern edge of Journal Square Plaza. Ground level retail and outdoor seating activate the space while landscaping and paving make walking interesting and enjoyable.





Entrance to PATH Station and Bus Terminal

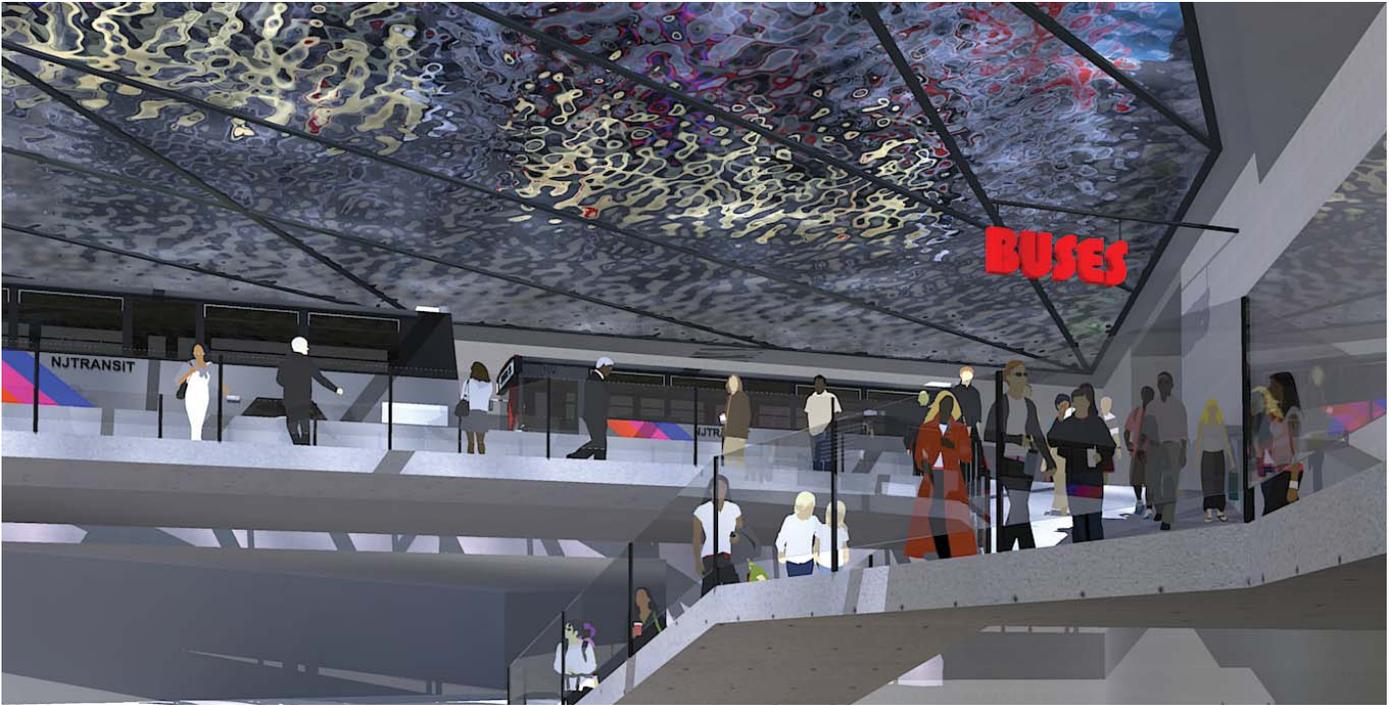
A new entrance to the Journal Square PATH Station and Bus Terminal will be one of the most prominent elements of Journal Square Plaza. A common entrance will provide access to the Journal Square PATH Station and regional bus terminal. The entrance has the potential to serve as a symbol of Journal Square's revitalization and should be of a distinctive architectural character.





Transportation Center at Journal Square

The Redevelopment Plan remakes the Journal Square Transportation Center into a modern transportation hub that combines access to regional buses and the PATH System below Journal Square Plaza. These renderings illustrate the open nature of the Transportation Center which is day lit through a glass bottomed reflecting pool located on the Plaza level.





View of Journal Square, Looking South

These rendering illustrate the transformation of Journal Square from an automobile oriented street to a pedestrian priority street with an emphasis on providing transit connections. Journal Square will once again serve as the primary location for local bus stops and be one of the major stopping points for a new streetcar line. Journal Square will serve as the beginning of a pedestrian priority street that connects Journal Square Plaza and City Center Park and extends along John F. Kennedy Boulevard to Tonnelle Avenue. The Loews Theater and Jersey Journal Building remain, providing a historic framework for new development.





View of Journal Square, Looking North

This rendering of Journal Square looking north further highlights the significant impact that transit accessibility and landscaping can have on improving the character of the Area. This rendering depicts the narrow gauge streetcar that is proposed to run along John F. Kennedy Boulevard, Journal Square, and Bergen Avenue. Such a line could connect Journal Square to McGinley Square and provide convenient transfers with the PATH system and local and regional buses.





City Center Park

The Redevelopment Plan seeks to remedy one of Journal Square's greatest deficiencies – the lack of public park and open space. The Redevelopment Plan proposes the creation of a vibrant network of open space on top of a deck that spans the existing railroad trenches. A portion of the City Center Park, west of Journal Square Plaza, is depicted in these renderings. The Park will include a mix of formal and informal spaces and provide much needed green space in the heart of Journal Square. The City Center Park is envisioned as a crucial link of the Jersey City Greenway that will extend to the Hudson River connecting the Area to the Waterfront.



Redevelop Journal Square

Section 3 Land Use





3.1 Land Use Overview

This Redevelopment Plan represents a great opportunity to create the appropriate framework for future development in Journal Square. The Land Use Plan establishes this framework by designating specific land uses for Journal Square and its surroundings that must be used to direct future land use decisions.

Land use districts were carefully placed to create a sense of place that encompasses the Journal Square City Center, neighborhood centers, shopping districts, and areas primarily dedicated to office space. The placement of land uses was carefully considered to ensure that adjacent uses complement each other and to encourage the most logical, comprehensive, and, most importantly, pedestrian-friendly design. Where possible, land uses mirror one another on each side of a street to create a coherent, well proportioned streetscape. Because mirroring uses is not possible in all areas, complimentary uses are placed across from one another when necessary. When this is done, the use of individual building widths (IBWs) with appropriate podium heights, cornice lines, and stepbacks can ensure that a consistent pedestrian scale of development is established.

The Land Use plan indicates the predominant use of a building or block at specific locations on the plan. When combined with the Frontage Plan a finer-grained land/building use plan emerges. To ensure consistency with existing City of Jersey City planning documents, the Journal Square Redevelopment Plan utilizes nomenclature from the City's Land Development Ordinance to describe recommended land uses.

The core of the Area is the Central Business District (CBD) zone which includes the Transportation Center and extends along Kennedy Boulevard, Sip Avenue, and Bergen Avenue. This district is intended to foster the development of a vibrant and accessible Citywide activity district that is a center of commerce and civic activity. A broad range of residential, commercial and government/public uses are permitted including mixed-use mid- and high-rise apartments, offices, transportation centers and colleges. This mix of uses reflects Journal Square's traditional role as the central business district as well as the dense pattern of development and extensive mass transit network that serves the area. Continuing this land use designation ensures Journal Square's continued revitalization and its role as Jersey City's downtown.

A Residential, Hotel, or Mixed-Use (R/H/MU) district surrounds the CBD and is the most common land use within the Area. This district is designed to accommodate both single use residential and hotel uses or mixed-use buildings in close proximity to the City Center. Attractive, context-sensitive, mixed-use, high-rise and mid-rise buildings that combine office, residential and retail uses are encouraged in this zone. The Redevelopment Plan seeks to increase the level of street vitality and foot traffic in this area, especially during the evening by providing opportunities for ground level neighborhood retail in the office/residential area to serve workers and residents.

The greatest departure from existing land uses recommended by the Redevelopment Plan is the addition of a significant amount of park and open space. A large portion of the deck spanning the railroad trench is envisioned as a park that inserts much needed green space into the heart of the Journal Square Central Business District. Finally, the William J. Brennan, Jr. Courthouse will continue to anchor Journal Square's traditional civic district. Each center will have a designated plaza or park.

The Land Use section sets forward standards for Land Use districts, permitted, interim, and prohibited uses, three levels of Modified Floor Area Ratios and Heights. The MFAR and Height plans have been developed in conjunction with the new Density Improvement Bonus (DIB) fund.

Exhibit 6 – Land Use Plan indicates the predominant uses for the Area. When combined with Exhibit 7 – Retail Frontage Plan, a more specific land/building use plan emerges.

The **Central Business District** requires a mix of at least two uses within each building, excluding accessory parking.

The **Residential, Hotel, or Mixed-Use District** continues the emphasis on mixed-use buildings from the CBD, but allows mid and high-rise single-use residential and hotel buildings.

The **Civil Uses District** includes lands dedicated to public institutions and uses, such as schools, community centers and government-owned or operated buildings, structures or land used for public purposes, not including houses of worship.

The **Parks, Plazas, and Open Space District** creates a system of open areas, parks, playgrounds, trails, paths and other recreational areas; scenic and historic sites. These sites will be open for the public but may be owned and maintained by either a public or private entities.

The **Palisade Protection Overlay** of the Land Development Ordinance which sets pre-existing regulations over the Area to protect the Palisades’ steep slopes. This overlay supersedes any regulations in this document that may be in conflict with it.

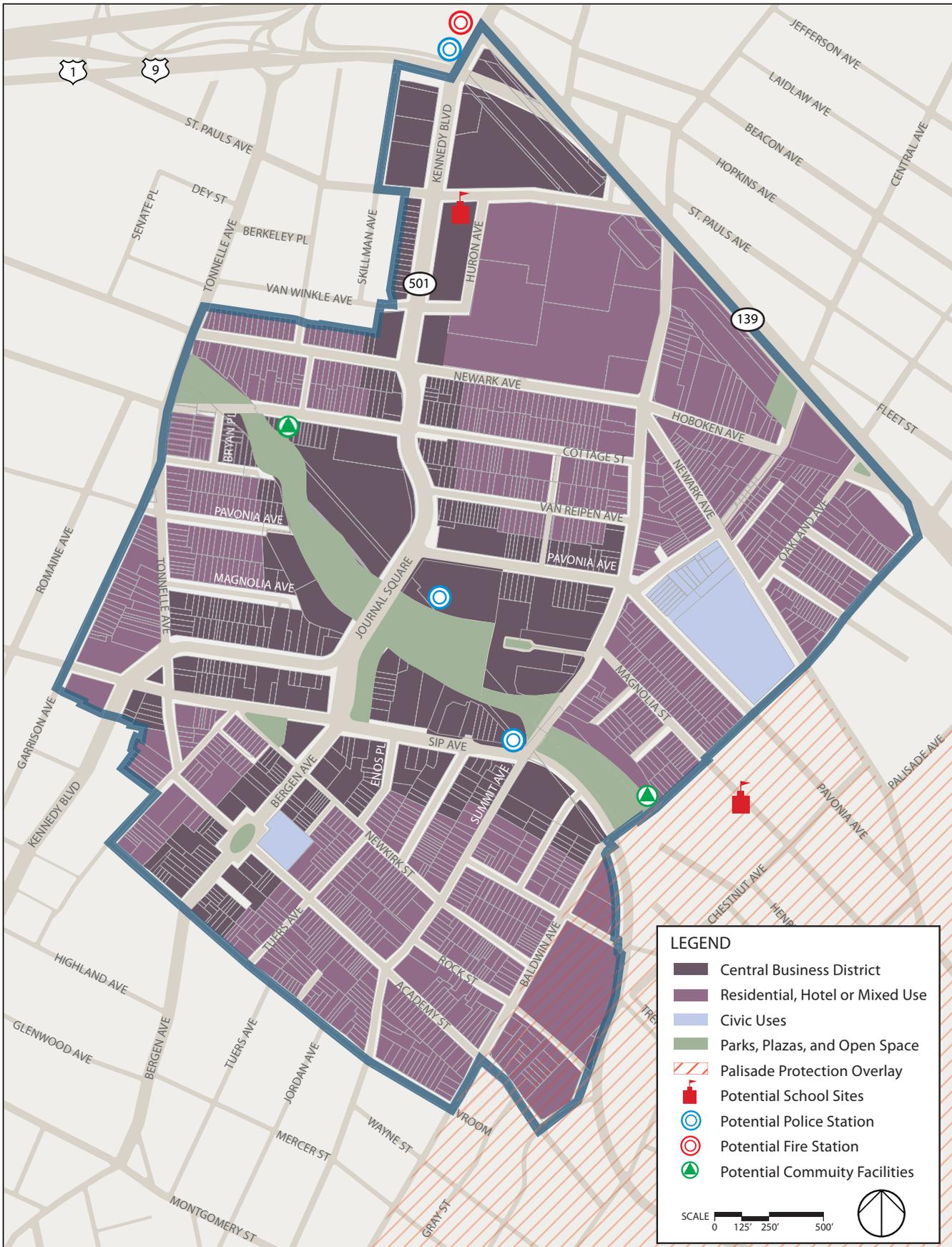
The Redevelopment Plan is intended to guide the development of the Area as it grows over the next fifty years. As the Area grows, additional community facilities such as police, fire and emergency rescue services, schools, and day/elder care will be required.

The construction of these types of facilities must be phased to meet the future needs of the Area taking into consideration the capacities of existing facilities. In addition to indicating the placement of land use districts, Exhibit 6 illustrates recommended locations for several types of community facilities. The map includes a potential location for a new school outside the eastern boundary of the Area as well as the possible reutilization of the St. Johns School on John F. Kennedy Boulevard. In addition, new schools should be incorporated into new mixed-use buildings.

Exhibit 6 also identifies potential locations for police and fire substations as well as opportunities for general community facilities. Because these facilities can often be integrated with existing buildings, they can be considered an in-land contribution in lieu of payment into the District Improvement Bonus fund.



EXHIBIT 6 - LAND USE PLAN





In the event of any conflict or inconsistency between the provisions of this Plan and the provisions of the City of Jersey City Land Development Ordinance, this Plan governs, except for the Palisade Protection Overlay of the Land Development Ordinance which sets pre-existing regulations over the Rehabilitation Area to protect the Palisades' steep slopes. This overlay supersedes any regulations in this document that may be in conflict with it.

For the purposes of thorough communication, redundancies may exist in the text. If there are any conflicts between these Land Use Regulations and information contained elsewhere in this section, these Land Use Regulations will take precedence.

The Land Use Plan sets forth specific and general recommendations for the redevelopment of the Area. The Land Use Categories define which general uses are permitted within each district. The Land Use Definition defines the exact permitted land uses as they relate to the Land Use Categories section. The Land Use Definitions also define permitted interim uses and examples of prohibited uses for the area. In addition to any uses listed here, there may be other uses of similar character that may be appropriate for the Rehabilitation Area provided they are not specifically listed under prohibited uses.

Specific application of the land use and development requirements of this Plan, as they affect existing uses, will be as follows:

- A. Nonconforming uses must become conforming uses if and when substantial modifications are made to the structure.
- B. All land use rights are vested from previous Redevelopment Plans or zoning codes, see appendices B, C, and D for previous Redevelopment Plans.

LAND USE CATEGORIES

Central Business District (CBD) Permitted Uses

The **Central Business District** can contain numerous commercial, residential, institutional and government/public uses in a high intensity urban context.

Two or more of the following permitted principal uses are required for any building in the Central Business District:

1. Government uses
2. Schools, public and private colleges, universities, and technical/vocational schools
3. Stand alone Parking garages (does not include in-building, under-building, or podium parking)*
4. Retail sales of goods and services
5. Offices
6. Hotels
7. Conference centers
8. Restaurants (category one and two)
9. Financial institutions without drive-thru facilities
10. Theaters/Cinema
11. Live-Work
12. Work-Live Artist Studio
13. Mid-rise apartments
14. High-rise apartments
15. Medical offices
16. Child Day Care Centers
17. Community Centers
18. Houses of Worship
19. Health spas, gyms, clubs



Uses incidental and accessory to the principal uses, such as:

1. Parking garages (in-building, under-building, or podium parking)
2. Meeting rooms, conference facilities, gymnasiums and exercise rooms and pools

*Must be mixed with retail, services, or office along the entire pedestrian realm, including sidewalks and lane ways.

Residential, Hotel or Mixed-Use District Permitted Uses

The **Residential, Hotel or Mixed-Use District** permits a mix of commercial, residential, institutional and government/public uses in a high to medium intensity urban context.

Permitted Single Uses:

1. Mid-rise apartments
2. High-rise apartments
3. Hotels
4. Government uses
5. Schools, public and private colleges, universities, and technical/vocational schools

Two or more of the following permitted principal uses must be used in any mixed-use building in the Residential, Hotel or Mixed-Use District:

1. Stand alone Parking garages (does not include in-building, under-building, or podium parking)*
2. Retail sales of goods and services
3. Offices
4. Hotels
5. Conference centers
6. Restaurants, category one and two
7. Financial institutions without drive-thru facilities
8. Theaters
9. Live-Work
10. Work-Live Artist Studio
11. Mid-rise apartments
12. High-rise apartments
13. Medical offices
14. Child Day Care Centers
15. Community Centers
16. Houses of Worship
17. Health spas, gyms, clubs

Uses incidental and accessory to the principal uses, such as:

1. Parking garages.
2. Meeting rooms, conference facilities, gymnasiums and exercise rooms and pools.

*Must be mixed with retail, services, or office along the entire pedestrian realm, including sidewalks and lane ways.

Civic District Permitted Uses

The **Civic District** can contain a mix of institutional and government/public uses in an urban context.

The following permitted principal uses may be used in any building in the Civic district:

1. Government uses.
2. Schools, public and private colleges, universities and technical/vocational schools.



3. Stand alone Parking garages owed by the City or the County (does not include in-building, under-building, or podium parking)*
4. Conference centers municipally funded
5. Community Centers

Uses incidental and accessory to the principal uses, such as:

1. Parking garages.
2. Meeting rooms, conference facilities, gymnasiums and exercise rooms and pools.

*Must be mixed with retail, services, or office along the entire pedestrian realm, including sidewalks and lane ways.

Parks, Plazas, and Open Space

The Parks, Plazas, and Open Space District can contain a mix of active and passive recreational uses, public displays, cafes, outdoor theater, kiosks, restaurants, and farmers market.

See the Landscape Section for additional definitions of permitted uses and functions for this district.

LAND USE DEFINITIONS

Permitted Uses

The following uses shall be permitted within the Area as defined by their district on the Land Use Plan.

- A. **Apartments, mid-rise and high-rise:** Includes premises available for long-term human habitation by means of ownership or rental, but excluding periods of less than a month's duration; excludes all boarding houses and rooming houses. A building containing three or more dwelling units that share common horizontal and vertical separations.
- B. **Office:** A place for the transaction of business where reports are prepared, records are kept and services rendered, but where no retail sales are offered and where no manufacturing, assembly or fabricating takes place. General and Professional offices, including, by way of example, but not limited to, general offices including doctors, dentists, lawyers, accountants, architects, and financial institutions and government offices.
- C. **Live-Work:** A single, enclosed, private space within a building that is designed to accommodate the two distinct functions of the living and working environment of residents who are creative in their professional work at home. This accommodation shall occur through the provision of appropriate ceiling heights, spatial arrangement, ventilation, sound attenuation and such other design considerations appropriate to a shared living and working environment; and the space/unit must also meet all appropriate codes for residential occupancy. Not more than one-half of the area of the total space may be dedicated to work space for the use of a person or persons engaged in a profession or other occupation in a business office setting. The remaining area within the space must be used for residential purposes and the person engaged in the profession or occupation must reside within the premises. Not more than two full time or full time equivalent employees not residing within the premises may be employed. The professions and occupations shall include:
 1. Computer or information technology.
 2. Computer graphics and computer aided design.
 3. The graphic arts.
 4. The offices of architects, planners, lawyers, accountants and other business professions.
 5. Similar business occupations as determined by the Planning Board.
- D. **Work/Live Artist Studio:** A single, enclosed, private space where at least one-half of the volume of the total space is devoted to work space for the creation, display and sale of art, and the remainder is used for living purposes. A minimum of one hundred fifty (150) square feet of living space per person occupying such work/living space shall be required. Nothing in this definition shall prohibit the use and occupancy of a "work/live artist studio" in a setting where shared kitchen and/or bath facilities are available, provided that applicable health and safety codes are met and maintained.



- E. **Retail:** An establishment engaged in selling goods or merchandise to the general public for personal or household consumption and rendering services incidental to the sale of such goods. An establishment providing services, as opposed to products, to the general public for personal or household use.
- F. **Restaurant, Category One:** A restaurant which is designed for and whose primary function and operation is the preparation and service by employees of meals to a customer or customers seated at the table at which the meal is consumed. A category one restaurant operates without substantial carry-out service; with no delivery service; with no drive-thru, drive-in, or service in vehicles; and without service at counters or bars unless the restaurant is licensed to serve alcoholic beverages.
- G. **Restaurant, Category Two:** A restaurant whose primary function is the preparation and service by employees of food or drink to customers as part of an operation designed to include substantial carry-out service; delivery service; self-service, and which may also include on-premises consumption, except that no drive-in, drive-thru, or service in vehicles is permitted.
- H. **Hotels:** A building designed for occupancy as the more or less temporary place of abode for individuals who are lodged with or without meals, in which there are ten (10) or more guest rooms or suites and in which there may be kitchens in any individual room or suite.
- I. **Governmental Uses:** Public institutions and uses, such as schools, community centers and government-owned or operated buildings, structures or land used for public purposes, not including houses of worship.
- J. **Garage, Parking:** Buildings used exclusively for the parking or storing of motor vehicles. Parking includes stand alone structures with retail or office at the base. Embedded, under-building, in-building, podium, or any other type of parking primarily serving a primary use shall be designated an accessory use and shall not be included in the minimum number of uses required per building.
- K. **House of Worship:** A building used for the assembly of members of a designated faith for religious instruction and worship of a deity such as a church, synagogue, mosque or temple.
- L. **Parks and Plazas:** Open areas, parks, playgrounds, trails, paths and other recreational areas; scenic and historic sites. These sites will be open for the public but may be owned and maintained by either a public or private entities.
- M. **Mixed-Use** that contains more than one permitted use.

Permitted Accessory Uses and Structures

A use of land customarily incidental and subordinate to the principal use of the land or building and located on the same lot with the principal use.

- A. Flag poles, landmarks, architectural features, gateway monuments, kiosks, outdoor displays, and bicycle rental racks.
- B. Private recreation facilities, including community buildings/clubhouses, swimming pools, tennis courts, etc.
- C. Wireless antennas.
- D. Embedded, under-building, in-building, podium, or any other type of parking primarily serving a primary use.
- E. Large Signs/Animated signs within the Journal Square Plaza only or by the determination of the Jersey City Planning Board.

Interim Uses

The following shall be permitted Interim uses in the Area. The duration during which such uses may remain in place shall be determined by the City of Jersey City. Any interim uses must not be a prohibited use.

- A. Parks.
- B. Construction Staging Areas.
- C. Temporary signs advertising the developments and sponsors.
- D. Any other use that will not encumber the property in any way as would hinder the ultimate development of the permanent permitted use.



Prohibited Uses

Any use not expressly permitted shall be prohibited within the Area. The following list includes examples of uses that are prohibited and is not intended to be exhaustive of all prohibited uses.

- A. Drive-through commercial uses where patrons remain in automobiles.
- B. Gas and service station and car wash facilities.
- C. Vending machines, except within buildings.
- D. Billboards.
- E. Depots for large scale storage or distribution of goods.
- F. Scrap yards for the processing, storage, and disposal of waste materials.
- G. Automotive sales, repair, or long-term storage.
- H. Adverse Impacts in General: uses with negative consequences for a use on adjacent lots, usually as a result of odor, vibration, noise, pollution, or socioeconomic disruption. Consequences confined to the lot boundary are not considered to create adverse impact. Specific performance standards may be set by the City.
- I. Surface parking lots along street frontages with the exception of temporary parking lots.
- J. No junked motor vehicles or parts thereof or boats shall be permitted to be stored on any lot within the Area. Outdoor parking of vehicles that are inoperable or unregistered shall be prohibited. Automotive repairs of any type are prohibited within the Area.
- K. Stand-alone, single use commercial parking lots, structures, or parking fields are not permitted (Mixed-use parking structures and embedded parking are allowed and encouraged, but shall not exceed maximum parking standards).
- L. Chain link fencing shall be prohibited along all street frontages within the Area, except during construction. Chain link fencing for construction shall be dismantled and removed prior to the issuance of a Certificate of Occupancy.
- M. The use of razor wire, barbed wire or other similar material is expressly prohibited within the Area.
- N. Warehousing or distribution facilities.
- O. Junk yards, recycling facilities, or outdoor storage.
- P. Industrial or manufacturing operations
- Q. Sale, display, or distribution of pornographic material.
- R. Cell phone towers, except within buildings or hidden by façades.
- S. Stand alone big box retail.
- T. Storage facilities.



3.4 Retail Frontage Plan

The Retail Frontage Plan indicates areas of the Rehabilitation Area where ground level retail is required or encouraged. Mandatory Retail Frontages are illustrated by solid red lines while recommended Retail Frontages are depicted by a dashed line. All locations not indicated on the Retail Frontage Plan must receive approval from the Planning Board.

The strategic location of ground floor retail is crucial to achieving the goals of increasing street activity and enhancing the pedestrian realm. Ground level retail is often combined with street trees, wide sidewalks, decorative lighting and, where possible, on-street parking to form a buffer between moving traffic and the sidewalk in lieu of on-street parking a line of bollards is recommended. The Redevelopment Plan locates mandatory retail frontages along John F. Kennedy Boulevard from Newark Avenue to Tonnelle Avenue and along Bergen Avenue south of the Journal Square station. The Frontage Plan also reinforces Newark Avenue's traditional role as a shopping corridor and suggests retail should also be placed along the Central Avenue extension near the proposed Hudson Bergen Light Rail stop.



EXHIBIT 7 - RETAIL FRONTAGES

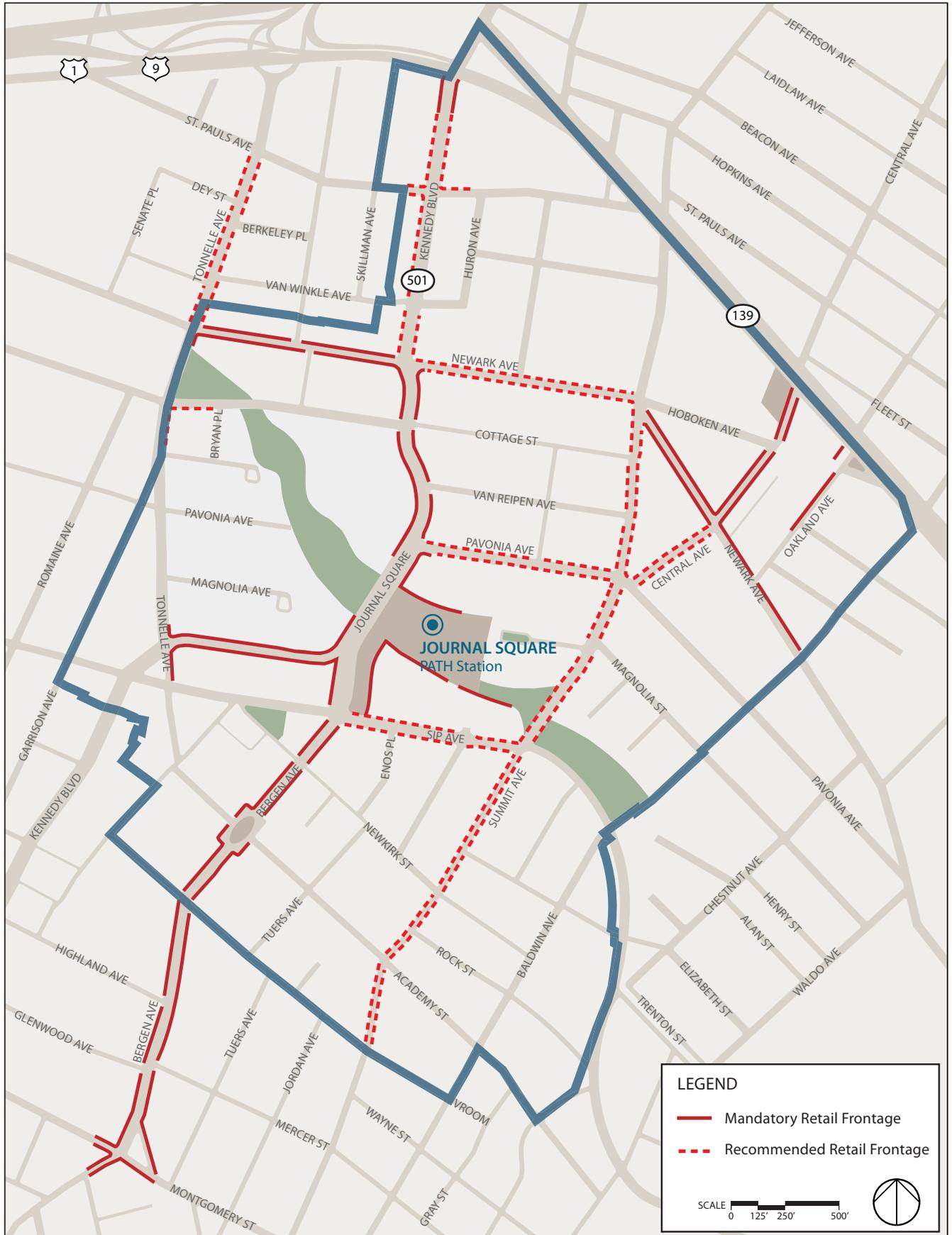
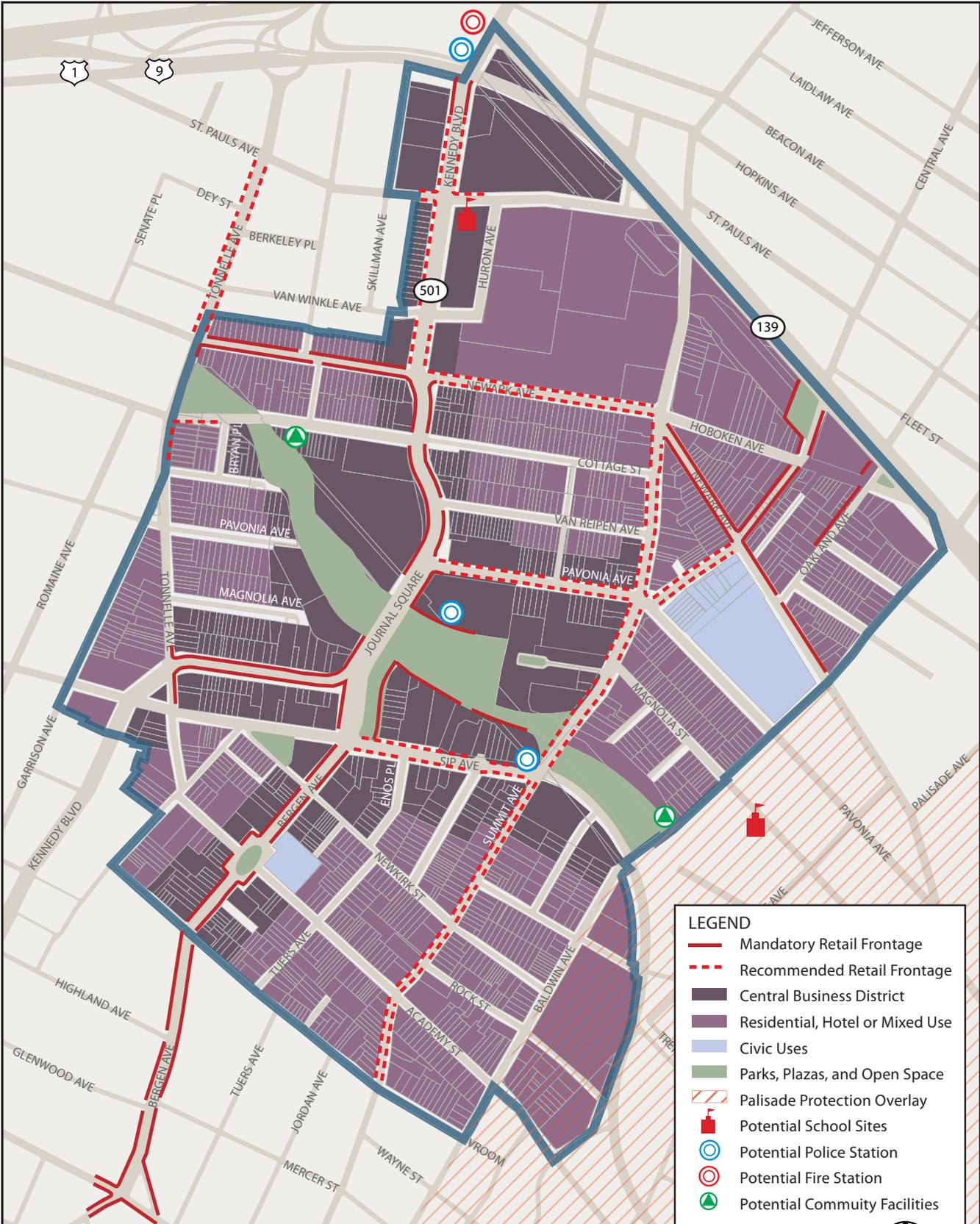




EXHIBIT 8 - SUPERIMPOSED PLAN



This plan combines both the Land Use plan and the Retail Frontage Plan to provide a more complete overview of land use and retail frontages for the Redevelopment Plan.



BASE MODIFIED FLOOR AREA RATIO AND HEIGHT STANDARDS:

This Plan requires that Modified Floor Area Ratios (MFAR) be used in conjunction with the Height Plans (Exhibits 12, 13 and 14) and other design guidelines (see Section 8 - Design Standards) to regulate the future form of the Area. MFAR expresses the relationship between the amount of floor area permitted in a building (or buildings) and the area of the lot on which the building stands. MFAR is obtained by dividing the gross floor area of a building (including any parking structure incorporated therein) by the total developable area of the lot. The developable area of a lot is determined by the Build-To Lines (BTL), which follow the public rights-of-way and park and plaza edges. For a further description on Build-To Lines see Section 6 – Streets; Section 7 – Landscape; and Section 8 – Design Standards. Any area of a building fully located below finished grade and sidewalk level adjacent shall not be added to a building’s chargeable floor area in terms of maximum development potential of a site.

Within this plan, three Height Plans and three MFAR Plans have been developed. The first set regulates the base Heights and MFARs for the area (see Exhibits 12 and 9, respectively, hereinafter referred to as the “Base Plans”). These Height and MFAR standards constitute the basic zoning (subject, as always, to Site Plan approval from the Planning Board).

Additional MFAR may be obtained through the use of the District Improvement Fund Bonus; the District Improvement Fund Bonus in conjunction with the Workforce Housing Bonus; or the Transit Accessibility Bonus, in conjunction with the District Improvement Fund Bonus and, if applicable, the Workforce Housing Bonus.

DISTRICT IMPROVEMENT FUND BONUS:

There is hereby established pursuant to this Plan a District Improvement Fund Bonus (the “DIB”). (See Exhibits 10 and 13.) The purpose of the DIB is to allow development at an increased density in the Area, up to a maximum specified amount, provided that a declaration of covenants and restrictions is executed and recorded and that a contribution has been deposited in the Journal Square District Improvement Fund (the “Fund”). The execution and recording of the declaration and the payment in full of such non-refundable contribution shall be a precondition to the filing for or issuance of any building permit allowing more than the basic maximum floor area for a development, enlargement or conversion. The redevelopment agreement shall address the timing and terms of payment of the contribution vis-à-vis the timing of the project approval process; provided, however, that each applicant seeking to utilize the DIB shall execute an escrow agreement and pay a deposit of ten (10%) percent of the estimated amount of the DIB contribution (the “Deposit”) when the applicant submits its application for site plan approval. The Deposit shall be held in escrow subject to the terms of the escrow agreement, pending completion of the approvals process. The Deposit, together with interest earned, less an administrative fee to be retained by the Agency equal to the greater of 33.3% of the interest earned on the Deposit or 1% of the Deposit, shall be refunded to the applicant in the event the application is withdrawn or denied.

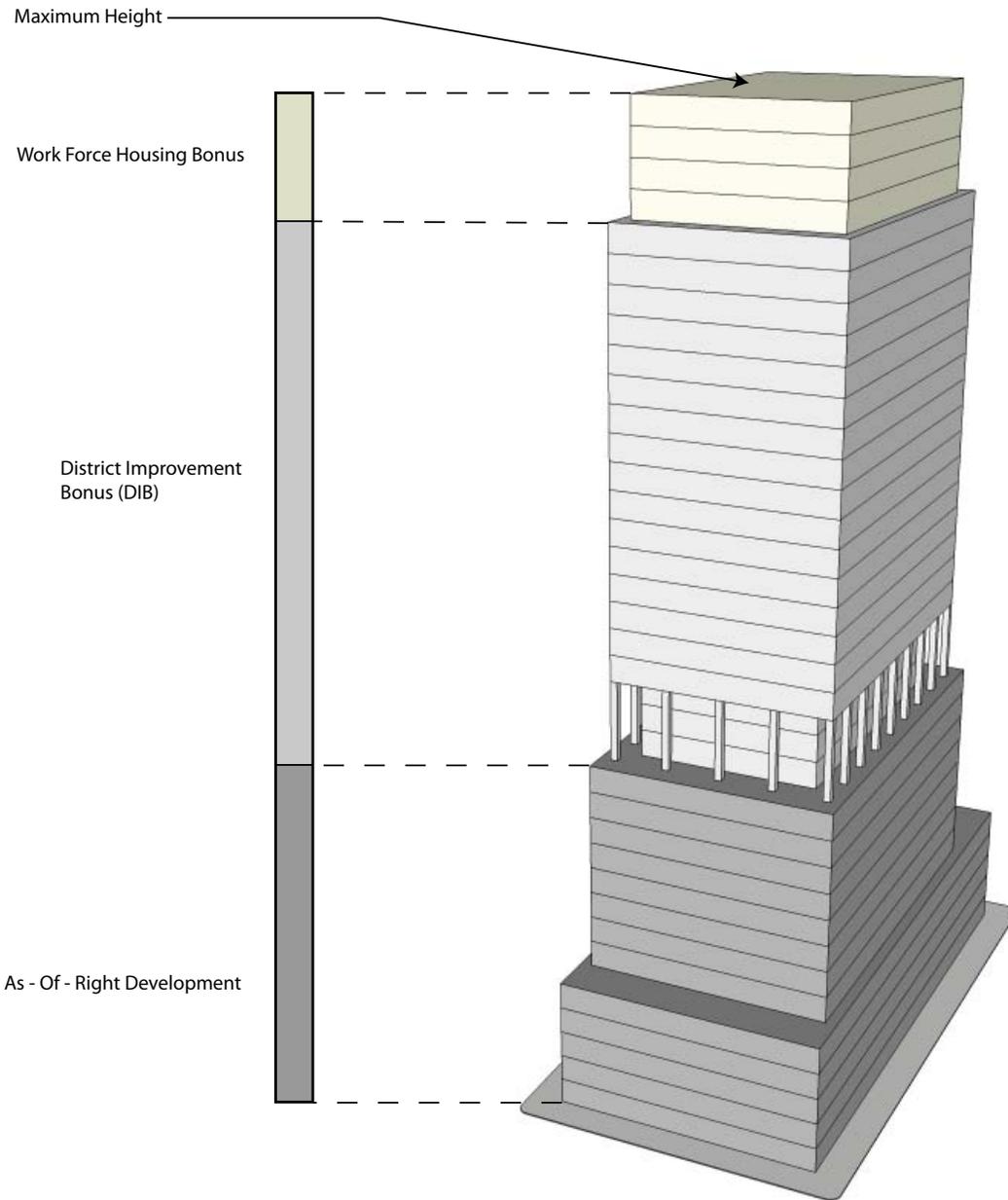
The Fund shall be a segregated account of the Jersey City Redevelopment Agency (the “Agency”). The Fund shall be administered by the Agency with the advice and consent of the District Improvement Fund Advisory Board hereinafter established, and may be used, pledged, or assigned in connection with the financing of improvements within the Area.

The Building Division shall not issue any building permit authorizing the construction of any development or enlargement utilizing the DIB established in this Section, including foundations with respect thereto, until the Agency has certified that the requirements of this Section have been met and the DIB contribution has been paid in full.

As of January 1, 2009 the DIB contribution amount shall be ten dollars (\$10) per square foot of gross floor area, and thereafter shall be adjusted automatically on January 1 of each successive year (the “Annual Adjustment”). The DIB contribution may also be adjusted no more than twice in any calendar year by the District Improvement Fund Advisory Board, as defined below (the “DIFAB Adjustments”). The Annual Adjustment shall be calculated by escalating the prior year’s DIB contribution amount by the Consumer Price Index for All Urban Consumers (“CPI-U”), provided that the CPI-U for the given period is a positive number. To the extent that the CPI-U for a given period is a negative number, no Annual Adjustment shall occur.



EXHIBIT D - DISTRICT IMPROVEMENT BONUS GRAPHIC



Development Example:

Base MFAR of 10, eligible for an additional bonus MFAR of 20

This hypothetical illustration demonstrates one possible development outcome for a parcel governed by a modified floor area ratio bonus system. In this scenario, a 36 story building using the total site area is proposed on a parcel with a base modified floor area ratio of 10. By utilizing the DIB they were awarded additional floor area of 20. This bonus is applied by adding an additional 20 stories to the development. By setting aside 10% of the gross floor area to workforce housing, the development earned an additional 10% to the building area.



The contribution amount for a project shall be determined based upon the rate in effect as of the date the redevelopment agreement is executed, and based upon the MFAR of the entire project, including the additional density. Contributions, including any portion thereof, may be made only on days when the Agency is open for business and during business hours as specified by the Agency.

MODIFIED FLOOR AREA RATIO AND HEIGHT STANDARDS PURSUANT TO THE WORKFORCE HOUSING BONUS:

There is hereby established pursuant to this Plan a Workforce Housing Bonus (“WHB”), to be utilized in conjunction with the DIB. Additional Height and MFAR in addition to that permitted pursuant to the DIB are allowed and encouraged for the provision of “workforce housing” as defined by the regulations of the New Jersey Housing and Mortgage Finance Agency in effect as of the date of this Plan.

Additional Height and MFAR allowed under this section may equal but not exceed 20% of the total gross floor area of the building (including the DIB), if not less than 10% of the entire building is devoted to workforce housing. For each square foot of workforce housing provided, two square feet of market rate housing may be added, up to the maximum WHB.

Workforce housing units shall be distributed throughout the building utilizing the WHB, if the building contains a residential use. Where a residential use is not present in a building, the units must be constructed at a different location within the Area. The WHB is separate and distinct from any COAH-related obligations resulting from construction of a project.

MODIFIED FLOOR AREA RATIO AND HEIGHT STANDARDS PURSUANT TO THE TRANSIT ACCESSIBILITY BONUS:

Upon the completion of a station and the institution of light rail service within the Area along the Bergen Arches there shall be established pursuant to this Plan, within the portion of the Area so designated in Exhibits 11 and 14, a Transit Accessibility Bonus (“TAB”), to be utilized in conjunction with the DIB and, if applicable, the WHB. Additional Height and MFAR in addition to that permitted pursuant to the DIB and WHB are allowed in locations adjacent to light rail stops, as and to the maximums set forth in Exhibits 11 and 14, the “Transit Plans”.

District Improvement Fund Advisory Board - Membership, Terms, Voting, Quorum and Powers:

There is hereby established a District Improvement Fund Advisory Board to advise and assist the Agency in connection with the administration of the Fund.

Membership: The District Improvement Fund Advisory Board (“DIFAB”) shall consist of the following seven (7) members:

- 1) the Business Administrator of the City of Jersey City, ex officio;
- 2) the Planning Director of the City of Jersey City, ex officio;
- 3) the Director of the Jersey City Redevelopment Agency, ex officio, who shall serve as chair of the DIFAB;
- 4) the Director of the Office of Strategic Revitalization of the County of Hudson, ex officio;
- 5) a member of the City Council of the City of Jersey City, appointed by a vote of the City Council; and
- 6) two public members, appointed by the Mayor.

Terms: The ex officio members of the DIFAB shall serve for a term concurrent with their employment in the respective positions enumerated. Those members appointed by the Mayor shall serve for one-year terms, but may be reappointed to additional one-year terms. The member of the City Council appointed by a vote of the City Council shall likewise serve a one-year term, subject to reappointment, provided however that if at any time during the one-year term on the DIFAB that



member of the City Council shall cease to serve on the City Council, his or her seat on the DIFAB shall be declared vacant and the City Council shall select a replacement from among its members by vote.

Voting and Quorum: Four members of the Board shall constitute a quorum for the purpose of transacting business. All meetings shall be conducted pursuant to the Open Public Meetings Act, N.J.S.A 10:4-1 et seq. Each member of the DIFAB shall have one equal vote.

Powers: The DIFAB shall have the following powers:

- 1) To procure professional, expert advice regarding the proper value of the Fund contribution;
- 2) To set the amount of the Fund contribution via the DIFAB Adjustments, no more than twice per calendar year;
- 3) Upon the recommendation of the Agency, to temporarily decrease the contribution amount, increase the contribution amount above the limits described above, or to temporarily suspend the use of the Fund program, in response to extraordinary financial or market conditions, provided, however, that the DIFAB must revisit such decrease, increase or suspension at least once every six (6) months.
- 4) To approve the utilization of an equivalent in-kind contribution rather than payment of the Fund contribution. For example, the DIFAB may approve the completion of certain infrastructure or other capital improvements by a designated redeveloper in lieu of all or a portion of the required contribution to the Fund. In such an instance, the redeveloper shall post a performance bond in favor of the City and the Agency for the completion of the improvements.

The DIFAB may also approve the establishment of or contribution to a theater, museum, cultural institution or performance space within the Area and open to the public, in lieu of all or a portion of the contribution.

- 5) To advise the Agency on the use of the monies within the Fund through the formulation of capital recommendations on an annual basis. Recommended uses for the Fund include, but are not necessarily limited to, the following capital items, including the acquisition of all materials, real property and equipment necessary therefor, as well as all costs necessary therefor and incidental thereto:
 - a) Public streetscape and landscape improvements in addition to on- and off-site improvements required for new development and rehabilitation;
 - b) Construction of new decking and a linear park over the existing railroad cut;
 - c) The creation of new streets and the widening of existing streets where appropriate;
 - d) The construction and maintenance of public parks and plazas;
 - e) The installation of Intelligent Transportation Systems for signalization;
 - f) Upgrades to utilities infrastructure, including but not limited to sewer, water and stormwater capacities;
 - g) The creation of bicycle infrastructure (e.g. bike lanes) and facilities;
 - h) Development of new transit systems such as a narrow gauge streetcar or the expansion of existing transit lines to service the Area;
 - i) The subsidization of solar, wind or other "green" technologies for projects within the Area;
 - j) The development of Remote (Local and Regional) Parking to serve the Area; and
 - k) The preservation of historic and cultural resources within the Area.
- 6) To procure professional services to audit the Fund on an annual basis, which audit shall be filed for public viewing in the Office of the City Clerk as well as at the Agency's offices.

EXHIBIT 9 - BASE MFAR PLAN

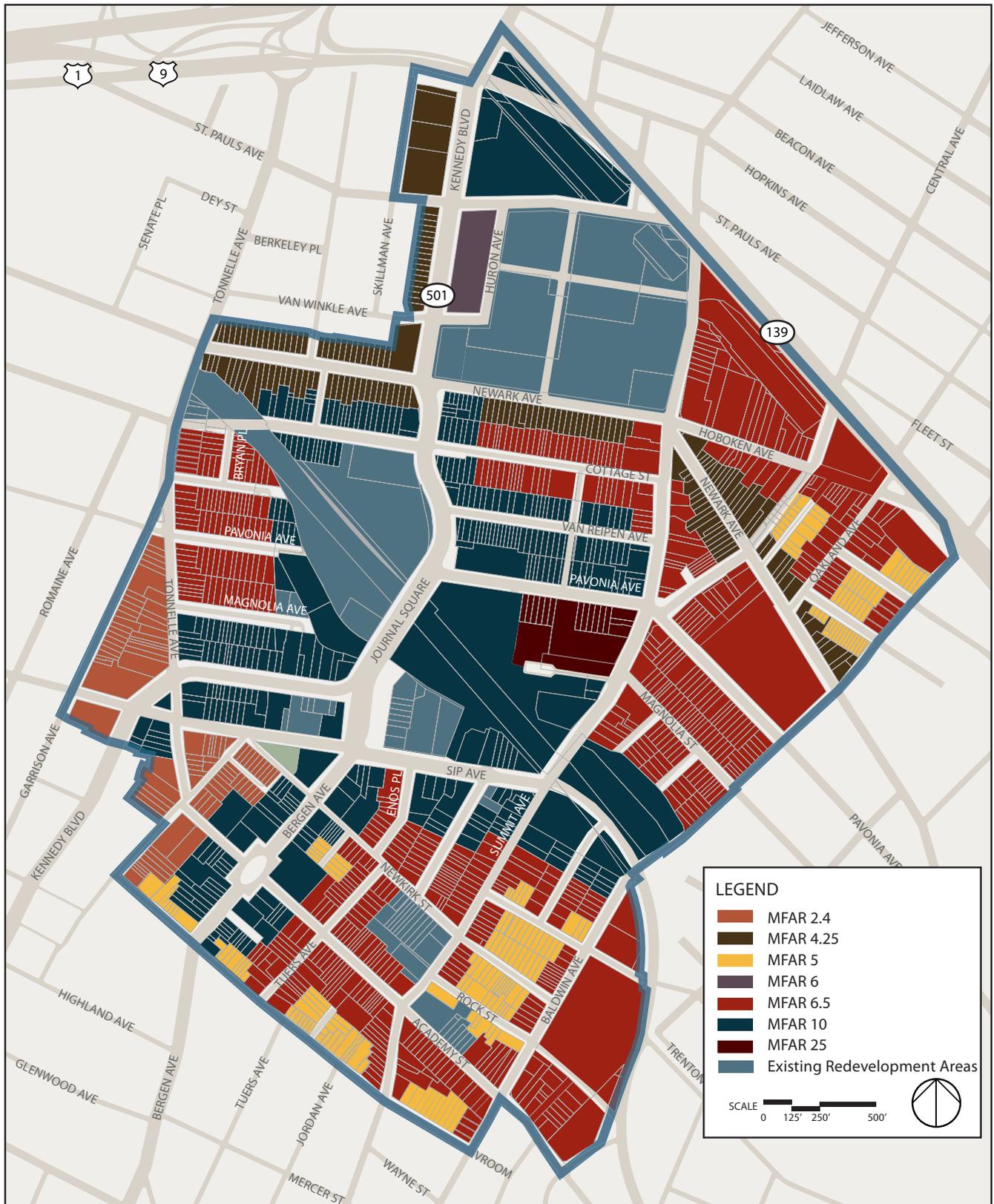
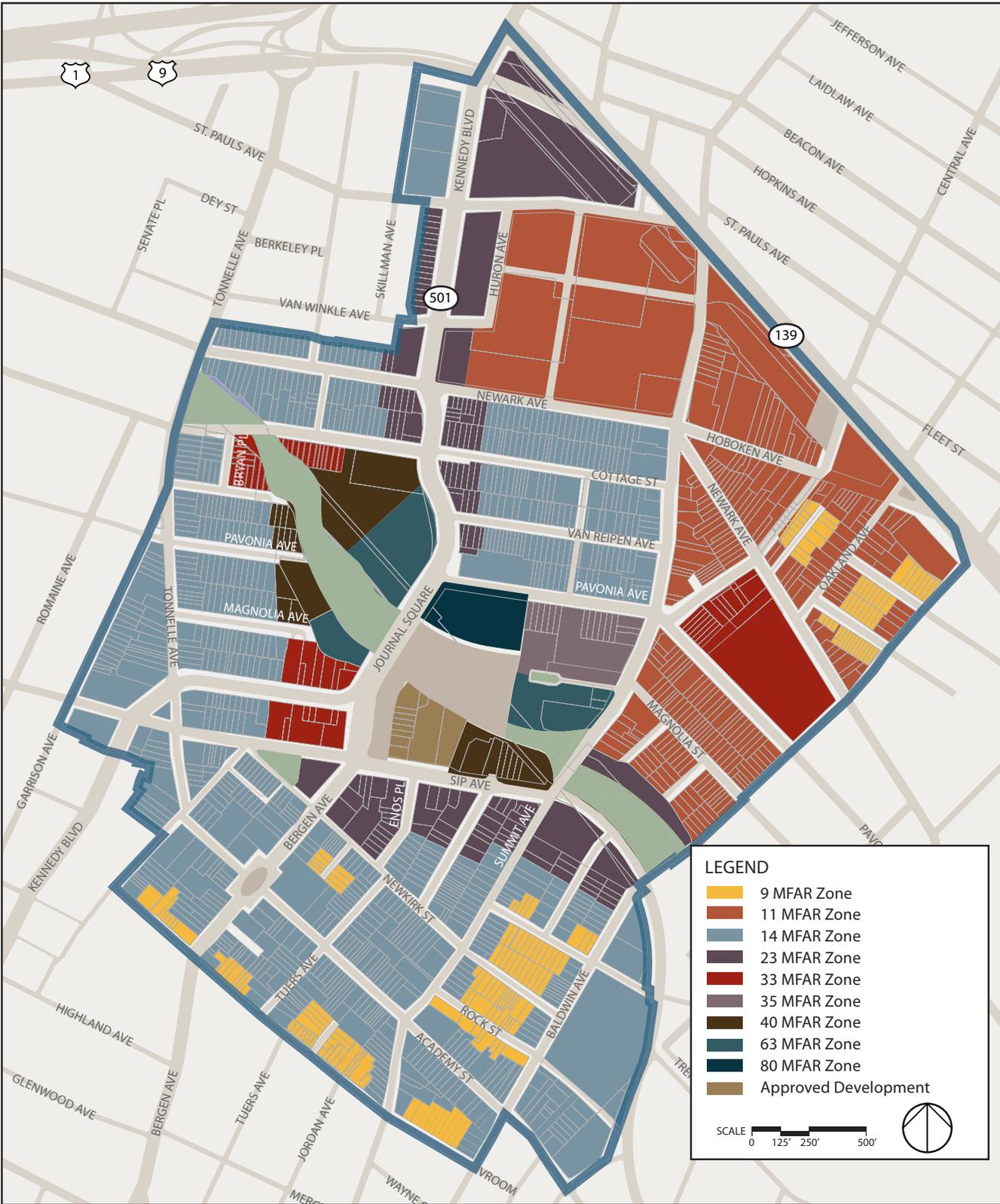


Exhibit 9 illustrates the base modified floor area ratio for the Journal Square Study Area. The Base MFAR is the permitted floor area. A developer is allowed to build up to the maximum base MFAR provided that the development standards and design guidelines are met.



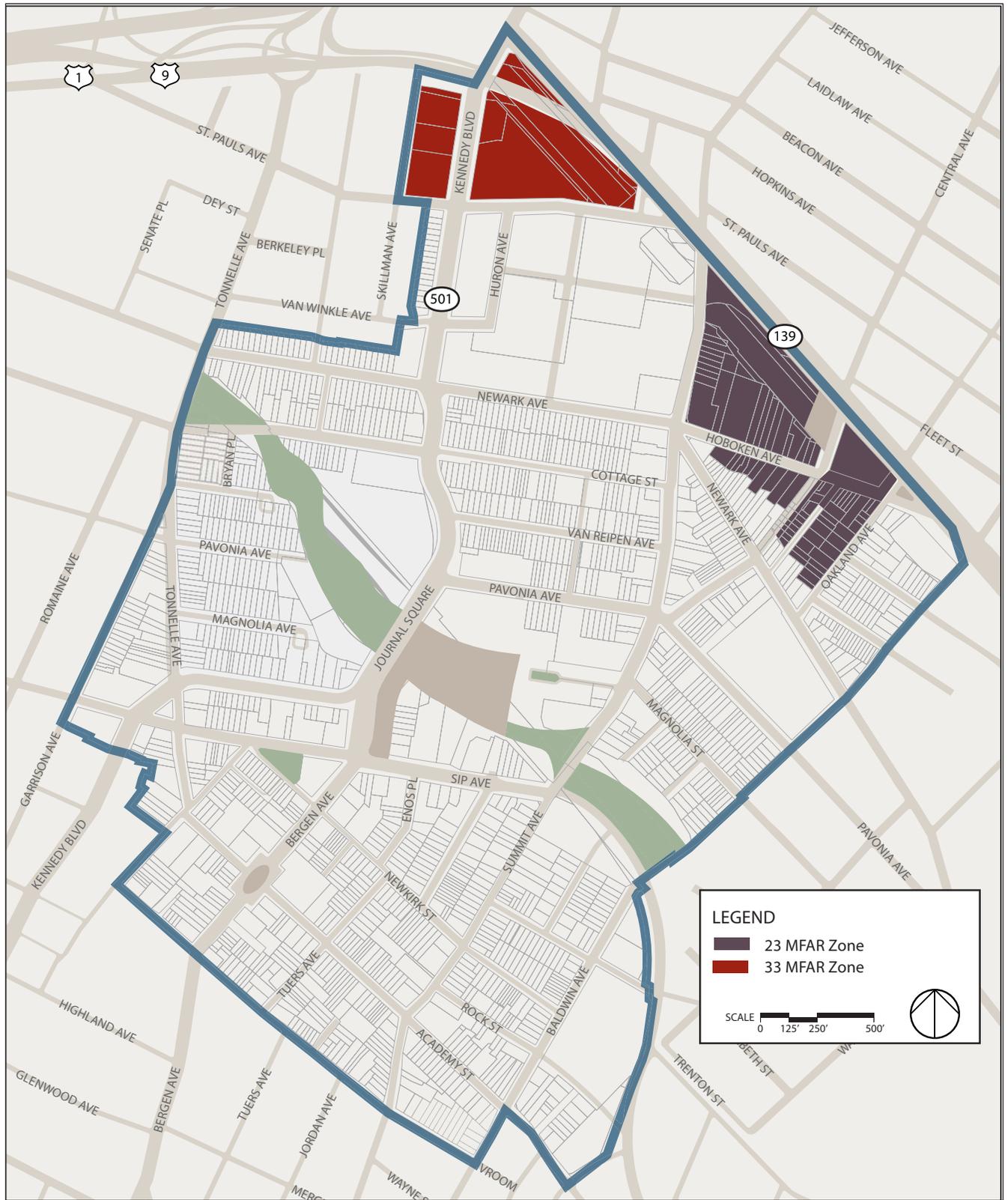
EXHIBIT 10 - BONUS MFAR PLAN



The Bonus MFAR plan sets out the maximum Modified Floor Area Ratios under the District Improvement Bonus program described previously. No MFAR above these maximums is allowed, except for the 10% workforce housing bonus. A developer must subtract the base MFAR for their property from the maximum MFAR to determine how much MFAR is available through the DIB.



EXHIBIT 11 - TRANSIT OVERLAY BONUS MFAR PLAN



The Transit Maximum Bonus MFAR Plan allows for increases in Modified Floor Area Ratios to the Bonus MFAR Plan if and when transit is extended along the Bergen Arches. There are two areas that receive increased height bonuses, both at proposed future transit stations.



3.8 Heights Plans

There are three building heights maps that apply to the Area. The first is the Base Heights Plan, the second is the Bonus Heights Plan and the third is the Transit Heights Overlay Plan.

Building heights are expressed in feet for each district and does not include the additional height needed for mechanical enclosures, architectural embellishments, building crowns, or roof access. There are three Heights Plans. The first illustrates the Base Maximum Heights allowed. All development in the Area will be held to a Base Height and Modified Floor Area Ratios. In order for developers to build above the base Heights and Modified Floor Area Ratios, they will need to utilize the District Improvement Bonus described above.

The maximum heights do not include the mechanical penthouses, parapets or architectural embellishments for the “crown” of the building or antennas provided that they are in integral feature of the building design. There must be a complementary façade cover hiding mechanical penthouses. All mechanical penthouses, parapets and architectural embellishments must be approved by the Jersey City Planning Staff. Maximum heights shall be calculated as the vertical distance of a structure measured from the average elevation of the finished grade surrounding the structure to the highest point of the structure, excluding mechanical penthouses, parapets, or architectural embellishments.

The minimum height for any new building in the Area is 65 feet.

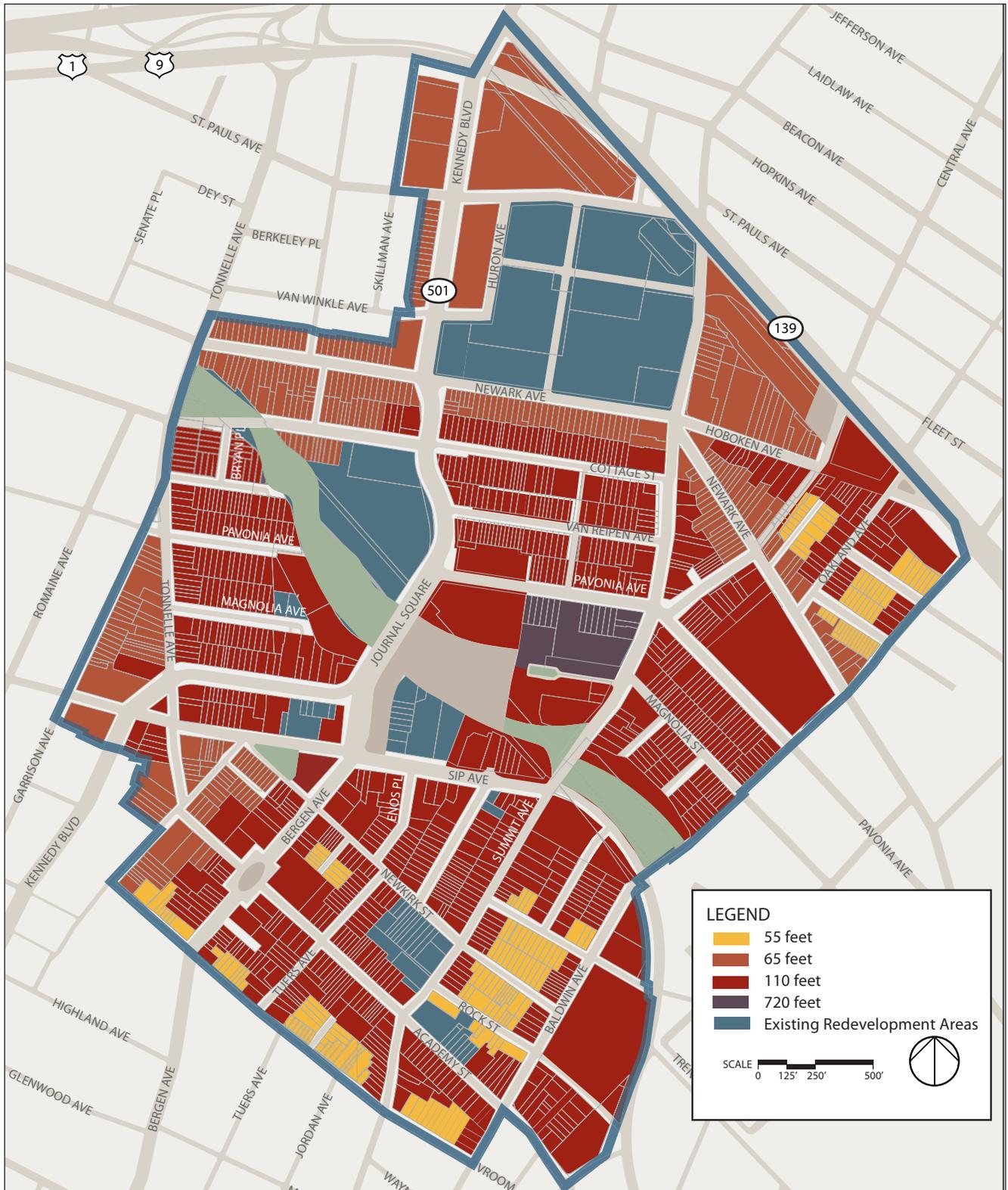
Currently there are four height categories allowed for the Existing Zoning Code. They range from a low of 55 feet to a high of 720 feet.

The Bonus Heights Plan illustrates the maximum heights possible under the District Improvement Bonus. The Bonus Heights Plan allows an increase in height up to 1000 feet on the site adjacent to the PATH/Bus terminal. Building heights decrease the farther they are located from the center of the Journal Square.

The Transit Heights Overlay Plan allows increases in heights to the Bonus Height Plan when transit is approved and funded along the Bergen Arches, as illustrated in Section 4 – Transportation. There are two areas that receive increased height bonuses, both at proposed future transit stations.



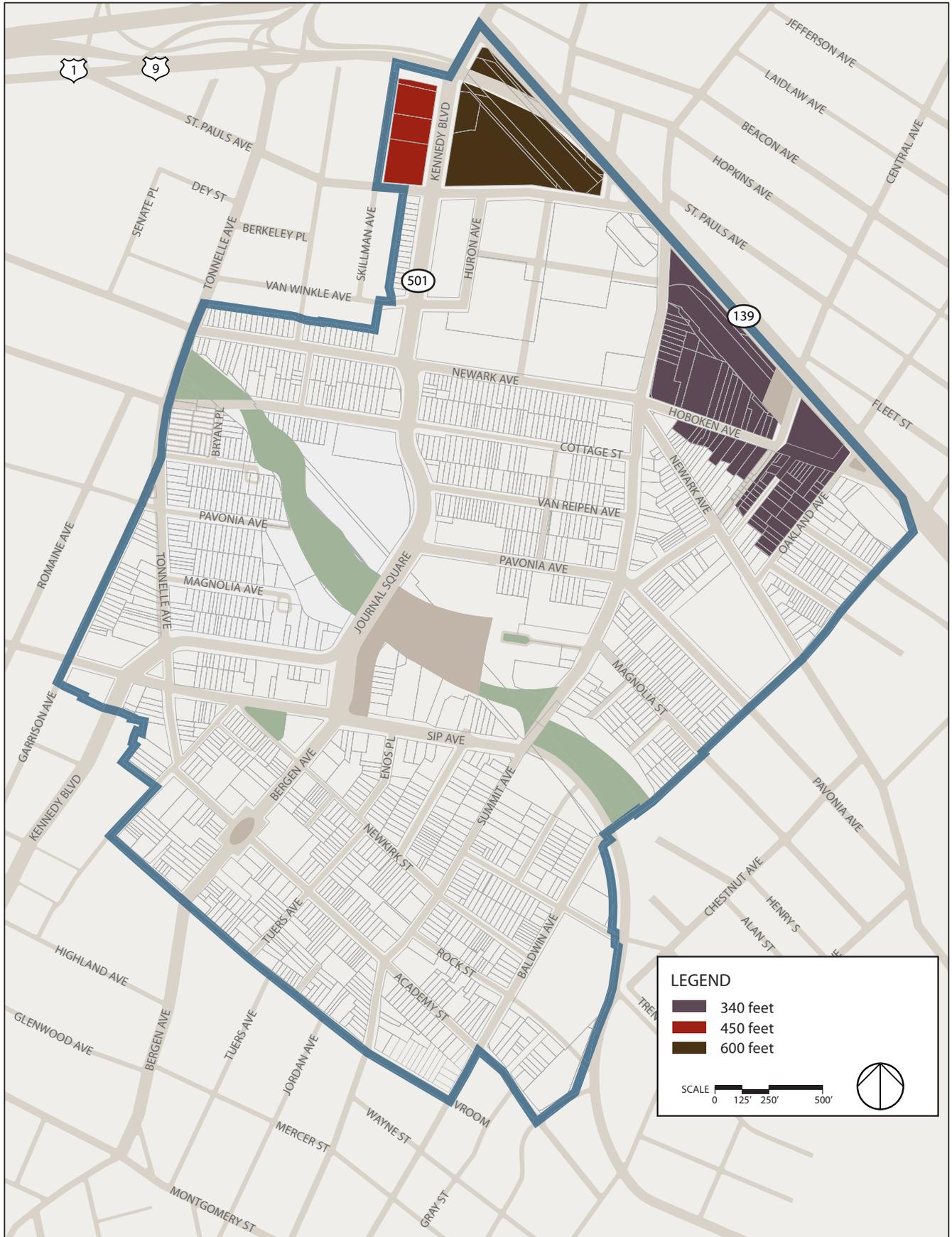
EXHIBIT 12 - BASE HEIGHT PLAN



The developer of Lots A2, B, and C2 is Block 586.5 ("air rights parcels") is entitled to develop these lots consistent with its vested rights under the previous Journal Square Redevelopment Plan. However, in the event that the developer of the air rights parcels seeks to develop under the base zoning standards, and the developer, in order to promote the intent and goals of this Plan, restricts the planned building footprint(s) to the areas on the north of the site, as shown and defined herein in Appendix C, then there shall not be any height limitations on the exceeded. It is understood that the green area covering these lots on this page does not restrict the vested base Height from the Existing Redevelopment Plan in that green area.



EXHIBIT 14 - TRANSIT HEIGHTS OVERLAY PLAN



LEGEND

- 340 feet
- 450 feet
- 600 feet

SCALE 0 125' 250' 500'

Redevelop Journal Square

Section 4 Transportation



The Transportation Section of the Journal Square Redevelopment Plan sets forth the standards and recommendations regarding mobility within the Rehabilitation Area. These regulations deal with vehicular circulation, transit, bicycle travel, the pedestrian realm and loading and disposal. Two closely related topics, streets and parking, are discussed in Sections 5 and 6 of this document. These three sections should be read together in order to gain a comprehensive understanding of the functional and aesthetic mobility standards of the Redevelopment Plan.

Exhibit E – Regional Transportation Connections provides an overview of the regional transportation network in the vicinity of the Area. The existing street network within the Area is automobile oriented, often to the detriment of the pedestrian environment. Major vehicular access is provided to the Area by US Routes 1 & 9, NJ Route 139, Hudson County Route 501, and Route 78 via Montgomery Avenue.

The region is well served by rail transit that includes NJ Transit, the Hudson Bergen Light Rail, and PATH. Journal Square possesses an extensive local and regional commuter bus and rail service network housed at the Journal Square Transportation Center (JSTC). The JSTC is home to the Journal Square station of the PATH and provides connections to critical regional destinations such as Hoboken Terminal, 33rd Street in Midtown Manhattan, the World Trade Center in Lower Manhattan and Newark Penn Station. Aside from the PATH station, Journal Square contains the City’s largest bus terminal and is a major origination and destination point for buses throughout the region. In addition, there is a system of jitney buses that drop off and pickup at the JSTC along with a fleet of taxis.

One of the transportation-related goals of the Redevelopment Plan is to limit the amount of parking provided within the Area and capture as much parking as possible in remote locations before it enters the City and the Journal Square Area. The main strategy for achieving this goal is the development of one or more regional intercept parking facilities. These large parking facilities would have regional highway access and/or existing or future transit access to the City and the Area. The Regional Transportation Connections Map highlights six potential locations for these types of facilities. The Redevelopment Plan recommends that a regional intercept facility located at Exit 15X of the NJ Turnpike (near the Frank R. Lautenberg Secaucus Junction Station) would be most beneficial to the effective redevelopment of Journal Square. This location already has excellent highway access and can potentially be linked to Journal Square and the Waterfront via an extension of the Hudson Bergen Light Rail or a shuttle bus service until such time as the HBLR is extended. Such an extension could branch off the existing HBLR system near the Pavonia-Newport Station and be aligned along either the Bergen Arches or the Embankment before cutting across the northern portion of the Redevelopment Area and continuing to Secaucus Junction. A more detailed description of parking standards for the Redevelopment Area is contained in Section 5 – Parking.



EXHIBIT E - REGIONAL TRANSPORTATION CONNECTIONS





For the purpose of thorough communication, redundancies may exist in the text. If there are any conflicts between these Transportation Regulations and information contained elsewhere in this section, these Transportation Regulations will take precedence.

VEHICULAR

- A. Exhibit 15 – The Vehicular Circulation Plan regulates the direction of traffic flow on streets within the Area.
- B. For the safety of pedestrians and cyclists, no right turns on red lights shall be permitted.
- C. A system of computer timed and controlled traffic signals must be installed at intersections of primary and secondary streets in order to insure orderly traffic flow and speed.

TRANSIT

- A. The Hudson Bergen Light Rail should be extended to provide service to the Journal Square Rehabilitation Area as described in Section 4.4 – Transit Plan. Specific alignment(s) for any extension of the Hudson Bergen Light Rail shall be coordinated with the City, property owner(s), and New Jersey Transit.
- B. A Streetcar line using a dedicated right-of-way and separated tracks shall run along Kennedy Boulevard and Bergen Avenue from Route 139 to McGinley Square/Montgomery Street.
- C. The Streetcar right-of-way must be used by a dedicated bus line until such time as the Streetcar is installed.
- D. Five (5) Streetcar stops are required at the locations indicated on the Rail Transit Plan.
- E. The Journal Square PATH Station must be upgraded to complement the redevelopment of Journal Square Plaza and to complement the station at the World Trade Center in New York City. The station should be day-lit so that natural light reaches the PATH platform and Bus Terminal.
- F. All local bus stops currently (as of December 2008) located at the Journal Square Transportation Center shall be relocated to the local bus plaza on Journal Square.
- G. The existing on-grade bus terminal shall be replaced with a new bus station located under Journal Square Plaza and above the PATH Platform that is accessed from Pavonia Avenue and Sip Avenue.
- H. All coach (regional and long haul) buses currently (as of December 2008) located at the Journal Square Transportation Center shall use the below plaza bus station.
- I. Coach buses are not permitted on designated 'Pedestrian Priority Streets as designated in Section 6 – Streets.
- J. All Streetcar and bus stops must provide seating and protection from the rain. Stops should complement the architectural character of surrounding buildings while being of a unique visual character.
- K. Rail Transit and bus stops must incorporate safety lighting and real-time passenger information systems that update passengers on transit location and arrival times.

BICYCLE NETWORK

- A. Exhibit 17 – Bicycle Plan designates the location of bicycle lanes and paths within the Rehabilitation Area. The placement and character of bicycle lanes shall be regulated by Section 6 – Streets.
- B. All paving for bicycle lanes must be striped and composed of brightly colored asphalt.
- C. Bicycle lanes and paths must have striping and colored paving that extends through every intersection in order to alert drivers to direct cyclists and alert drivers to their presence.
- D. Bicycle wayfinding signage shall include: identification, routes, and crossings for bicycle lanes and pathways.
- E. Bicycle parking facilities must be provided at locations designated on Exhibit 17 -- Bicycle Plan.
- F. Indoor bicycle parking must be provided for 5% of projected workers and 15% of projected residential tenants within any building.
- G. Each commercial building over 100,000 square feet or 300 employees shall provide showers and changing areas for its tenants.



PEDESTRIAN REALM

- A. Exhibit 18, the Pedestrian Realm Plan designates the widths of sidewalks along every street.
- B. Sidewalk areas must be provided along all streets and shall be sized according to the standards presented in the Pedestrian Realm Plan and for each street typology in Section 6 – Streets.
- C. The minimum sidewalk width, inclusive of curbside tree planting area, shall be twenty (20) feet along Primary Streets, a minimum of fifteen (15) feet along Secondary Streets, and a minimum of twelve (12) feet along Tertiary Streets. Section 6 – Streets describes specific sidewalk standards for each of these street types.
- D. Sidewalk areas shall be attractively landscaped and durably paved in conformance with minimum City standards. Sidewalks shall be provided with adequate pedestrian-scaled lighting and contain decorative texture and pavement patterns.
- E. Pedestrian signalization shall be installed as determined to be necessary by the Planning Board and Engineering Department of Jersey City, based on generally accepted traffic engineering standards and/or as approved by the New Jersey Department of Transportation.
- F. Traffic signage shall be consolidated and affixed onto lampposts and traffic signal posts to the maximum extent practical so as to reduce the number of poles, obstructions and visual clutter that impede pedestrian movement and detract from the streetscape.
- G. All signal and light posts must be a consistent dark color.
- H. All sidewalks and intersections must be ADA compliant.
- I. Textured crosswalks are required at each intersection.
- J. The paving must be a continuous texture along Pedestrian Priority Streets and the Journal Square Plaza.

LOADING AND DISPOSAL

For the good of the community and for the Plan to be successful it is necessary to restrict the locations of loading areas and docks, garbage and recycling facilities. The following standards apply:

- A. Each building shall be designed so as to accommodate easy, safe, and sanitary access to garbage dumpsters and recyclable containers by building tenants and carters.
- B. All loading areas shall be provided for in the interior of the structure either above or below grade and accessed through a 24' wide two-story access space leading to the service area. Access shall be from secondary streets only and parking or loading functions shall be on ground level.
- C. Developers shall demonstrate that sufficient off-street loading will be provided to meet the needs of the proposed use. Loading operations shall be conducted so as to minimize conflicts with traffic circulation. All off street loading areas must be able to be closed with door(s) where vehicles enter that complement the architecture of the structure it is serving.
- D. Off-street loading docks shall be provided for buildings with uses that require large truck deliveries e.g. food stores, hardware stores, etc. At a minimum, buildings shall have one loading dock. The number of loading docks required will be based on review by the Planning Board.
- E. Specific locations on each block must be designated as loading and package drop-off zones. These locations shall be identified with appropriate signage, ground texture and/or striping. The minimum length of these zones shall be 30 feet. On-street parking will not be permitted in these zones.
- F. On-street loading shall be limited to light deliveries that support the day-to-day functioning of the building with which they are associated. Specific on-street loading areas must be designated at one per block.



Exhibit 15 – The Vehicular Circulation Plan regulates the direction of traffic flow within the Rehabilitation Area and establishes the hierarchy of streets in Journal Square that includes Principal Arterials, Primary Streets, Secondary Streets, Tertiary Streets, and Pedestrian Priority Streets. This street network is designed to accommodate varying levels of vehicular traffic within the Area. In general, Primary Streets are designed to move larger amounts of traffic more efficiently than Secondary and Tertiary Streets. The Vehicular Circulation Plan manages the projected traffic and circulation needs of the Journal Square Rehabilitation Area by distributing vehicles evenly and appropriately on roads throughout the Area.

Major highway access to the Rehabilitation Area is provided by US Routes 1 & 9 and NJ Route 139 and Route 78. The Vehicular Circulation Plan designates several streets as Primary Streets: Tonnelle Avenue, Sip Avenue, St. Paul's Avenue, and portions of John F. Kennedy Boulevard, Summit Avenue, and Pavonia Avenue. Secondary Streets include critical streets such as Newark Avenue, Hoboken Avenue and portions of Bergen Avenue and Summit Avenue. These streets serve to link Tertiary Streets, the smallest streets within the Area, Primary Streets and regional highways. Standards and recommendations governing each of these street types are presented in Section 6 – Streets.

Where possible, streets carry two-way traffic. However, one-way streets are recommended in specific locations where limited right-of-way or other constraints make two-way traffic impossible. One-way streets are identified with arrows indicating the direction of their flows. Standards for each street type can be found in Section 6 – Streets.

The Vehicular Circulation Plan identifies the location of a Pedestrian Priority Zone comprised of Journal Square, John F. Kennedy Boulevard from Tonnelle Avenue to Journal Square and Bergen Avenue from Sip Avenue to Journal Square. Local buses, taxis and streetcars shall be permitted to travel through the Pedestrian zone; however personal automobiles will be restricted. This plan will be realized through phases over multiple years. The Pedestrian Priority Zone includes a unique pedestrian retail street along sections of Kennedy Boulevard while also providing a safe pedestrian connection between Journal Square Plaza and City Center Park. This pedestrian retail corridor will be enhanced by wide sidewalks and streetscaping, safe and easy street crossings and convenient access to mass transit. The establishment of this zone allows an on-street bus plaza to be restored to Journal Square, as it was before the construction of the existing bus depot.

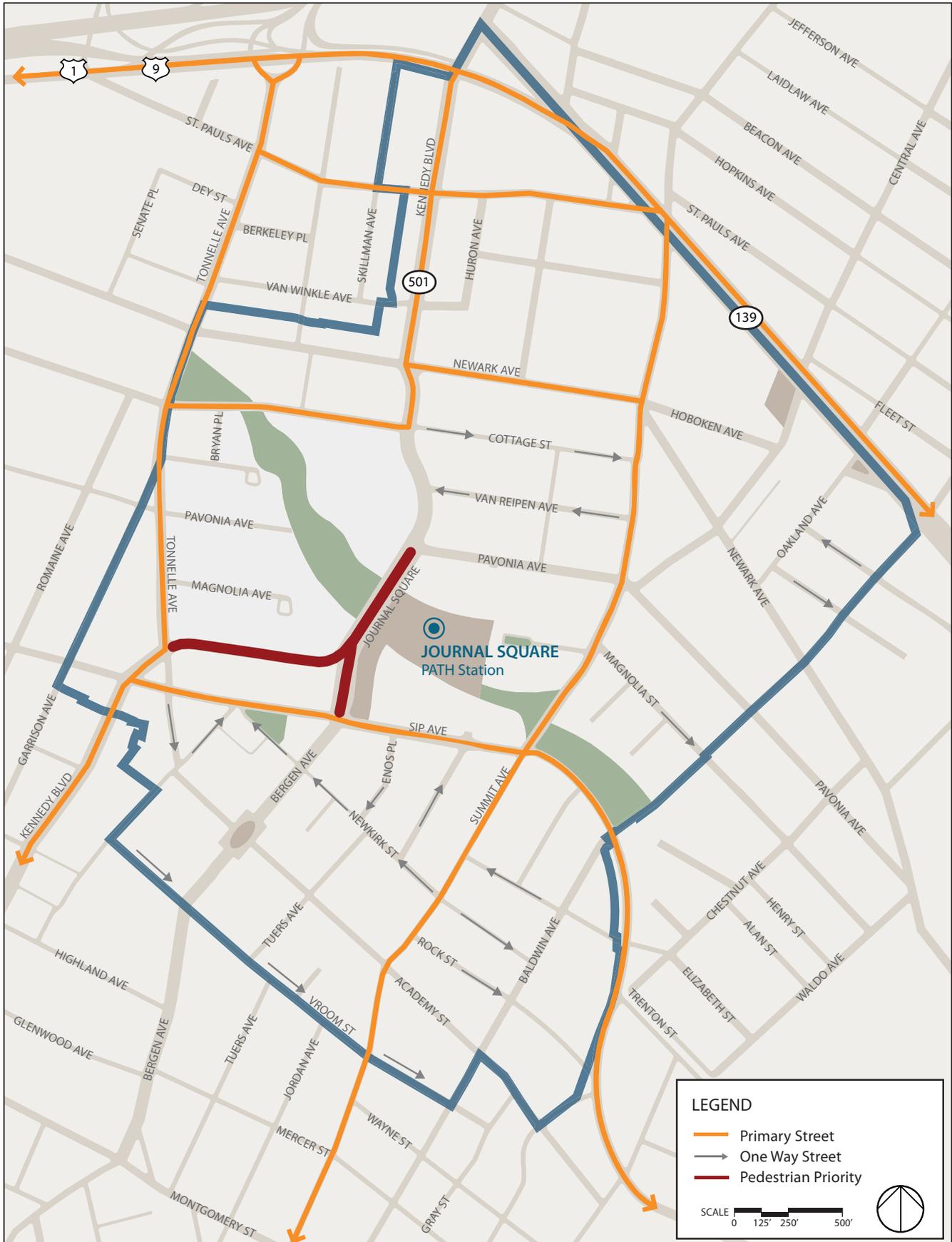
The plan recommends widening of Tonnelle Avenue with new connections from Tonnelle Avenue to Kennedy Boulevard via Cottage Street, Newark Avenue, St. Paul's Avenue and Routes 1, 9, and 139. The new proposed St. Paul's Viaduct Interchange is required for the future circulation and Parking Plan.

Several aspects of the Redevelopment Plan ensure that the creation of a Pedestrian Priority Zone will not have an adverse impact on the accessibility of Journal Square and circulation within the Area. Primary Streets are strategically placed outside of the Pedestrian Priority Zone to ensure that vehicle accessibility is maintained throughout the Rehabilitation Area.

The Plan recommends an extensive transit network that adds a streetcar system and an extension of the Hudson Bergen Light Rail to an already robust bus infrastructure. Furthermore, a regional parking strategy seeks to limit the number of cars that enter the City while a comprehensive approach to creating an efficient and appealing pedestrian realm will make walking even more convenient than it already is.



EXHIBIT 15 - VEHICULAR CIRCULATION PLAN



Transit has played a critical role in the historical development of Journal Square and it will continue to have a large role in its redevelopment. The Journal Square Redevelopment Plan requires significant upgrades to both the PATH station and bus terminal designed to capitalize on the existing transit infrastructure. The Plan also recommends transit initiatives designed to enhance the accessibility of the Journal Square and lessen dependence on personal automobiles. These initiatives include an extension of the Hudson Bergen Light Rail, the creation of a new Streetcar system along John F. Kennedy Boulevard and Bergen Avenue and the possible addition of commuter rail that would link Journal Square directly to Hoboken Terminal.

LIGHT RAIL

The Redevelopment Plan proposes that the Hudson Bergen Light Rail be extended from downtown Jersey City to the Journal Square Rehabilitation Area. Two potential alignments are identified on Exhibit 16 -- Rail Transit Plan. The first alignment (depicted alongside Route 139) would utilize the Bergen Arches to connect downtown to a Remote Intercept Parking Facility located near Exit 15X of the NJ Turnpike. This extension of the HBLR would contain two stops closely linked to the Area. A stop located near the intersection of Kennedy Boulevard and Route 139 would be within an approximately 12 minute walk of the PATH Station. Travelers in this location would also have the option of utilizing the proposed streetcar system that would link the Light Rail stop to Journal Square and destinations along Bergen Avenue.

A second light rail stop near the intersection of Central Avenue and Route 139 would be within a 3 minute walk of the Hudson County Court Complex and an 8 minute walk from the PATH Station. The proposed light rail stations will each be incorporated into new public "transit" plazas. Each of these stops will be linked to a local intercept parking facility that will provide parking for visitors and residents of Journal Square. These stops will serve as gateway elements to the northern entrances of Journal Square.

The second light rail alignment can create a direct connection from downtown Jersey City to the Journal Square PATH Station by utilizing an abandoned rail line adjacent to the existing PATH tracks. Such an alignment could contain stops located at Journal Square and to the southeast of the of the Rehabilitation Area near a Local Intercept Parking Facility situated near Waldo Avenue.

STREETCAR

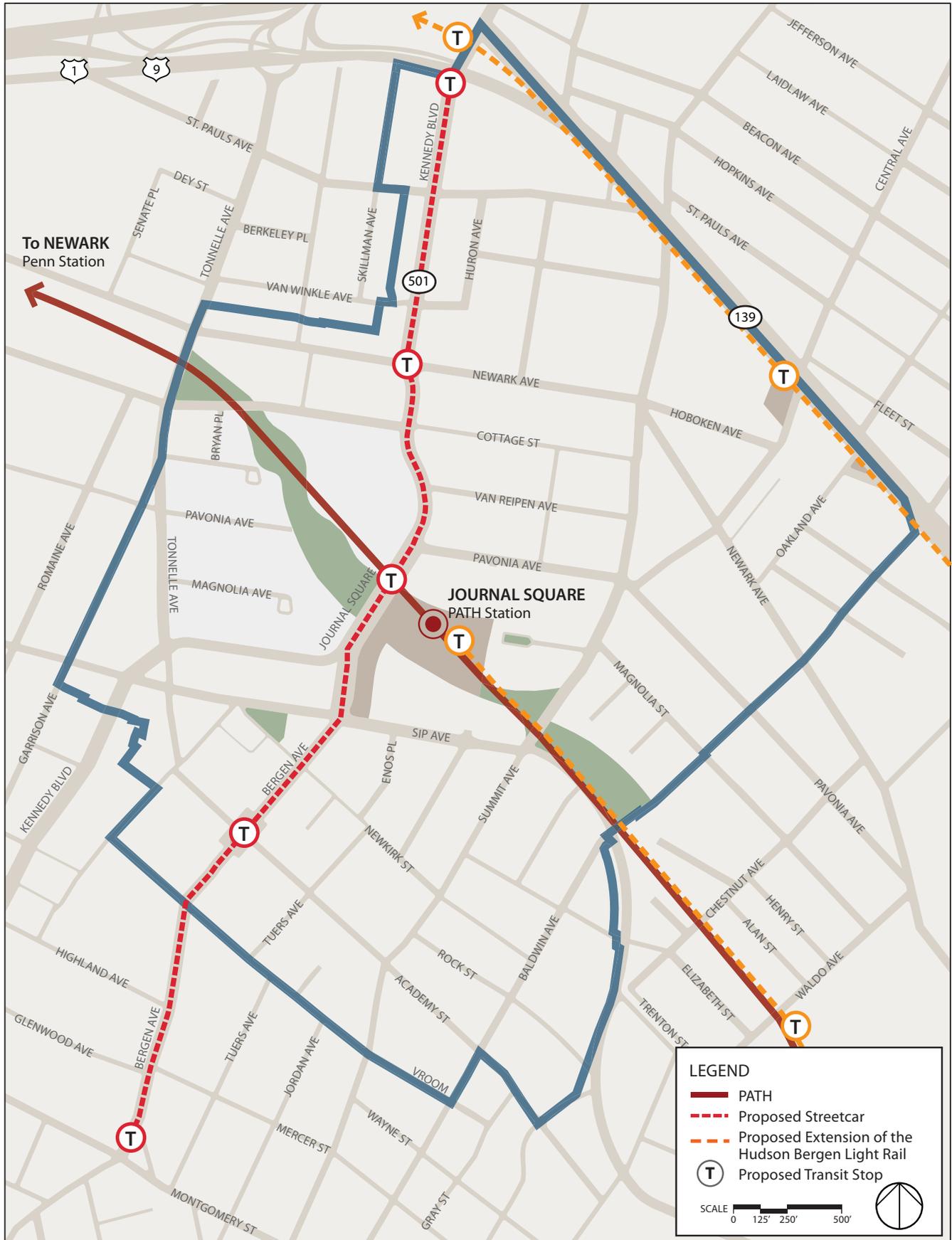
The Plan proposes a narrow-gauge streetcar line that runs along Kennedy Boulevard, Journal Square and Bergen Avenue. This streetcar system is critical north/south component of a comprehensive transit network designed to service Journal Square and surrounding neighborhoods. A streetcar system will ensure that new and existing developments along Kennedy Boulevard and Bergen Avenue have convenient access to the Journal Square Transportation Center and proposed extensions of the Hudson Bergen Light Rail.

The Redevelopment Plan recommends that a streetcar run from the intersection of John F. Kennedy Boulevard and Route 139 to Bergen Avenue at McGinley Square. Five (5) stops are anticipated along this route. Each of these stops has a primary service area defined by a 3-5 minute walk. The streetcar will have a dedicated lane within the vehicular right-of-way as defined by Section 6 – Streets.

During the development of a streetcar system, a dedicated bus lane could be implemented along the streetcar right-of-way with stops at the locations designated in the Rail Transit Plan. All stops should be permanent and substantial in nature. A transitional bus along the streetcar route can help establish a riding habit among Journal Square residents and encourage transit appropriate development along the eventual streetcar corridor. The streetcar may be extended in the future to service additional neighborhoods.



EXHIBIT 16 - RAIL TRANSIT PLAN



PATH AND HEAVY RAIL

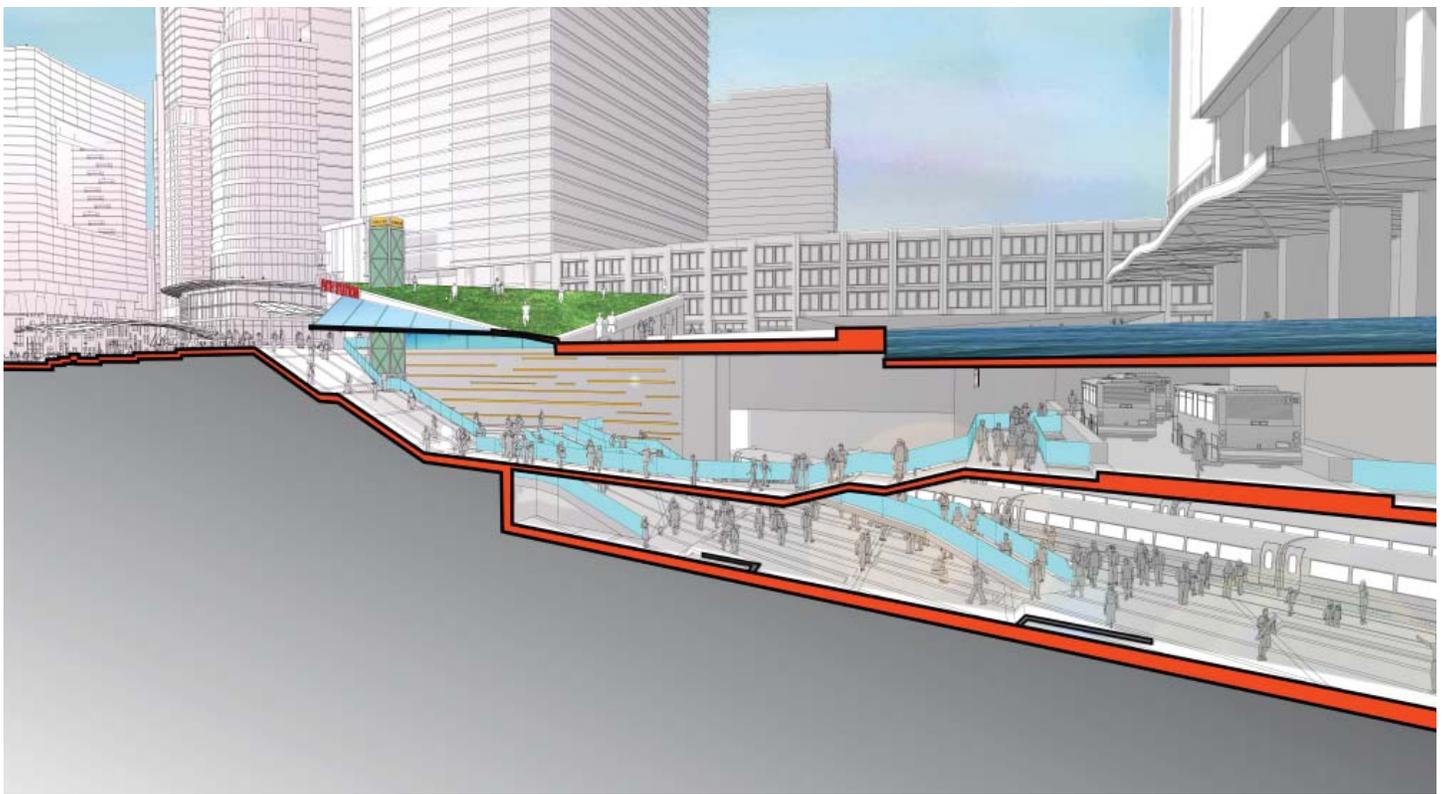
A reconfigured Journal Square Transportation Center will combine access to both the Journal Square PATH Station and the Journal Square Bus Terminal. The co-location of these transit stations will need to be coordinated with the redevelopment of Journal Square Plaza. The entrance to the Transportation Center shall be located on the Plaza and provides the opportunity for an iconic entrance way which can serve as a symbol of Journal Square's redevelopment.

The Redevelopment Plan recommends that the possibility of a direct rail connection between Journal Square and Hoboken be investigated. Rail lines from Hoboken Terminal currently emerge just north of the Area near the intersection of Kennedy Boulevard and Route 139. A rail stop in this location could capitalize on the placement of proposed streetcar system and/or extension of the Hudson Bergen Light Rail.

BUS

In Journal Square's heyday, the center of the Square was enlivened by the activity of buses discharging and picking up passengers. The Redevelopment Plan proposes bringing local buses back to the center of Journal Square Plaza. It is required that all local buses move their stops from the Journal Square Transportation Center to the front of the Plaza on Journal Square. This Plaza would restore the activity brought by bus transportation to the heart of Journal Square as well as facilitate easier pedestrian crossings.

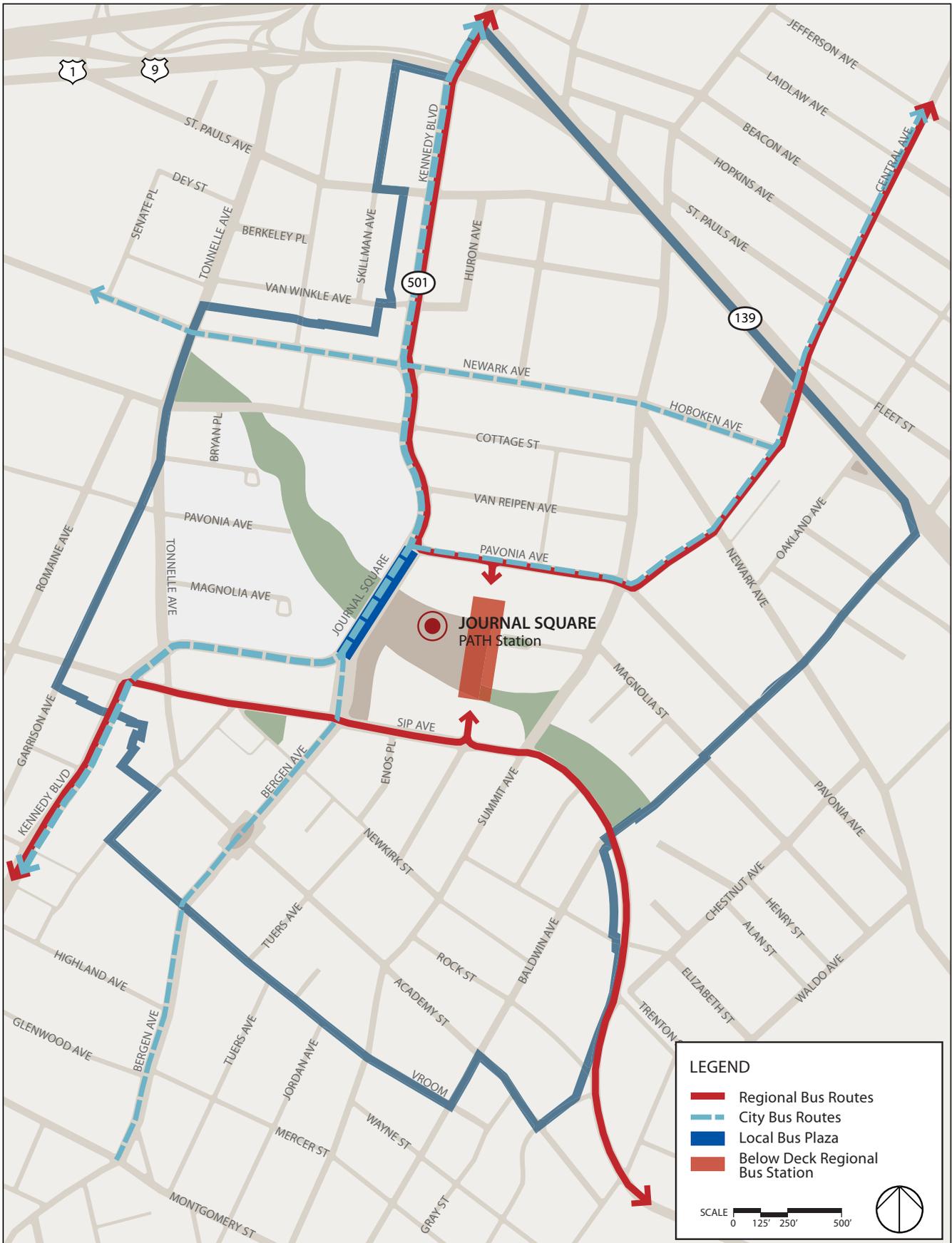
All regional buses would be relocated to a new bus station located under the new Journal Square Plaza. The station should be partially day-lit by a glass bottom reflecting pool in the center of the new Plaza. All current regional bus movements and routes would remain. The new regional bus station would have six lanes and have access for buses off of Pavonia Avenue and Sip Avenue. The design of the multi-modal station would facilitate easy movement between the PATH and buses. Recommended local and regional bus routes and station access is detailed in Exhibit F Bus Plan.



A diagram of a reconfigured Journal Square Transportation Center. This multimodal station combines the Journal Square PATH Station with newly designed bus terminal. Access for each of these services would be shared and entered from Journal Square Plaza.



EXHIBIT F - BUS PLAN



If all drivers in the USA used a bike instead of a car for just 0.5% of the driving they do, the US would save 462 million gallons of gas a year. Bicycling also adds to the sustainability of urban areas by reducing air pollutants. For these reasons, the Bicycle Plan is integral to the success of the overall Redevelopment Plan. The Bicycle Plan is designed to encourage cycling along the cartway in designated lanes. Cycling should be an easy, safe, and fast journey to and from home, retail shops and offices, transit stations, institutional and civic places, adjacent neighborhoods, and along the parks and plazas. As a sustainable urban city center, the Plan emphasizes a greater reliance on walking and cycling as a form of transportation.

Exhibit 17 – Bicycle Plan illustrates the recommended bicycle network, location of pedestrian priority streets and the placement of bicycle storage facilities. The Bicycle Plan establishes on an interconnected network of bicycle lanes and paths and illustrates major crossing points. The Bicycle Plan features two (2) types of bicycle lanes and paths: striped on-street bicycle lanes and multi-use paths through City Center Park and Journal Square Plaza.

Cycling is permitted on pedestrian priority streets within bus right-of-ways. Bicycles shall not be permitted to travel within the streetcar right-of-way.

Each bicycle lane is well demarcated and wide enough to accommodate at least one cyclist comfortably safe from traffic. The specific placement and characteristics of each bicycle lane is regulated in Section 6 – Streets.



EXHIBIT 17 - BICYCLE PLAN





Successful sustainable urban neighborhoods encourage large numbers of people to walk on their sidewalks. The Pedestrian Realm Plan is a network of sidewalks, crosswalks, pathways and a dedicated street network designed to maximize pedestrian activity. The Plan is designed to encourage walking to retail shops and offices, to adjacent and integrated recreational, institutional uses, to multiple transit opportunities, and to home. The sidewalks must be safe and pleasant. The landscape treatment of the pedestrian realm requires street trees, bollards, pedestrian scaled lighting, and texturing. In addition, textured crosswalks shall be placed at each intersection and where streets cross the greenway.

Exhibit 18 -- Pedestrian Realm Plan regulates the sidewalk widths along each block within the Journal Square Rehabilitation Area. Sidewalk width is influenced by both the adjacent land use and cartway width. In general, wider cartways are abutted by wider sidewalks. Wide sidewalks provide the room necessary to accommodate the amenities that make pedestrians feel safe and comfortable when walking such as street trees, bollards and street furniture. Specific sidewalk standards for individual streets are described in Section 6 – Streets.

The Journal Square Plaza, located above the Journal Square PATH Station and Bus Terminal, is the epicenter of pedestrian activity within the Rehabilitation Area. The Plaza will be lined with retail on all edges and is the largest paved pedestrian area in the Area. The Plaza shall be landscaped and attractively paved and should include an iconic entrance to the transit stations below, sitting areas, and a glass bottom reflecting pool that allows daylight to reach the underground bus and PATH platforms. See Section 7 – Landscape for more details on Journal Square Plaza.

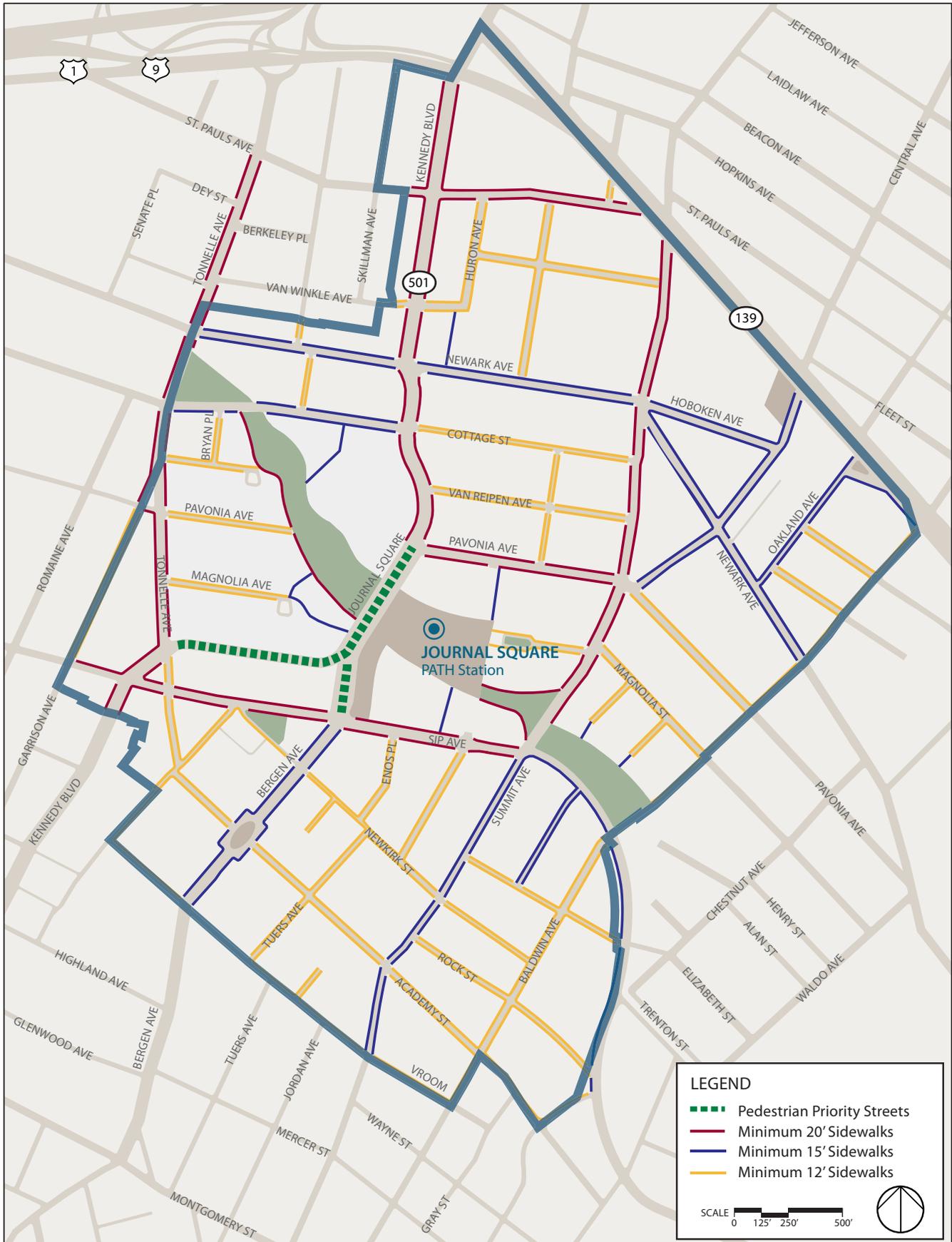
The Pedestrian Priority Streets radiate from Journal Square Plaza and are a key element of the Pedestrian Realm Plan. The Pedestrian Priority Streets include Journal Square, Kennedy Boulevard from Tonnelle Avenue to Journal Square and Bergen Avenue from Sip Avenue to Journal Square. The Pedestrian Priority Streets connect the Journal Square Transportation Center with the Kennedy Boulevard retail district thereby providing direct pedestrian access to Journal Square’s major shopping area. The Pedestrian Priority Streets are continuously paved which will have an open-air interactive retail edge which can include arcades, large awnings and windows, interesting signs and outdoor cafes.

The City Center Park is another critical piece of the Journal Square pedestrian network. The Park is comprised of two separate green spaces on either side of Journal Square Plaza that are to be built on the platform that will span the existing rail cuts. The first piece of the park starts at the Tonnele Avenue bridge and extends to the Pedestrian Priority Street at Journal Square. The second portion of the Park begins at the southeast corner of the Plaza and extends to the Baldwin Avenue edge of the Rehabilitation Area. The Center City Park is important to the redevelopment of Journal Square because it provides recreation opportunities and enhances the value and viability of new development opportunities created by decking over the railroad cuts.

Over time, City Center Park is envisioned as becoming a significant citywide greenway that provides pedestrian and bicycle connections to the waterfront and Hudson River walkway. Such a greenway could utilize the 6th Street Embankment as a connection between Journal Square and the waterfront.



EXHIBIT 18 - PEDESTRIAN REALM PLAN



Redevelop Journal Square

Section 5 Parking and Utilities



Parking will be one of the most important factors in the future of Journal Square. A detailed parking study was conducted in the Spring of 2008 by A. Nelessen Associates and Dean Marchetto Architects as part of the Journal Square planning process. The Journal Square Parking Report summarized these findings.

The Journal Square Rehabilitation Area currently contains approximately 9,843 parking spaces that are housed in a combination of off- and on-street parking facilities. Off-street facilities are owned by the City of Jersey City as well as private companies. These facilities are a mix of parking structures and surface lots primarily located near the Transportation Center and nearby office buildings and institutional uses. On-street parking consists of spaces that are metered or unmetered with provisions for residential permit parking. Parking facilities serve a variety of users with the largest contingents being commuters utilizing area transit options, employees of nearby offices, and visitors to local shops and services.

Approximately 49 acres, or 20%, of the Rehabilitation Area are dedicated to parking. In general, the largest structures are located near the Transportation Center and adjacent to major office centers and institutions. Smaller surface lots are scattered throughout the Area and cater to a variety of Journal Square residents and visitors. Most of these smaller lots are undeveloped paved parcels that front onto the street with some smaller areas for parking located in the center of blocks behind buildings.

In addition to 49 acres of off-street parking, there are approximately 1373 on-street parking spots in the Area. These spots are both metered and unmetered and follow a range of regulations based on City ordinances and enforced by the Jersey City Parking Authority. Most metered spaces are located along retail frontage and most on street parking in residential areas is unmetered.

A large majority of the total parking capacity in the Rehabilitation Area is contained in off-street facilities. The Off-Street Parking Inventory Map on the next page highlights these facilities. Off street parking is provided in a variety of public and private structures and lots throughout the Journal Square Rehabilitation Area. Slightly more than half of all off-street parking is open to the public. Furthermore, the majority of off-street public parking is provided by parking structures whereas the majority of private parking is accommodated by surface lots.

Private facilities include permit parking areas and any facility classified as office, commercial, educational, governmental, religious, or residential parking that is not accessible to the general public. Most of these private facilities are surface lots although four private parking areas are classified as structures. Of these, two are structures associated with the State of New Jersey office building and Hudson County Administration building respectively. One small private structure is associated with the high-rise apartments located along Summit Avenue with the final structure integrated into the Social Security Administration building at the corner of Newark Avenue and John F. Kennedy Boulevard. Approximately 13% of the total private parking capacity is accommodated in structures and the remaining 86% is surface. Private surface lots provide parking for a variety of uses and are evenly dispersed throughout the entire Rehabilitation Area.

Four of the five public parking structures in the Area are located around the Journal Square Transportation Center. These decks are heavily used by commuters utilizing the PATH and Port Authority Bus transit connections. These four decks make up 27% of the total parking capacity in the Area and 62% of the off street public parking. The remaining 38% of the off street public parking is located in surface lots spread throughout the Area and one additional public structure located off of Bergen Ave.

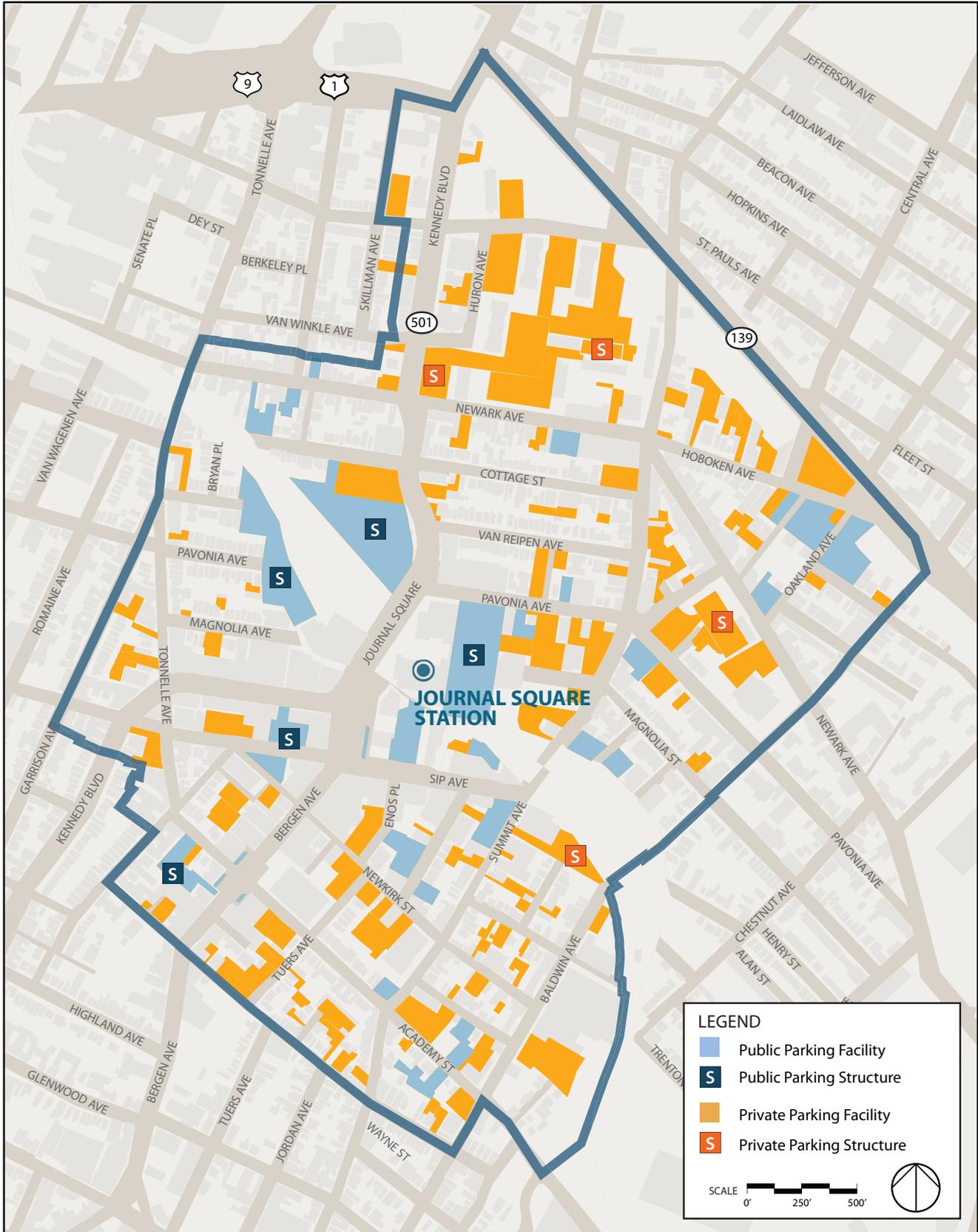
Hourly parking rates for off street parking vary wildly throughout the Area. Daily rates vary from \$7.00 - \$17.00. Monthly rates range from \$95.00 to \$190.00.

<u>TYPE OF PARKING</u>	<u>SPACES</u>	<u>PERCENT</u>
<i>On-Street Unmetered</i>	1,068	10.85%
<i>On-Street Metered</i>	305	3.10%
Total On-Street Parking Spaces*	1,373	13.95%
<i>Off-Street Public</i>		
Surface Lots	1,493	15.17%
Structures	2,813	28.58%
Subtotal	4,306	43.75%
<i>Off-Street Private</i>		
Surface Lots	3,662	37.20%
Structures	502	5.10%
Subtotal	4,164	42.30%
Total Off-Street Parking Spaces	8,470	86.05%
GRAND TOTAL Parking Spaces	9,843	100.00%

*On-street parking numbers were estimated using both field observation and aerial photography.



EXHIBIT G - OFF-STREET PARKING INVENTORY



EXISTING PARKING FACILITIES



GENERAL PARKING STANDARDS

A. Parking for specified uses within the Rehabilitation Area is to be provided on-site of the use or in a remote location.

Within the Rehabilitation Area, the following forms of on-site parking are permitted:

- a. Under Building (below grade) – one or more levels of underground parking connected by ramps
- b. Embedded – Parking structures that are wrapped by the outer edges of a building containing retail, residential or office uses. Wrapping the structure conceals any exterior exposure to parking. Embedded parking is the preferred on-site parking technique. It must be fully ventilated.
- c. Podium – Lower floors within a building where parking extends to the build-to-line. The first and second floor of buildings utilizing podium parking must be dedicated to retail, office, or residential uses. The facades of parking floors must replicate the faced found on upper floors or utilize architectural screening to conceal the parked cars and interior lighting. In general, podium type parking is discouraged.

Within the Rehabilitation Area, the following forms of remote parking are permitted:

- d. Walkable – A mixed-use parking structure within a short walking distance of a development. These neighborhood-scaled structures should be dispersed throughout the Rehabilitation Area to create a network of convention parking facilities within Journal Square.
 - e. Local Intercept – A mixed-use parking structure with a capacity of 2,000 to 5,000 vehicles located on the periphery of the Area that is linked by transit service to neighborhoods or developments in Journal Square.
- B. A Regional Intercept parking facility with a capacity of 5,000 to 10,000 vehicles should be developed at a location with major regional highway access and transit connections to Journal Square and Jersey City. Such a facility can serve as a parking ‘intercept’ that prevents the flow of some automobiles in Downtown and the Redevelopment Area.
- C. The first level of below grade parking for residential structures must be recessed into the ground 6 to 10 feet.
- D. Parking embedded within a building shall be located on the first floor through the sixth floor. On these floors, the parking shall be surrounded by a liner building using a single loaded corridor. Above parking levels, residential uses should use a double loaded corridor circulation system over the roof of the parking structure.
- E. Embedded parking levels must have a minimum height clearance of 8 feet from the finished floor to accommodate typical vehicles. The first level of embedded parking must have a minimum height clearance of 11 feet to accommodate service vehicles.
- F. Off-Street Parking spaces shall be a minimum of 8.5 by 18 feet (8 by 16 feet for compact cars and 8 by 8 feet for ultracompact cars) and aisles shall be between 22 and 24 feet wide. Curb cuts made for access to parking structures or under building parking shall be a maximum of 22 feet in width.
- G. Valet parking may be allowed with the approval of the Jersey City Planning Board, if it can be demonstrated that a safe and convenient means of operation can be provided.

PARKING RATIOS

- A. The amount and design of parking shall be in accordance with the following maximum requirements:
1. Offices, financial institutions, brokerage houses, governmental uses and colleges and universities: 0.5 spaces for each 1,000 square feet of gross floor area
 2. Retail sales of goods and services, restaurants, bars, theaters and night clubs: 0.5 spaces for each 1,000 square feet of gross floor area, excluding the first 5,000 square feet of gross floor area
 3. Residential: 0.5 spaces per unit
 4. Hotels: 0.5 spaces per 1,000 square feet of gross floor area including all-purpose rooms such as banquet, meeting and conference rooms
- B. Parking for specific uses can be provided using on-site and remote parking facilities. Developers seeking to provide additional parking, above and beyond the maximums listed above, may purchase or lease parking capacity from one or more walkable, local intercept, regional intercept facilities.



ON-STREET PARKING

- A. On-street parking spaces shall not be attached to any specific use or fulfill any specific parking requirement, but shall be used for guest and short term retail and office parking.
- B. All on-street parking shall be priced and metered at market rates to allow for a 10% vacancy rate at any time.
- C. Two spaces per block must be set aside for loading and delivery services. These spaces should be specifically designated with special paving textures and signage.

PARKING STRUCTURE REQUIREMENTS

- A. All parking structures, except for regional remote facilities, must be mixed-use. Where parking structures front on a public right-of-way, park, or plaza. The structure shall contain commercial or mixed-uses on the ground floor. Where appropriate, second story commercial uses are recommended.
- B. On all facades, parking structures shall be designed to complement the architectural style, building scale, mass, building materials and colors of the principal or historic buildings in the adjacent area. Parking structures shall be indistinguishable from residential, commercial, or mixed-use buildings in the use of solids and voids. The scale and mass of parking structures shall be mitigated through wall offsets, openings and other distinctive design elements. Window openings shall be vertical or square in proportion. No long horizontal openings shall be permitted.
- C. Parking structures shall not express sloping floor decks in the facades of the structure. Architectural elements in the façade shall be expressed as horizontal and vertical architectural features and lines similar to other types of buildings.
- E. Parking structures shall have pedestrian entrances with direct access by elevator onto a sidewalk.
- F. One vehicle entrance is required for every 400 spaces.
- G. Robotic parking is highly recommended.
- H. All parking façades should be designed to eliminate interior lighting from being visible from the outside of the structure. Various techniques, including double screening, can be used for this purpose.
- G. Parking structures must step back between the 4th and 6th floor and have a distinctive cornice line at the stepback.
- I. Where under building parking exists, the façade of the building must mimic a basement façade with real windows and must be fully ventilated.
- J. Each floor of a parking facility shall be coded for the ease of locating vehicles.
- K. Automated parking is highly recommended.
- L. Green roofs are highly recommended for the tops of parking structures.

BICYCLE PARKING

- A. Bicycle parking shall be provided through the provision of bicycle racks and bike lockers
- B. Bicycle parking shall be provided at all public plazas and parks. Where possible, covered parking for bicycles should be provided. See Exhibit - 18 Bicycle Plan.

SHARED PARKING REQUIREMENTS

Shared parking is parking which is available to more than one building or land use. In general, different types of land uses may create different demand for parking throughout the day. Shared parking is highly encouraged within the Redevelopment Area in order to promote more efficient use of parking facilities. Shared parking arrangements in which a dedicated parking space may counted towards the parking requirement of two or more uses are permitted upon submission of a parking plan by the developer demonstrating that such an alternative parking arrangement is appropriate and approval by the Planning Board of such parking plan. Planning Board approval will be contingent upon:

- A. The applicant's demonstration to the Planning Board's satisfaction that demand for these shared parking spaces by each use, based on the time of day, will not substantially overlap;
- B. No more than 75 percent of the parking spaces counted towards any use are shared spaces. In the absence of extenuating circumstances, office and residential uses shall be deemed non-overlapping uses.

One of the primary recommendations to emerge from the Journal Square visioning process is the need to limit the amount of parking within the Journal Square Rehabilitation Area and capture as much parking as possible in intercept locations before it enters the City. Providing parking at intercept locations can significantly reduce the flow of vehicular traffic through the Area and the City. Along with regional intercept facilities, several types of parking will help achieve the goals of the Redevelopment Plan. These parking types fall into two broad categories: on-site parking and remote parking.

On-site parking refers to in or under building parking in which cars are placed within or underneath the buildings themselves. In building parking includes embedded or podium parking configurations. Under building parking can be located either below grade or on grade. In general, on-site parking should be limited so that more land and building volume can be dedicated to productive uses while also reducing congestion and improving the pedestrian experience. No additional surface parking lots will be permitted within the Rehabilitation Area. Over time, existing surface lots will be candidates for infill development. Development of these currently underutilized, valuable parcels can help revitalize Journal Square by providing housing near job centers and transit, increasing the property-tax base, creating opportunities for mixed-use parking and providing new residents to support shopping districts and services.

Three types of remote parking have been identified as appropriate for Journal Square: regional intercept, local intercept and walkable parking.

1. **Regional Intercept Parking:** Parking facilities that have access to major regional highways and/or existing or future transit links to Journal Square. These facilities have the ability to act as a ‘parking intercept’ that prevents the flow of some automobiles into the downtown or Rehabilitation Area. These types of facilities are a key component of a long term comprehensive parking strategy for Journal Square and the City. The Journal Square visioning process identified a site near Exit 15X of the NJ Turnpike as the most effective location for a regional remote facility. The placement of a remote facility at this location would be contingent upon an extension of the Hudson Bergen Light Rail or the provision of a shuttle bus connecting the facility to Journal Square. The recommended size of a facility of this type is 2,500 – 10,000 spaces.
2. **Local Intercept Parking:** Mixed-use parking structures that are accessible from major roads and linked by transit service to neighborhoods or developments in Journal Square. These facilities are closer to the center of Journal Square than a regional parking intercept facility.

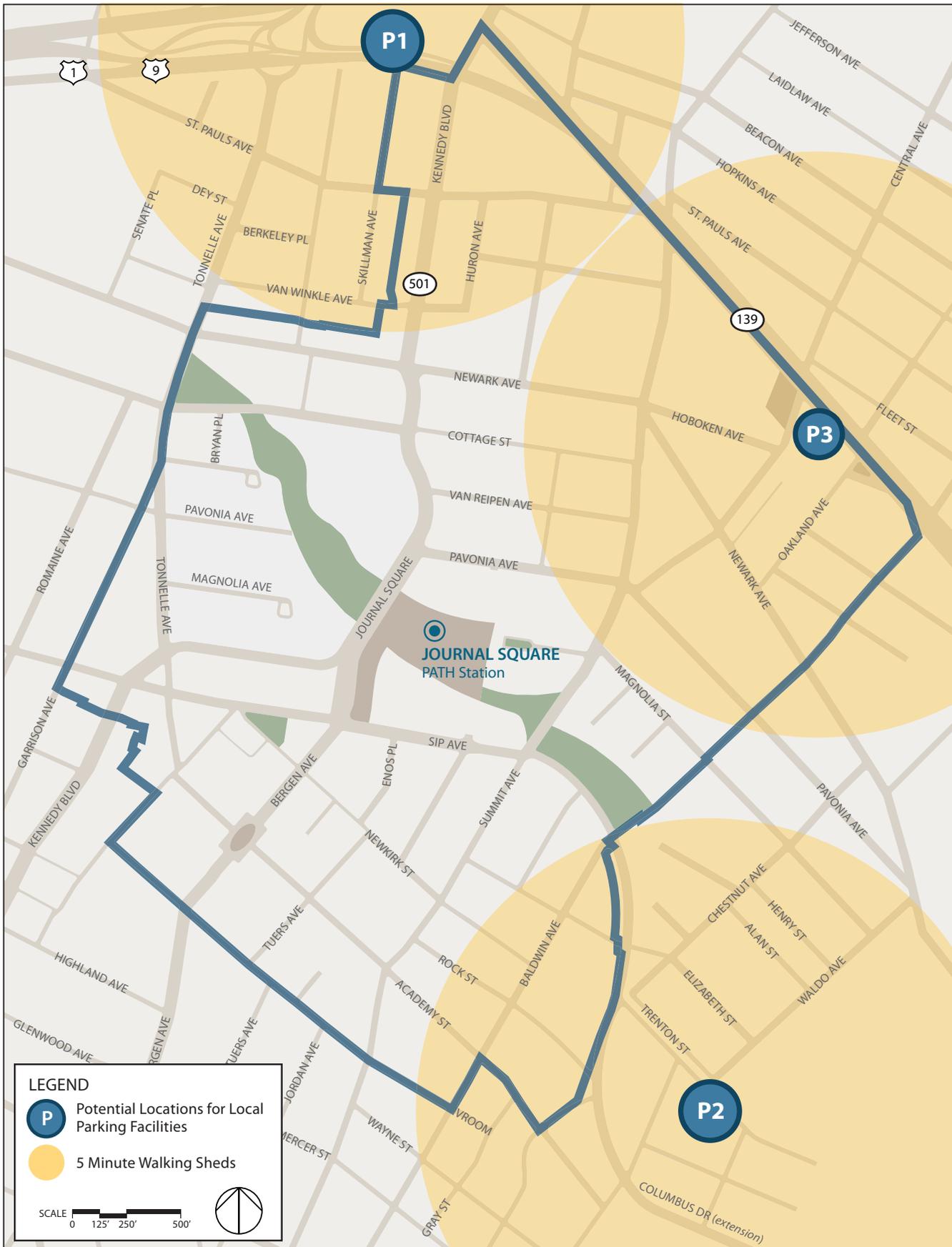
The Local Remote Parking Facilities Plan identifies the recommended location of three local intercept facilities designated as P1 through P3. These locations were partially chosen due to their location along a potential extension of the Hudson Bergen Light Rail as detailed in Section 4.5 – Rail Plan. A ¼ Mile (roughly five minute walk) walking shed is depicted for each site to illustrate its approximate walking service area.

P1 has the potential to be the largest of the local intercept facilities and should be located near the intersection of Route 139 and Routes 1 & 9 to the north of the Area. The construction of a local intercept facility at this location should be coordinated with the proposed St. Paul’s Viaduct Redesign Project and capitalize on any infrastructure investment by NJ Transit. This structure should contain up to 5,000 spaces and be constructed to provide well-designed pedestrian connections to Kennedy Boulevard and direct connections to proposed transit options such as the Kennedy Boulevard streetcar, extension of the Hudson Bergen Light Rail and the possible addition of heavy rail service linked to the Hoboken Terminal. A facility at this location is outside the jurisdiction of the Rehabilitation Area but is critical to the success of Journal Square. The Redevelopment Plan recommends that a special urban design plan be prepared for this area.

A second local intercept facility, P2, is recommended to be located to the southeast of the Rehabilitation Area. One potential site is the abandoned Conrail property near the Columbus Drive ramp that connects to Interstate 78. A facility at this location could be developed in coordination with the construction of a new street that would link Christopher Columbus Drive to Sip Avenue. A facility at this location would have highway access to Interstate 78 and possible transit connections to Journal Square by utilizing an extension of the Hudson Bergen Light Rail or an expansion of PATH service that would make use of the existing maintenance yard stop.



EXHIBIT H - LOCAL INTERCEPT PARKING FACILITIES





A mixed-use stop at this location could contain up to 5,000 cars. The Redevelopment Plan recommends that an urban design plan for the area between Interstate 78 and Baldwin Avenue be completed to ensure that any infrastructure improvements in this area complement the Redevelopment Plan.

The third and smallest of the local intercept facilities, P3, should be located on block 566.1 within the Rehabilitation Area. This land is owned by Hudson County and the Jersey City Parking Authority and is currently used as a permit surface with a capacity of approximately 118 cars. Already accessible from major roads in the Rehabilitation Area, the Plan recommends connecting Central Avenue between Newark Avenue and Hoboken Avenue to create a more direct pedestrian connection to Journal Square and improve vehicular circulation.

Locating a parking structure in this portion of the Rehabilitation Area can help satisfy Hudson County parking demand as well as accommodate new parking demand that is expected to be generated by the anticipated redevelopment of these blocks. A mixed-use parking structure at this location will need to incorporate specific design considerations and appropriate entrances off of Route 139 in order to minimize any impact on the existing neighborhood.

In total, several thousand parking spaces could be provided by local intercept facilities. Any surplus parking contained in these structures could help satisfy parking demand as the Port Authority site and existing public garage is redeveloped.

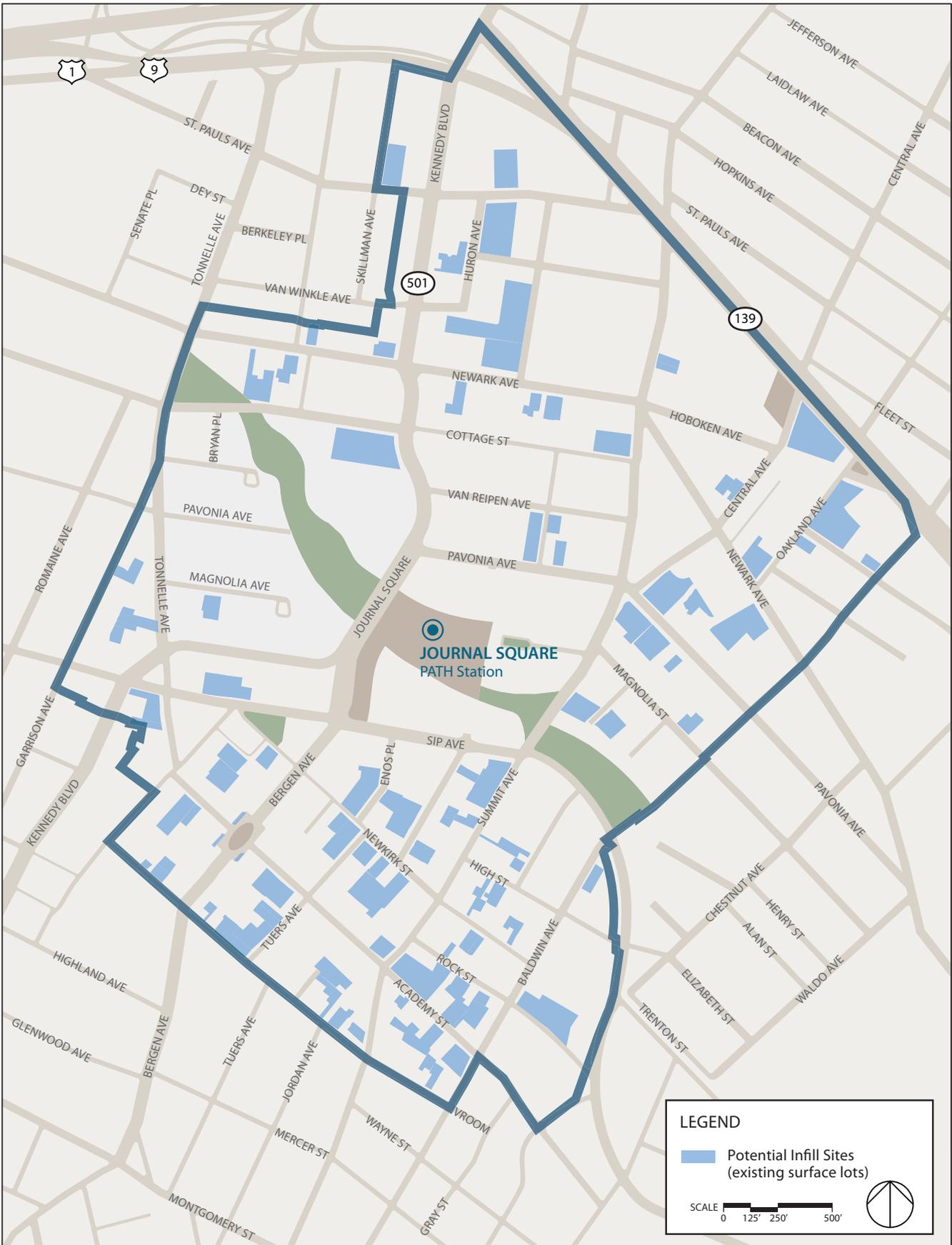
3. **Walkable Parking:** Mixed-use parking structures within a short walking distance (3 to 5 minutes) of a development. These smaller neighborhood-scaled structures should be dispersed throughout the Rehabilitation Area to create a network of convenient parking facilities in Journal Square. These structures should blend in with the architectural context of surrounding buildings. To the extent possible, they should be located on one of the major streets to avoid intruding into the more residential areas. In general, these facilities should contain 100-500 parking spaces and include provisions for motorcycles and bicycles. Car share programs are highly encouraged in these types of facilities.

The Walkable Parking Options map on the following page shows potential locations for walkable mixed-use parking facilities. The areas highlighted on this map represent existing surface parking lots. These lots are prime candidates for infill development that will include walkable parking facilities. Specific design standards must be applied to any site that is developed as a mixed-use parking structure to ensure that they meet the Redevelopment Plan objectives.

In general, each of the three types of intercept parking facilities should be as efficient as possible. Their efficiency will be maximized if they are composed of sets of basic 120 ft. (or larger) by 60 ft. bays that measure a minimum of 240 ft. long. Facilities should be mixed-use and incorporate retail and office uses on the ground floor along pedestrian sidewalks. These intercept parking types will encourage the effective redevelopment of Journal Square while discouraging additional traffic, minimizing noise and pollution and enhancing the pedestrian environment and the quality of life.



EXHIBIT I - WALKABLE PARKING OPTIONS





Journal Square currently contains nearly 1,400 on-street parking spaces. While on-street parking will continue to play a role in Journal Square's future, the Redevelopment Plan recommends that on-street parking be removed from central locations where it is currently permitted in order to accommodate a variety of infrastructure and transit improvements. This removal of on-street parking from strategic locations would result in a reduction of on-street parking capacity by approximately 332 spaces. It is believed that any demand for these spaces can be satisfied in the short term by existing off-street parking facilities and in the longer term by local and regional intercept parking facilities.

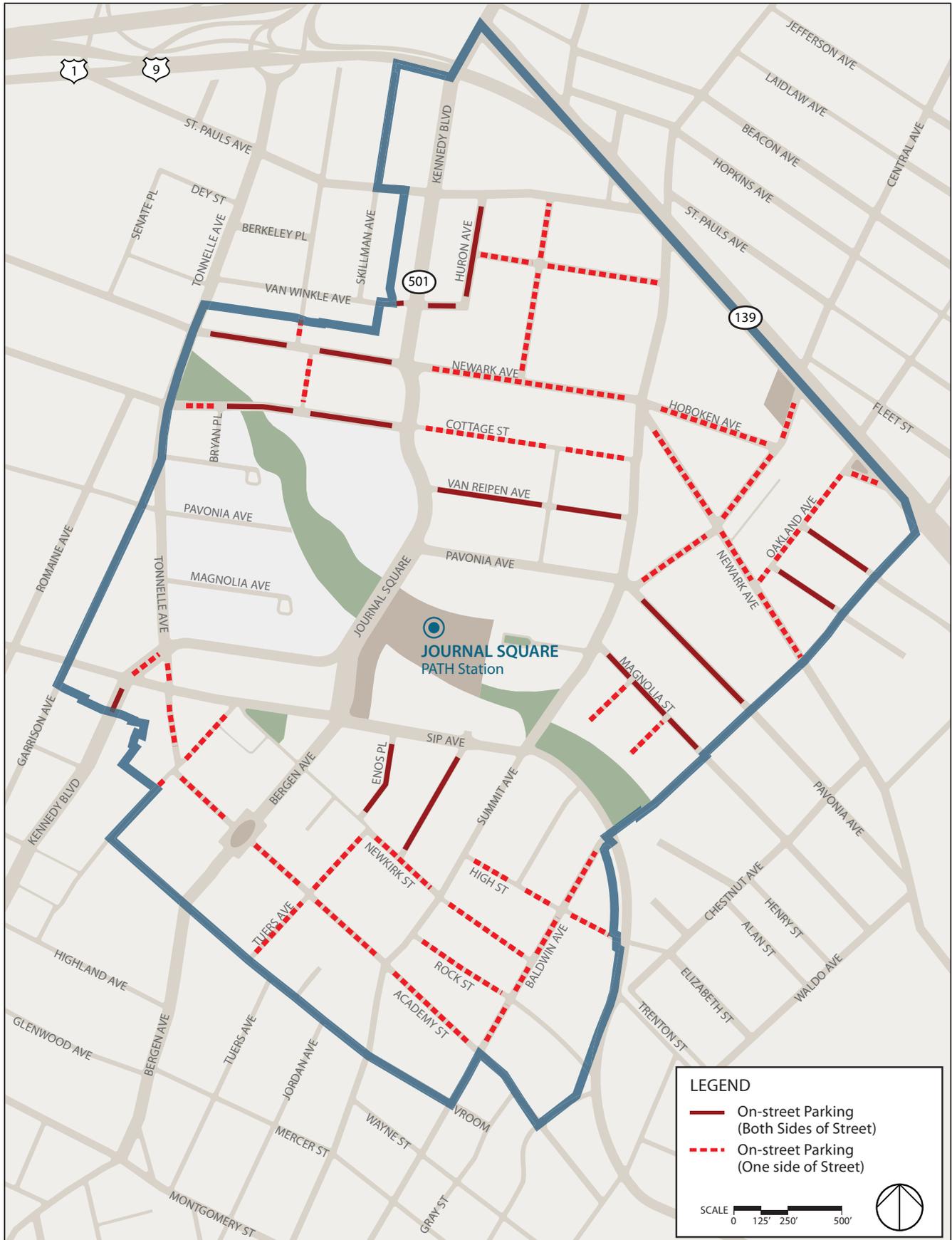
The On-Street Parking Plan indicates where on-street parking is allowed within the Rehabilitation Area. In places, on-street parking is allowed on both sides of a street while in other places, parallel parking is permitted on only one side.

While on-street parking is required on many streets within the Area, parallel parking is limited or not permitted on arterial streets or streets that cannot accommodate it. For example, on-street parking is not permitted on arterial streets such as John F. Kennedy Boulevard, Pavonia Avenue, Sip Avenue, Tonnel Avenue and Summit Avenue in order to accommodate vehicular flow, wider sidewalks, transit and bicycles.

Streets allowing on-street parking must be consistently and clearly marked to indicate where parking is permissible. On-street spaces will not be attached to any specific land use and cannot be used to satisfy any parking requirement. On-street parking placement must meet existing standards regarding distance from corners, fire hydrants, bus stops and delivery zones.



EXHIBIT 19 - ON-STREET PARKING PLAN



The redevelopment of Journal Square will need to coincide with long term utility and infrastructure planning not only for the Journal Square Rehabilitation Area but for all of Jersey City. Infill and targeted redevelopment within the Journal Square Rehabilitation Area may require upgrades to existing water, stormwater management and sewer systems. With the majority of the Journal Square's existing infrastructure and utility systems over fifty years old age and facing obsolescence, redevelopment in the area affords a good opportunity to reassess future utility needs and constraints.

All proposals related to utilities, stormwater management, and water and sewer infrastructure within the Redevelopment Area shall be subject to approval by the relevant City agency. Redevelopers shall satisfy the following requirements, by which the provision for the necessary utilities is accomplished in a way that advances the health safety and welfare of the general public for the life cycle of the building. In addition, the following requirements shall be met by any development proposal:

- A. At such time that redevelopment allows, existing utility poles should be removed and all utilities should be placed under the streets. All utility distribution lines and utility service connections from such lines to the Area's individual uses shall be located underground, including utility and signal mechanized boxes. Utility appliances, regulators, transformers, and metering devices shall be located underground and can be located in the semi-public edge provided that the exposure is screened by landscaping material. Remote readers are required for all utilities, in lieu of external location of the actual metering devices. Developers are required to arrange for connections to public and private utilities.
- B. All machinery and the mechanical controls for same, including but not limited to transformers, dumpsters, junction boxes, lift stations, electrical meters, condensers, and signal boxes, shall be masked from frontages by building elements in a manner consistent with the design of the building, incorporating false windows and dispersed venting to maintain the window rhythm and building pattern design. (A wall of venting for mechanical rooms is not acceptable.) When a mid-block location is incorporated into the project or phase, the above referenced utilities shall be located mid-block if technologically feasible.
- C. Development plans shall meet Jersey City and New Jersey Department of Environmental Protection stormwater requirements. The use of sustainable technique to minimize stormwater runoff should include green roofs, water cisterns and use of stored water for irrigation.
- D. Detention Facilities may be provided to meet the runoff calculations (taking into consideration green roofs and other mechanisms proposed to capture and hold rain water). Detention facilities must not be open, above ground, or on grade.

Vision Journal Square

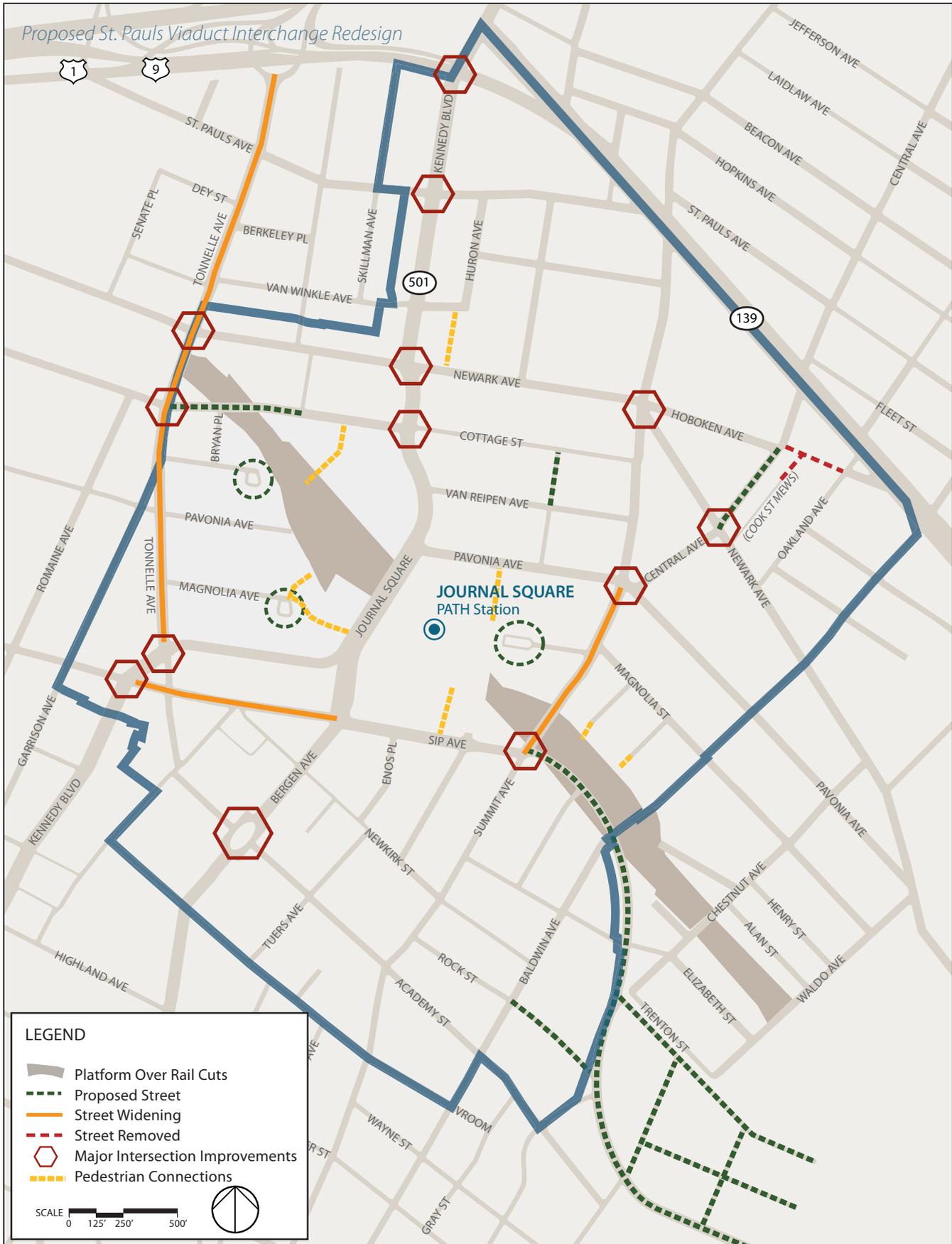
Section 6 Streets

6.1 Street Network Improvements

In order to improve vehicular and pedestrian circulation, the Journal Square Redevelopment Plan proposes the infrastructure updates shown in Exhibit 20 – Street Network Plan. By reconnecting the grid across the deck and widening Primary Streets, traffic within the Area will benefit from enhanced access to Routes 1, 9, and 139. The improvements shown in Exhibit 20 – Street Network Plan are essential to Journal Square’s future development.



EXHIBIT 20 - STREET NETWORK PLAN



A hierarchy of streets has been defined for the Area to allow through traffic to travel the periphery while creating a pedestrian friendly environment surrounding the City Center. There are four (4) major street types within the Area. A fifth street type, a Principal Arterial, forms the Northern border of the Area and is shown for reference on the map to the right. The four (4) major street types in this section, Primary Streets, Secondary Streets, Tertiary Streets, and Pedestrian Priority Streets have specific dimensions for cart ways, sidewalks, tree planting, etc., as they relate to this Redevelopment Plan. The street type standards in this section have no specific relationship to any county, state, or federal standards that may use similar terminology to define street types. The maps in the Plan have included the proposed intersection improvements scheduled for the Route 1, 9, 139, and Tonnelle Avenue - St. Paul's Viaduct Interchange. This interchange is in desperate need of an upgrade and when the proposed redesign is completed circulation to and from the Area will be greatly improved.

The Primary streets will accommodate major through traffic. Tonnelle and Summit Avenues will serve as the major North/South connections to the Principal Arterials, Route 1&9 and Route 139. Kennedy Boulevard will provide a major connection to the North and South of the Area, but will not be continuous through the entire site for vehicular traffic. Portions of Sip, Pavonia, and St. Paul's Avenues will serve as major East/West connections within the Area. The Primary Streets are shown in Dark Red on the map to the right.

Secondary Streets connect the Primary streets to the Tertiary Streets. The Secondary Streets have smaller cart way widths than Primary streets. The Secondary streets are designed to have striped bicycle lanes, wider sidewalks, on-street parking and varied frontages. Newark Avenue, Central Avenue, and parts of Hoboken Avenue, Summit Avenue, and Bergen Avenue have been classified as Secondary Streets. It is recommended that Sip Avenue be extended along the periphery of the Area to connect with Academy Street which would extend to Christopher Columbus Boulevard. This would provide a connection to both the NJ Turnpike and Jersey City Waterfront from Journal Square. The Secondary Streets are shown in Orange on the map to the right.

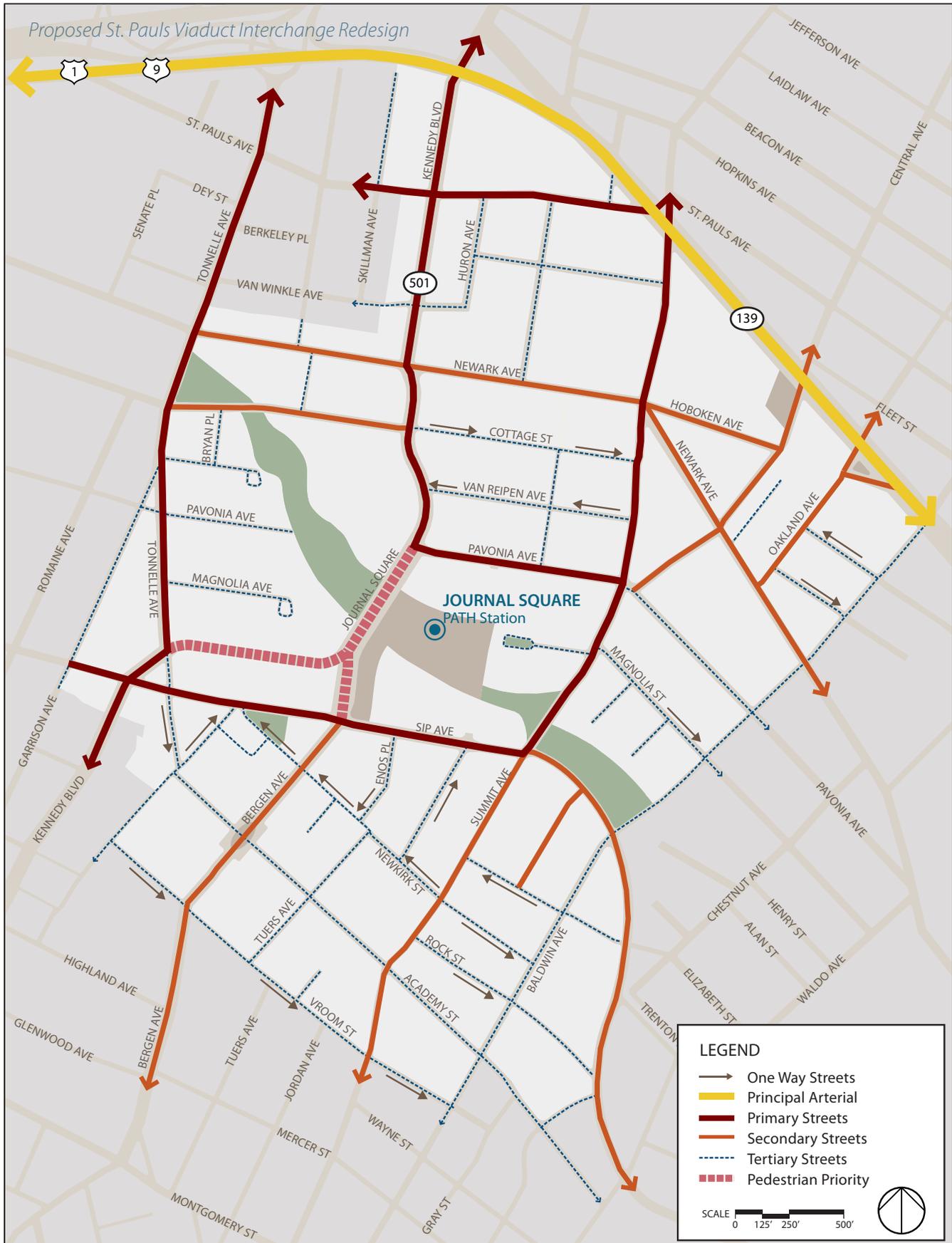
The Tertiary Streets are narrower, quieter, and more residential in character. Within the Tertiary Street types, there are variations allowing for one-way and two-way streets. A Tertiary Street has been cut through between Van Reipen Avenue, Cottage Street, and Newark Avenue to create smaller more walkable blocks. The addition of Tertiary Streets between Newark Avenue, Kennedy Boulevard, St. Paul's Avenue, and Summit Avenue will also create more pedestrian scale blocks in the Northern portion of the Area. The Tertiary Streets are shown as Blue Dashed Lines on the map to the right.

The Pedestrian Priority Street is a combination of Bus Plaza and Pedestrian/Bus Only Street. This street type is based off the Denver model of combining pedestrianism and transit. The Pedestrian Priority Street connects the Journal Square PATH Station, the new Bus Station, the new Bus Plaza, and the City Center Park to the Kennedy Boulevard shopping district. By removing traffic from Kennedy Boulevard between Pavonia Avenue and Tonnelle Avenue, the street will become permeable to the pedestrian allowing for a greater flow of pedestrian traffic between both sides of the street. Vehicular flow has been re-directed around the Pedestrian Priority Street. The adjacent street network is intended to accommodate increased traffic flow. The Primary Streets have been designed to carry an increased amount of traffic that will be phased in from streets that will become Pedestrian Priority Streets.

Specific Street Sections have been prepared for all street types (Exhibit 22 through Exhibit 38). These street sections include details on build-to-lines, streetscaping, on-street parking, bicycle lanes, transit lanes, and cartway widths.



EXHIBIT J - STREET HIERARCHY PLAN



For the purpose of thorough communication, redundancies may exist in the text. If there are any conflicts between these Streets Regulations and information contained elsewhere in this section, these Streets Regulations will take precedence. The General Street Standards shall apply to all street types. More detailed standards for each Street Type follow the General Street Standards.

GENERAL STREET STANDARDS

The Streets Regulations consist of lanes for vehicles and bicycles, as well as pedestrians. Sidewalks and landscaping along these streets are required. The following standards must apply.

- A. The street sections as shown in Exhibits 20 - 36 are mandatory, except for minor deviations to the plan approved by the Planning Board.
- B. The new street section centerlines shall correspond with the existing city and county centerlines.
- C. Tonnelle Avenue must be widened between Kennedy Boulevard and Routes 1 and 9 in accordance with the Exhibit 22 - Primary Street A.
- D. Sip Avenue must be widened between Bergen Avenue and Kennedy Boulevard in accordance with Exhibit 25 - Primary Street D.
- E. Summit Avenue must be widened between Sip Avenue and Pavonia Avenue in accordance with Exhibit 22 -Primary Street A.
- F. The street system shall reconnect the grid pattern as shown in Exhibit 20 - Street Network Plan. The grid pattern shall create development blocks across the new railroad deck.
- G. Cottage Avenue, Pavonia Avenue, Sip Avenue, Central Avenue, Rock Street, and Homestead Place must be extended in accordance with the Exhibit 20 - Street Network Plan and corresponding Street Sections.
- H. Hoboken Avenue shall no longer extend between Central Avenue and Oakland Ave as shown in the Exhibit 20 - Street Network Plan.
- I. Cook Street shall be transformed into a Mews and through traffic shall no longer be allowed.
- J. Three (3) turnarounds are required as shown on Exhibit 20 - Street Network Plan. These turnarounds shall serve as drop off locations for any buildings fronting on them.
- K. A key enabler to support this plan and the continued growth of the Journal Square Area is the proposed redesign of the St. Paul's Viaduct infrastructure. The redesign this interchange and its interface with Tonnelle Avenue should be a regional infrastructure priority.
- L. As properties are redeveloped the new right-of-way width, must be reserved and a portion of the cost of the new streetscape equivalent to the width of the property and half the width of the street must be set aside for such a time as the City determines the entire street shall be redeveloped.
- M. The build-to-line shall be determined by measurement from the centerline of the existing streets as determined by the Governing Body controlling that right-of-way
- N. All on-street parking shall be priced and metered at market rates to allow for a 10% vacancy rate at any time.
- O. A key enabler to support this plan and the continued growth of the Journal Square Area is the proposed redesign of the St. Paul's Viaduct infrastructure. The redesign this interchange and its interface with Tonnelle Avenue should be a regional infrastructure priority.

Pedestrian Amenities and Access

- A. All streets shall be open to the public. All sidewalk and streetscapes shall be improved to finished specifications prior to the occupation of the building and offered to the city for dedication.
- B. All City Center Park blocks edges that are not bound by a street must have a sidewalk between building edges and park space.
- C. Retail uses on corners shall not hinder pedestrian flow.
- D. Outdoor corner cafes and eating areas should be edged with planters, low fences, or other decorative features to designate the café area from the pedestrian circulation along the sidewalk. A minimum of 50 percent of the sidewalk width must be reserved as barrier-free pedestrian area between the edge of the curb and the edge of the café.
- E. Sidewalk areas shall be continuous across any driveway, including any decorative paving elements.
- F. Bollards must line any curb edge where travel lanes are adjacent to pedestrians.
- G. All crosswalks must be a minimum of 6 feet in width and follow the standards for their respective street type.



Curbing

- A. Access to any driveway, garage, parking alley or common parking area shall be via a dropped curb and sloped apron. Said access shall not be provided by the use of radius curbing and an extension of the street pavement.
- B. Driveway widths and curb cuts shall be kept to the minimum width and number necessary. Shared curb cuts and driveways are encouraged.
- C. All curbing should be granite, or French grey colored poured brushed concrete. Curbing type must be continuous for the entirety of the block. Curbs shall be 6 inches in height from the final top height of the pavement. Asphalt curbing is prohibited.
- D. Barrier free access to all pedestrian spaces is required.

Bicycles

- A. Bicycle lanes must be at least 4' wide. Where bicycle lanes are adjacent to parallel parking, they must be at least 5' in width.
- B. Bicycle lanes shall be composed of colored asphalt or concrete.
- C. Bicycle racks or other suitable means of securing bicycles shall be provided and incorporated into the streetscape/ street furniture design

Landscape Elements

- A. Street trees shall not be planted within the clear vision triangle of each intersection according to City regulation.
- B. All street trees shall be planted within a planting well and should be surrounded with either dry-laid pavers or ground cover protected with decorative fencing or raised brick/ stone barrier 8-12 inches in height (this does not apply to Tertiary Streets, see Tertiary Streets Standard F). Tree pits shall be a minimum of 4-5 feet wide by 6-8 feet long (this does not apply to Tertiary Streets, see Tertiary Streets Standard F). Soil handling and compaction standards (Section 7 – Landscape) for all tree planting must be met.
- C. Utilities shall not interfere with street tree planting.
- D. The use of decorative grates around trees is discouraged because they constrict the growth of the tree if not maintained properly and can lead to the death of the tree in later years.
- E. All street furniture including benches, trash receptacles, bicycle racks, kiosks, planters, and bollards, shall be of a consistent and uniform design vocabulary to establish visual continuity throughout the Area. Final selection of street furniture shall be made by the Planning Board Site Plan Review Subcommittee.
- F. Trash receptacles, mailboxes, bicycle racks, and other pedestrian obstructions shall be located at the outer edge of the sidewalk (within 4 feet of the curb). Exceptions include: Vending racks and sidewalk dining may encroach on the sidewalk as long as a minimum of 50 percent of the sidewalk width is reserved as barrier-free pedestrian area between the edge of the curb and the edge of the café.

Lighting

- A. Street lighting shall be placed at the outer edge of all sidewalks.
- B. All street lighting must be dark sky compliant. The control of light pollution to surrounding areas shall be in accordance with the lighting requirements of the Jersey City Zoning Code, Jersey City Ordinances, or this Plan.
- C. Metal Halide (or comparable light quality) lamps are suggested because of their efficiency and light quality.
- D. Mercury Vapor and high pressure sodium lighting shall not be allowed.
- E. Cobra Head light fixtures are forbidden.
- F. A comprehensive lighting plan for the entire Area shall include a hierarchy of lighting types as designated for Primary Streets, Secondary Streets, Tertiary Streets, and Pedestrian Priority Streets in this section. Final selection of fixtures shall be made by the Planning Board Site Plan Review Subcommittee.



STANDARDS FOR PRIMARY STREETS

- A. All Primary Street travel lanes shall be 11 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Primary Streets must be at least 20 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Primary Streets. No more than three (3) approved sidewalk paving materials shall be used per block.
- C. The outer four to five (4–5) feet of sidewalks shall be constructed of bricks (or approved alternative paving material) adjacent to and between street trees. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.
- D. Textured pavement such as brick, cobblestone or pavers, or stamped concrete shall be used on all Primary Street crosswalks at to act as a traffic-calming device.
- E. Crosswalks on Primary Streets shall be of similar material and color as the paved sidewalks.
- F. All street trees planted on Primary Streets shall be a minimum of 4 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting. A branch height of 12 feet must be maintained adjacent to vehicular travel lanes. Please see Section 7 – Landscape for planting specifications.
- G. Medium-Large street tree species are required along Primary Streets. Medium-Large trees shall be spaced every 30 feet on center along streets and park edges.
- H. A unified standard for street lighting along Primary Streets must be used.
- I. Primary streets shall have long bracket posts with tear drop lamps that should be 24 feet in height. Street lights on Primary Streets shall be spaced 60 feet on center depending on wattage.

STANDARDS FOR SECONDARY STREETS

- A. All Secondary Street travel lanes shall be 10.5 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Secondary Streets must be at least 15 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Secondary Streets. No more than three (3) approved sidewalk paving materials shall be used per block.
- C. The outer four to five (4 – 5) feet of sidewalks shall be constructed of bricks (or approved alternative paving material) adjacent to and between the planting strip. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.
- D. Textured pavement such as brick, cobblestone or pavers, or stamped concrete shall be used on Secondary Street crosswalks at major intersections at to act as a traffic-calming device.
- E. Major crosswalks on Secondary Streets shall be of similar material and color as the paved sidewalks.
- F. All street trees planted on Secondary Streets shall be a minimum of 3.5 inches in caliper measured at 6 inches from the ground level and have a minimum branch height or 8 feet at time of planting. A branch height of 12 feet must be maintained adjacent to vehicular travel lanes. Please see Section 7 – Landscape for planting specifications.
- G. Medium sized street tree species are required along Secondary Streets. Medium size trees shall be spaced every 25 feet on center along streets and park edges.
- H. A unified standard for street lighting along Secondary Streets must be used.
- I. Secondary streets shall have short bracket posts with tear drop lamps that should be 18 feet in height. Street lights on Secondary streets shall be spaced 50 feet on center depending on wattage.



STANDARDS FOR TERTIARY STREETS

- A. All Tertiary Street travel lanes shall be 10 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Tertiary Streets must be at least 12 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Tertiary Streets. Alternative paving materials shall include, bluestone, and approved gray concrete pavers. No more than three (3) paving materials or colors shall be used per block.
- C. Crosswalks on Tertiary Streets shall be clearly striped at all intersections and wherever pedestrian traffic conflicts with vehicular traffic.
- D. All trees shall be a minimum of 3 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting.
- E. Small - Medium sized street tree species are required along Tertiary streets. Small - Medium size trees shall be spaced every 20 feet on center along streets and park edges.
- F. All Tertiary Street trees shall be planted within a planting well and shall be surrounded by vegetative ground cover protected with a raised curb or decorative fencing 6-10 inches in height. Tree pits shall be a minimum of 4-5 feet from curb to sidewalk and 4-6 feet in width.
- G. A unified standard for street lighting along Tertiary Streets must be used.
- H. Tertiary Streets shall have post top lamps that should be 12-14 feet in height. Street lights on Tertiary Streets shall be spaced 40 feet on center.

STANDARDS FOR PEDESTRIAN PRIORITY STREETS

- A. The proposed Pedestrian Priority Street is to function as the major retail corridor as well as provide pedestrian, bus, and taxi access from the Journal Square Plaza and PATH Station to Tonnelle Avenue.
- B. The Pedestrian Priority Street shall not provide any vehicular access, with the exception of NJ Transit Buses, Taxis, Emergency Vehicles, and light deliveries that support the day-to-day functioning of the building with which they are associated.
- C. The Pedestrian Priority Street must have a continuous paving that extends from building edge to building edge.
- D. The paving material used for pedestrian priority streets shall be complementary of materials used in the Journal Square Plaza and the City Center Park, such as granite, cobblestone or pavers, or stamped concrete. Asphalt is prohibited.
- E. Pedestrian Priority Street C shall have designated pedestrian crosswalks that must be clearly distinguishable. These crosswalks shall be paved with materials complementary of the materials used in the Journal Square Plaza as described in the previous regulation.
- F. Travel lanes for bus access shall be between 11 to 12 feet in width.
- G. All trees shall be a minimum of 4 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting. Trees shall be maintained to a minimum of 12 feet in height where they are adjacent to bus/ transit lanes.
- H. Medium-Large street tree species are required along Pedestrian Priority Streets. Medium-Large trees shall be spaced every 25 feet on center along streets and park edges.
- I. A unified standard for street lighting in the Pedestrian Priority Street area must be used.
- J. Pedestrian Priority Streets shall have post top lamps that should be 12-14 feet in height. Street lights on Pedestrian Priority Streets shall be spaced 50 feet on center.

The Street Plan exhibits a hierarchical interconnected network. All streets will receive streetscape improvements. There are four (4) overall street categories; Primary, Secondary, Tertiary, and Pedestrian Priority. Within these categories more specific Street Standards were developed.

The following street labels refer to specific street designs included in the Street Standards:

- ST – Street
- BLVD – Boulevard
- BL – Bicycle Lane
- BR – Bicycle Route
- TR – Transit Route

The labels above are used in conjunction with Right-Of-Way widths and Cartway widths to create a Street Classification. For example the Street Classification, ST:80:38, is a "Street" with an 80 foot right-of-way and a 38 foot cartway width from curb to curb.

There are fifteen (15) street typologies within the Area. Exhibit 21 - Street Typology Plan, illustrates the location of these street types. Each type has a set of recommended standards including travel and parking lane widths, vehicular design speed, bicycle lane width, sidewalk width, street lighting, and street tree type and dimensions.

HOW TO INTERPRET THE STREET SECTIONS FOUND IN THIS DOCUMENT:

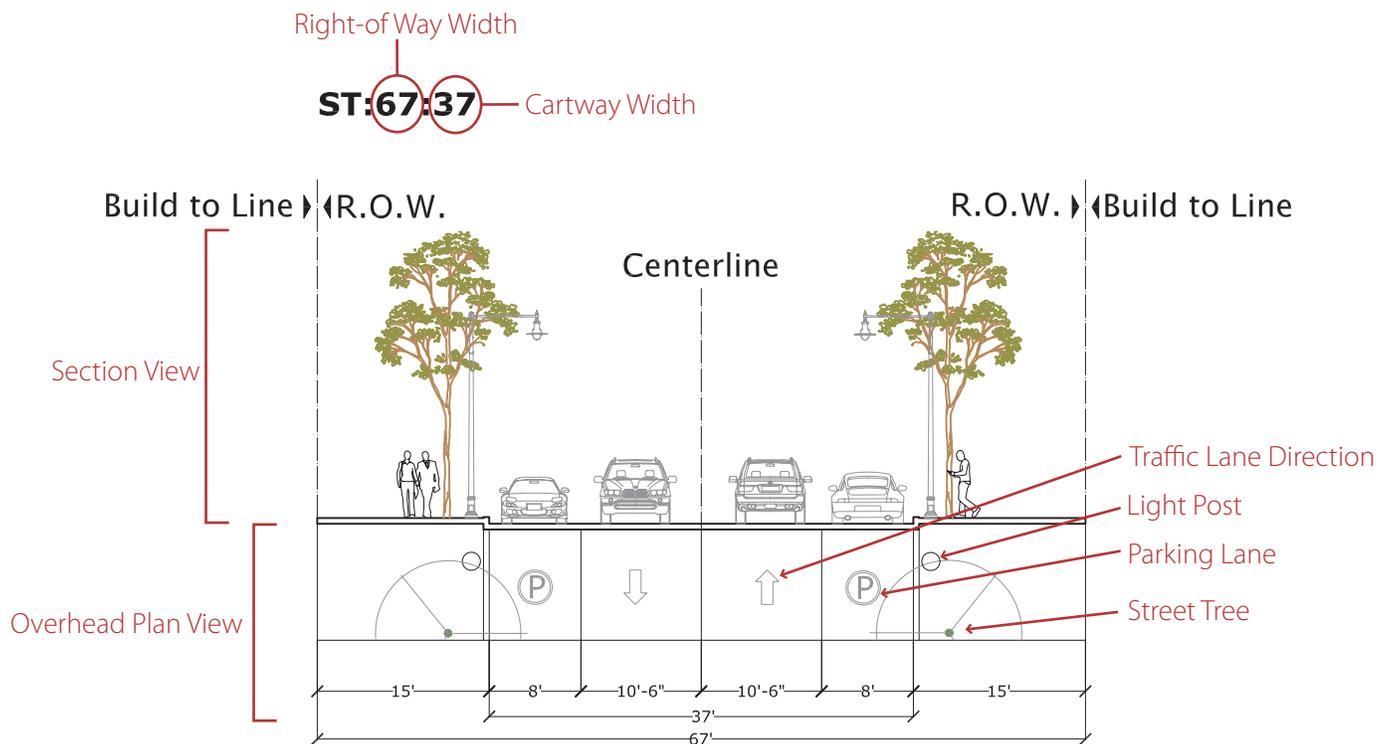
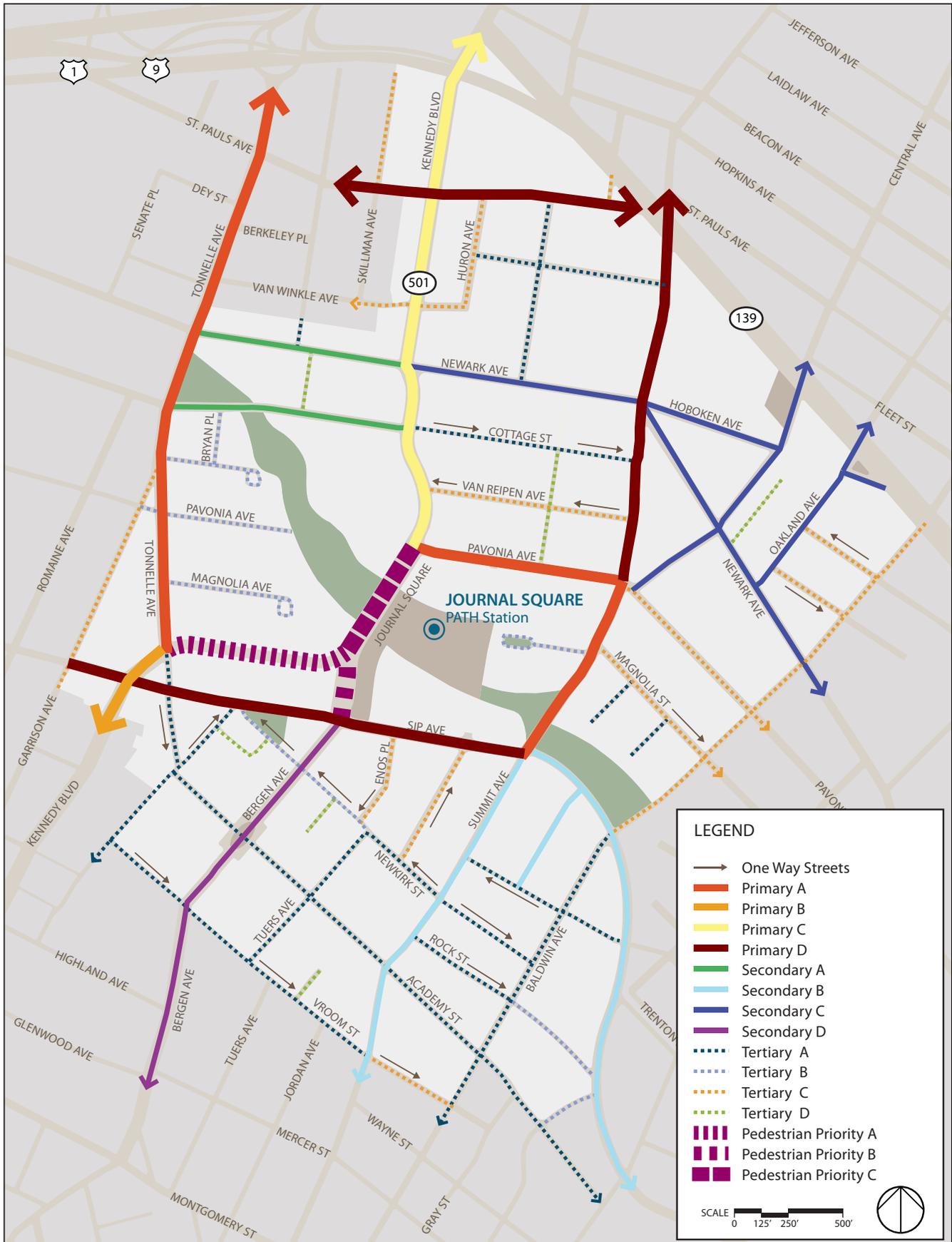




EXHIBIT 21 - STREET TYPOLOGY PLAN



LEGEND

- One Way Streets
- Primary A
- Primary B
- Primary C
- Primary D
- Secondary A
- Secondary B
- Secondary C
- Secondary D
- Tertiary A
- Tertiary B
- Tertiary C
- Tertiary D
- Pedestrian Priority A
- Pedestrian Priority B
- Pedestrian Priority C

SCALE 0 125' 250' 500'



An urban Primary Street system is designed to carry high volumes of traffic with greater land access and less through traffic than a principal arterial system (such as Route 1). It should be the primary link between the principal arterials and Secondary Streets. Primary Streets should not intrude into residential neighborhoods. This section provides specifications for all Primary Streets in the Journal Square Area.

STANDARDS FOR PRIMARY STREETS

- A. All Primary Street travel lanes shall be 11 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Primary Streets must be at least 20 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Primary Streets. No more than three (3) approved sidewalk paving materials shall be used per block.
- C. The outer four to five (4–5) feet of sidewalks shall be constructed of bricks (or approved alternative paving material) adjacent to and between street trees. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.
- D. Textured pavement such as brick, cobblestone or pavers, or stamped concrete shall be used on all Primary Street crosswalks to act as a traffic-calming device.
- E. Crosswalks on Primary Streets shall be of similar material and color as the paved sidewalks.
- F. All street trees planted on Primary Streets shall be a minimum of 4 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting. A branch height of 12 feet must be maintained adjacent to vehicular travel lanes. Please see Section 7 – Landscape for planting specifications.
- G. Medium-Large street tree species are required along Primary Streets. Medium-Large trees shall be spaced every 30 feet on center along streets and park edges.
- H. A unified standard for street lighting along Primary Streets must be used.
- I. Primary streets shall have long bracket posts with tear drop lamps that should be 24 feet in height. Street lights on Primary Streets shall be spaced 60 feet on center depending on wattage.

Examples of Primary Streets





EXHIBIT K - PRIMARY STREETS

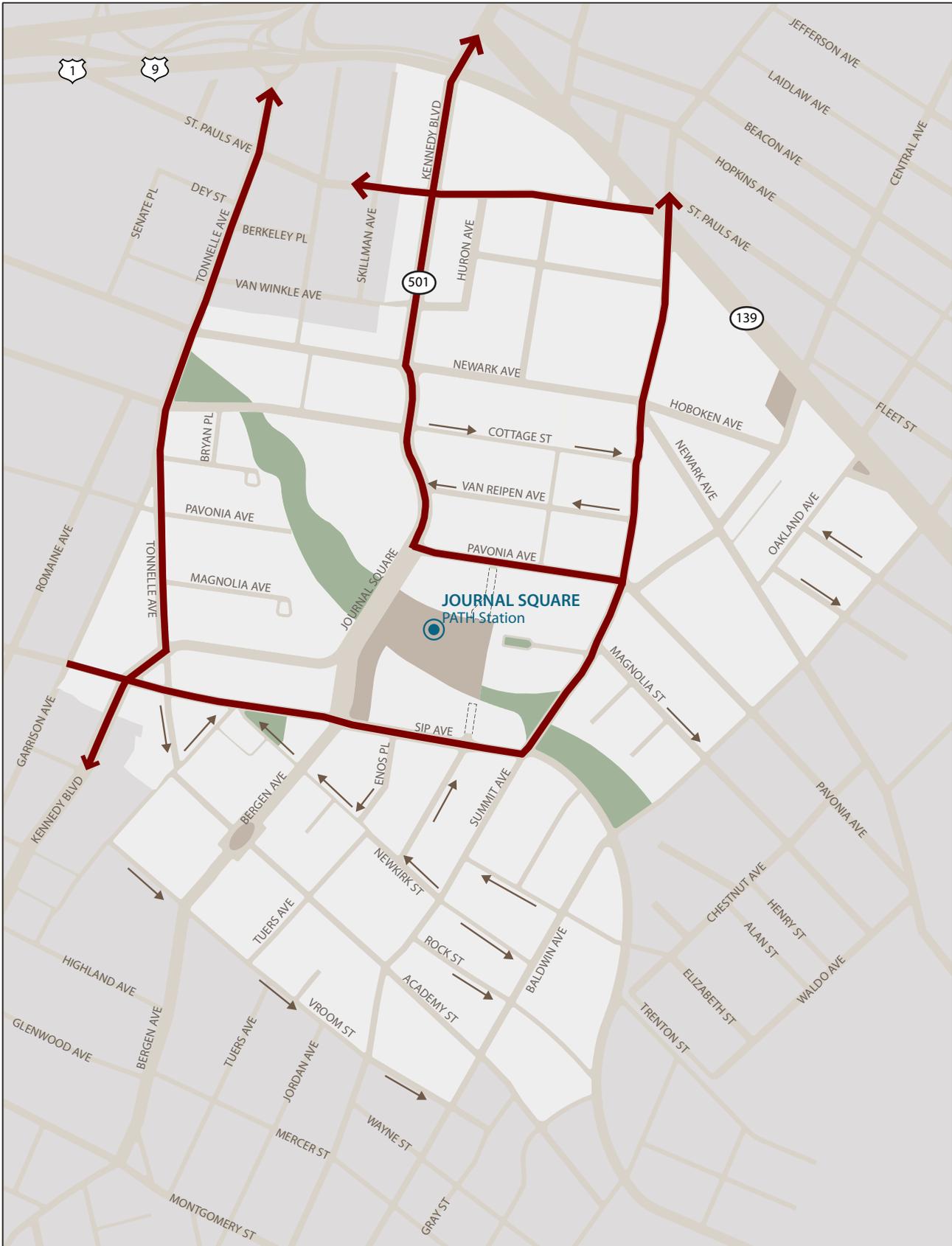
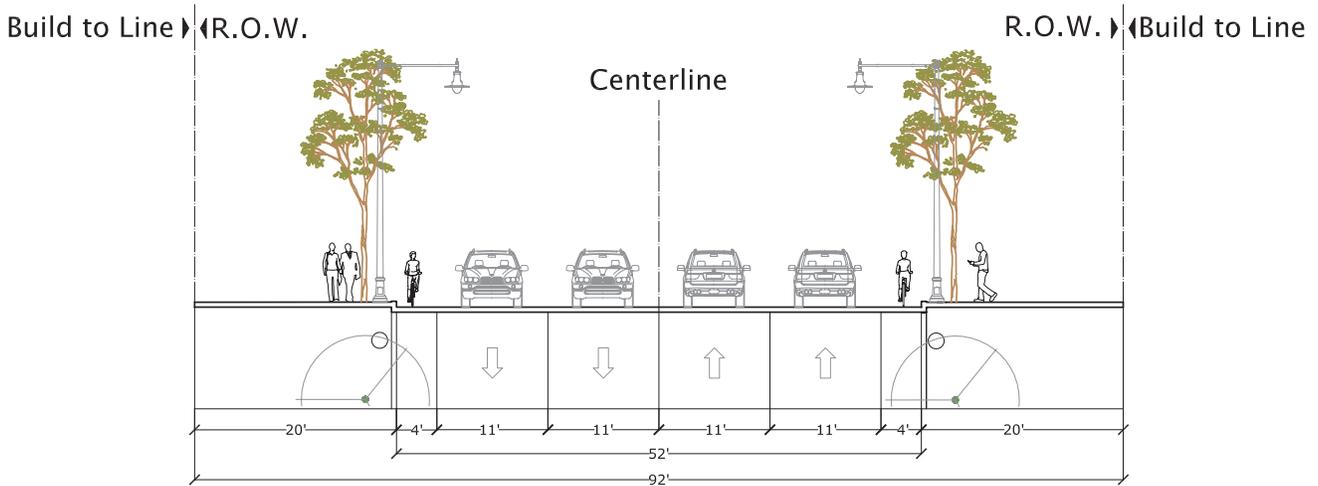


EXHIBIT 22 - PRIMARY STREET A ST:92:52:BL



Primary A ST:92:52:BL	
Travel Lanes	4
Lane Width	11 feet
Design Speed	35 mph
Bike Lane Width	4 feet
Sidewalk Width	20 feet
Light Type	Long Bracket
Light Spacing	60 feet
Street Tree Spacing	30 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree

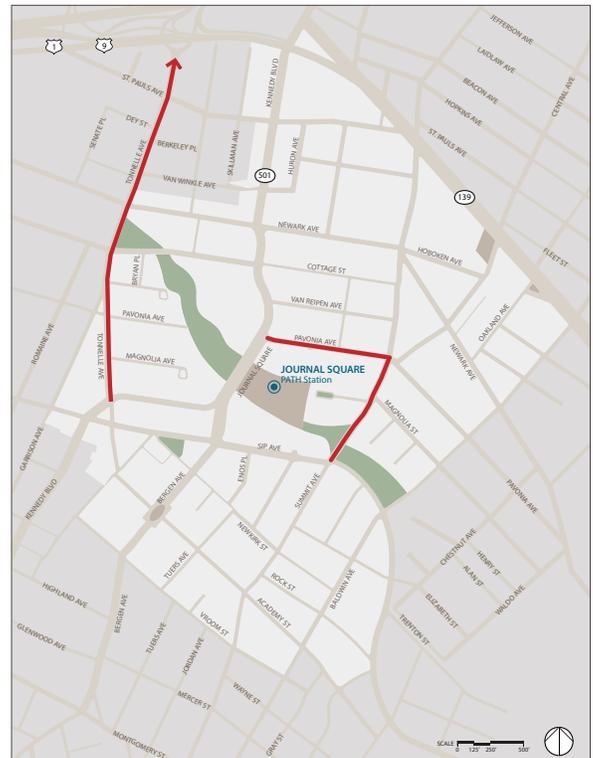
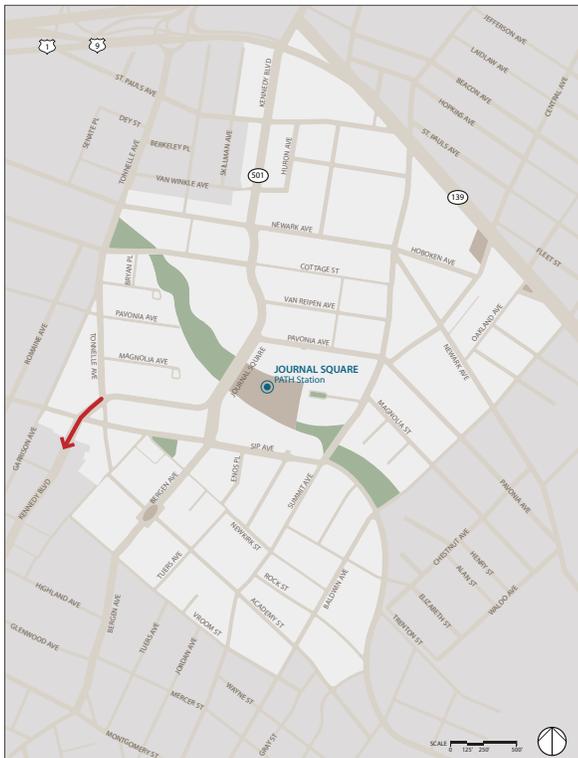
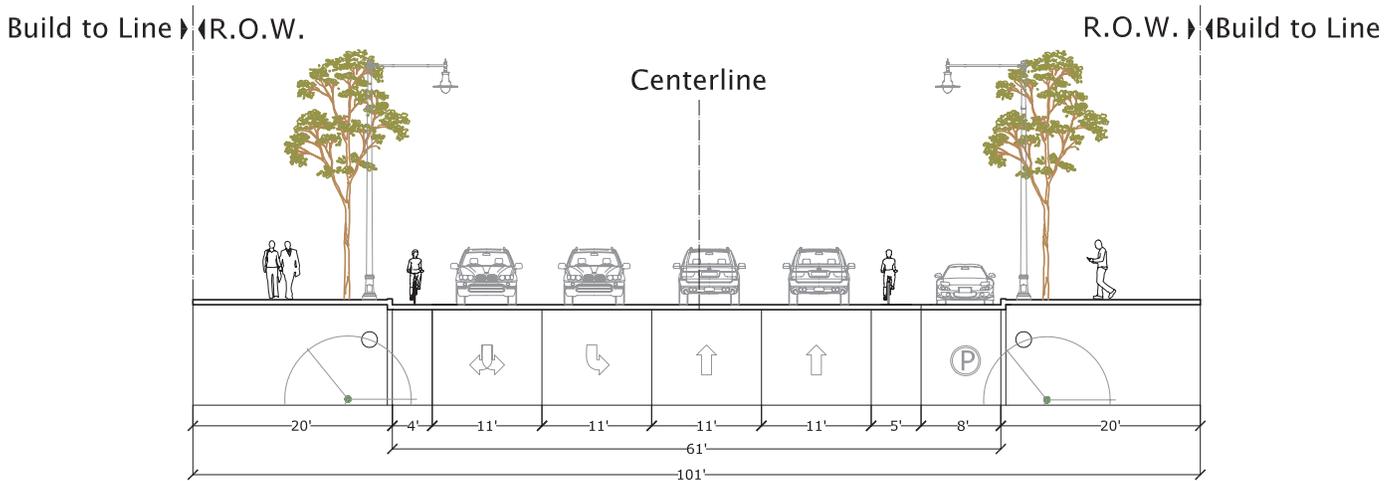


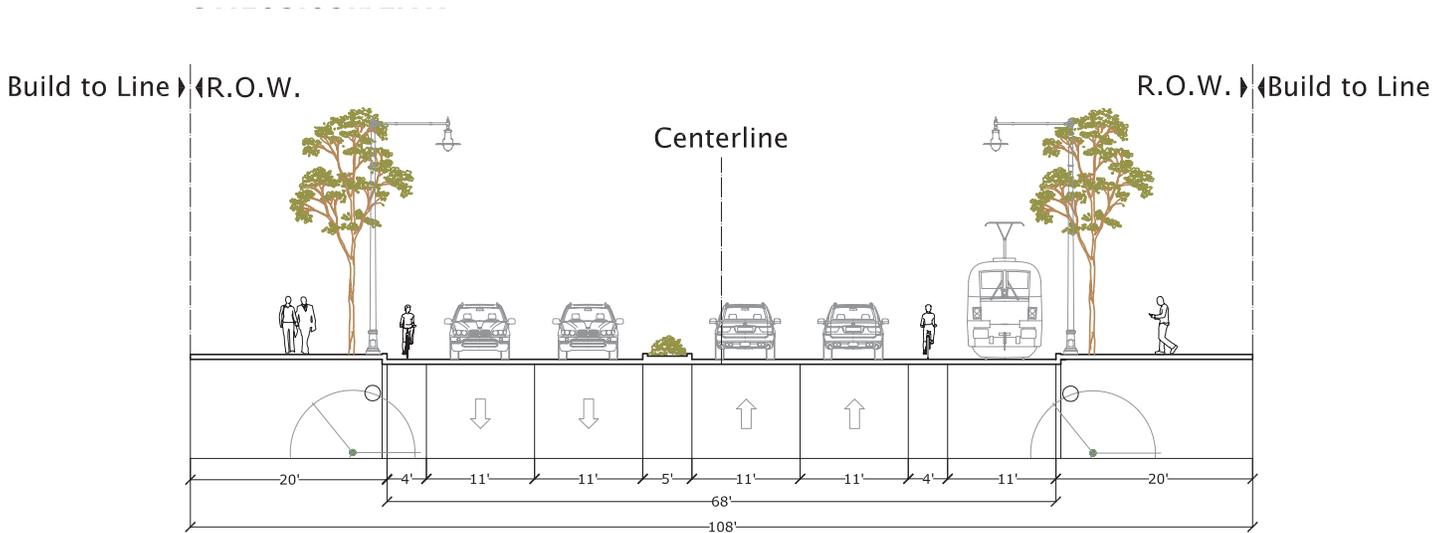


EXHIBIT 23 - PRIMARY STREET B ST:101:61:BL



Primary B ST:101:61:BL	
Travel Lanes	4
Lane Width	11 feet
Design Speed	35 mph
Parking Lane	one side
Parking Lane Width	8 feet
Bike Lane Width	4 feet/5 feet next to parking
Sidewalk Width	20 feet
Light Type	Long Bracket
Light Spacing	60 feet
Street Tree Spacing	30 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree

EXHIBIT 24 - PRIMARY STREET C ST:108:68:BL:TR



Primary C ST:108:68:BLVD:BL:TR	
Travel Lanes	4
Lane Width	11 feet
Transit Type	Streetcar
Transit Lane Width	11 feet
Design Speed	35 mph
Bike Lane Width	4 feet
Sidewalk Width	20 feet
Light Type	Long Bracket
Light Spacing	60 feet
Street Tree Spacing	30 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree*

*Columnar habit required along transit lines

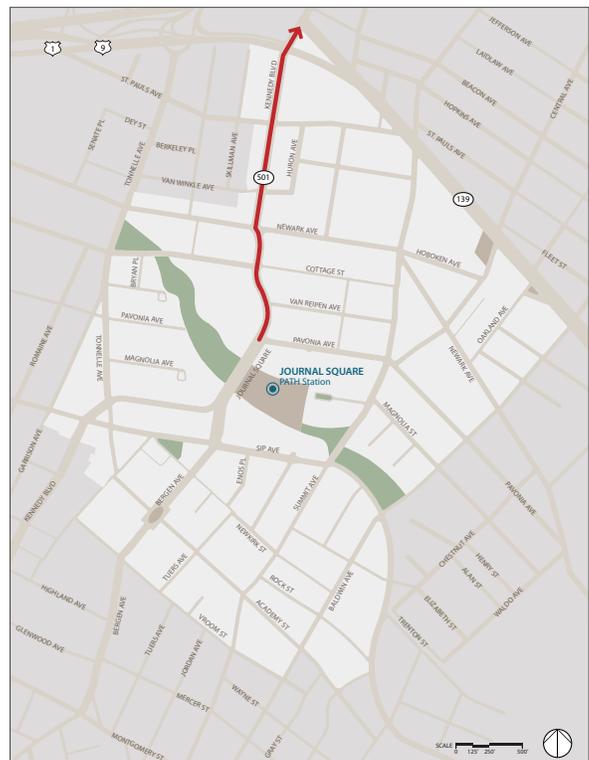
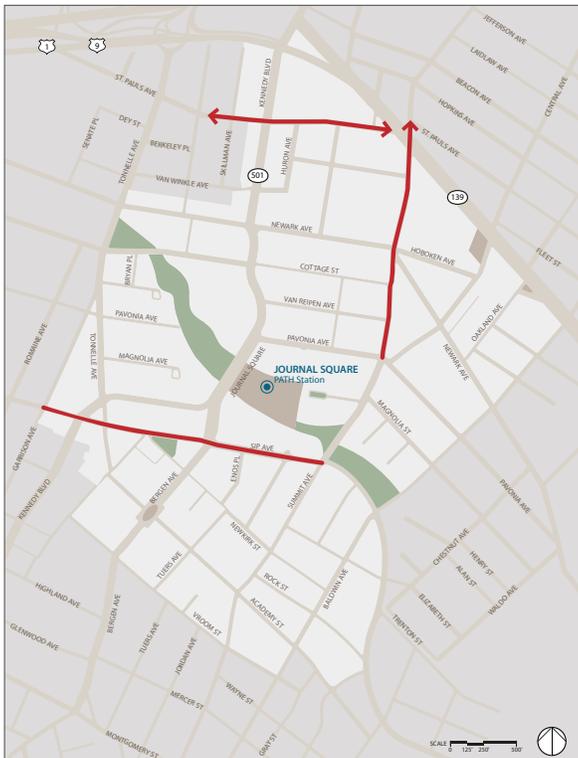
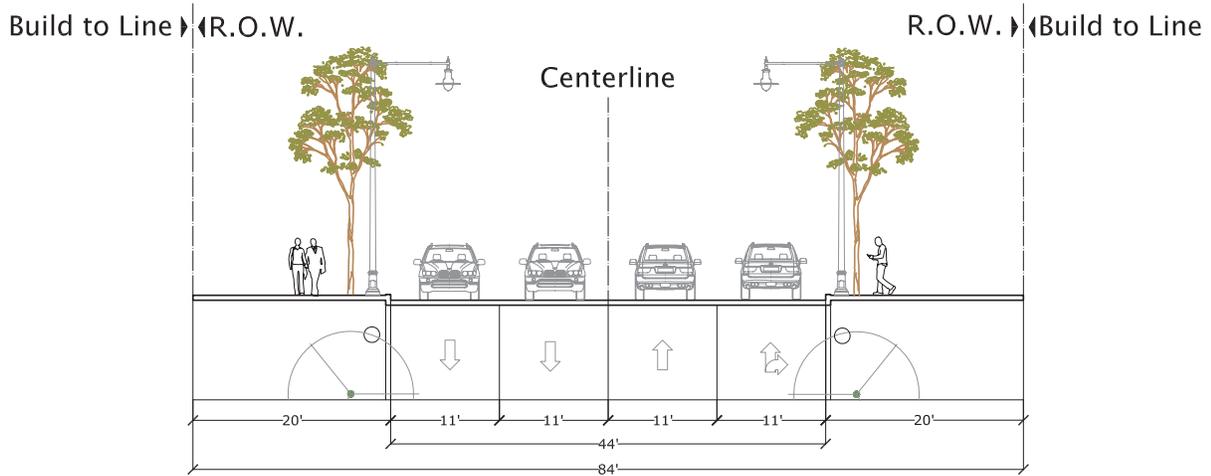




EXHIBIT 25 - PRIMARY STREET D ST:84:44



Primary D ST:84:44	
Travel Lanes	4
Lane Width	11 feet
Design Speed	35 mph
Sidewalk Width	20 feet
Light Type	Long Bracket
Light Spacing	60 feet
Street Tree Spacing	30 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree



An urban Secondary Street system is designed to carry moderate volumes of traffic with an approximately equal emphasis on land access and traffic mobility. It should be the primary link between Primary Streets and Tertiary Streets. These roads may, but do not necessarily, run through residential neighborhoods. This section provides specifications for all Secondary Streets in the Journal Square Area.

STANDARDS FOR SECONDARY STREETS

- A. All Secondary Street travel lanes shall be 10.5 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Secondary Streets must be at least 15 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Secondary Streets. No more than three (3) approved sidewalk paving materials shall be used per block.
- C. The outer four to five (4 – 5) feet of sidewalks shall be constructed of bricks (or approved alternative paving material) adjacent to and between the planting strip. Alternative paving materials shall include granite, bluestone, and approved gray concrete pavers.
- D. Textured pavement such as brick, cobblestone or pavers, or stamped concrete shall be used on Secondary Street crosswalks at major intersections to act as a traffic-calming device.
- E. Major crosswalks on Secondary Streets shall be of similar material and color as the paved sidewalks.
- F. All street trees planted on Secondary Streets shall be a minimum of 3.5 inches in caliper measured at 6 inches from the ground level and have a minimum branch height of 8 feet at time of planting. A branch height of 12 feet must be maintained adjacent to vehicular travel lanes. Please see Section 7 – Landscape for planting specifications.
- G. Medium sized street tree species are required along Secondary Streets. Medium size trees shall be spaced every 25 feet on center along streets and park edges.
- H. A unified standard for street lighting along Secondary Streets must be used.
- I. Secondary streets shall have short bracket posts with tear drop lamps that should be 18 feet in height. Street lights on Secondary streets shall be spaced 50 feet on center depending on wattage.

Examples of Secondary Streets





EXHIBIT L - SECONDARY STREETS

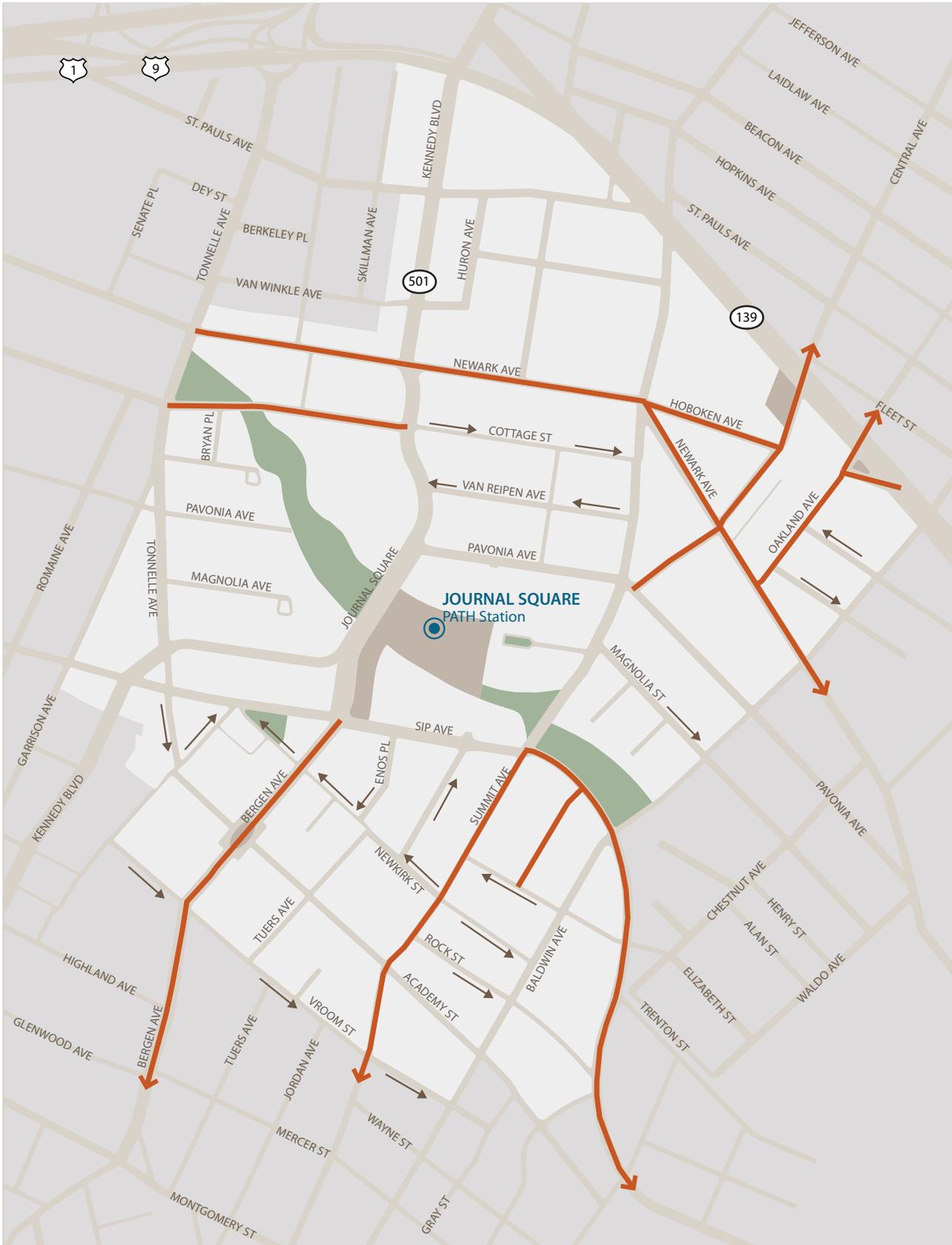
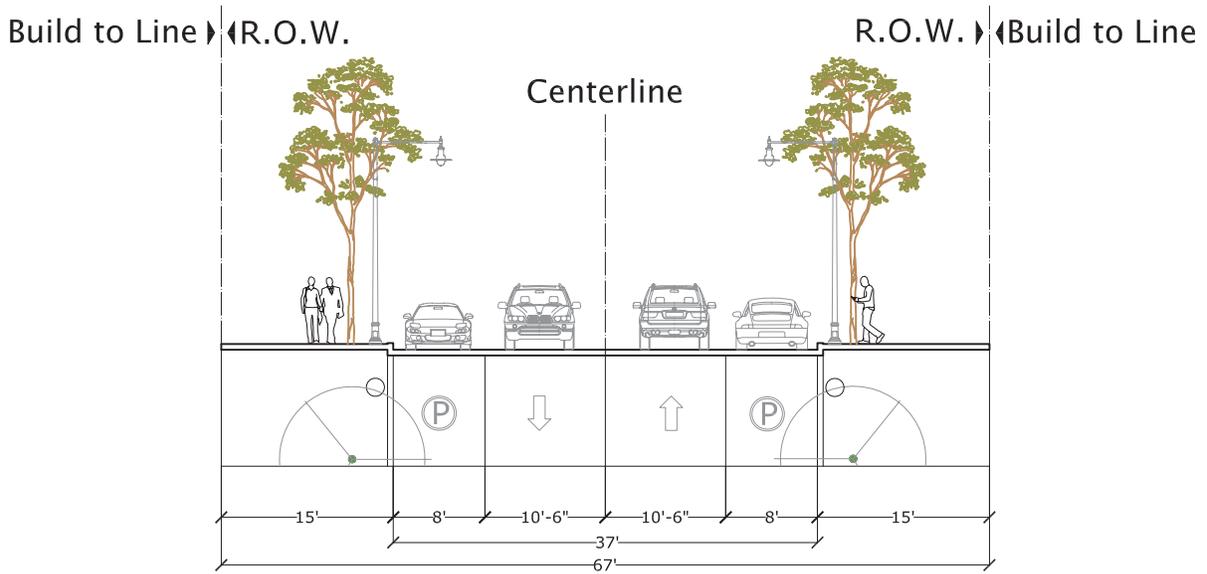


EXHIBIT 26 - SECONDARY STREET A ST:67:37



Secondary A ST:67:37	
Travel Lanes	2
Lane Width	10 feet 6 inches
Design Speed	30 mph
Sidewalk Width	15 feet
Parking Lane	two sides
Parking Lane Width	8 feet
Light Type	Short Bracket
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	3.5 inches
Street Tree Type	Medium Shade Tree

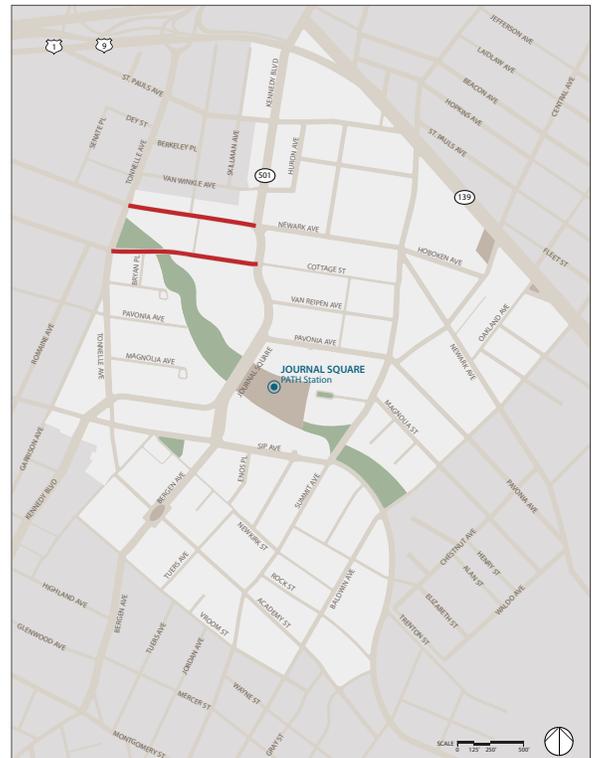
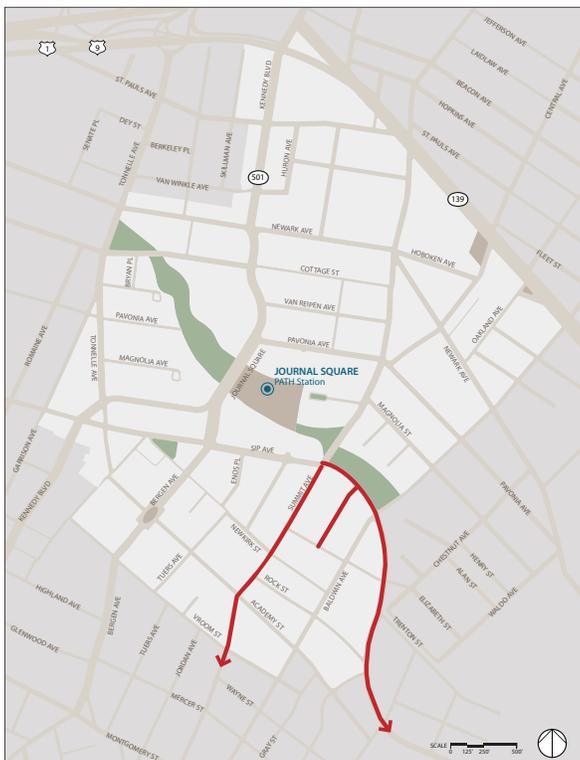
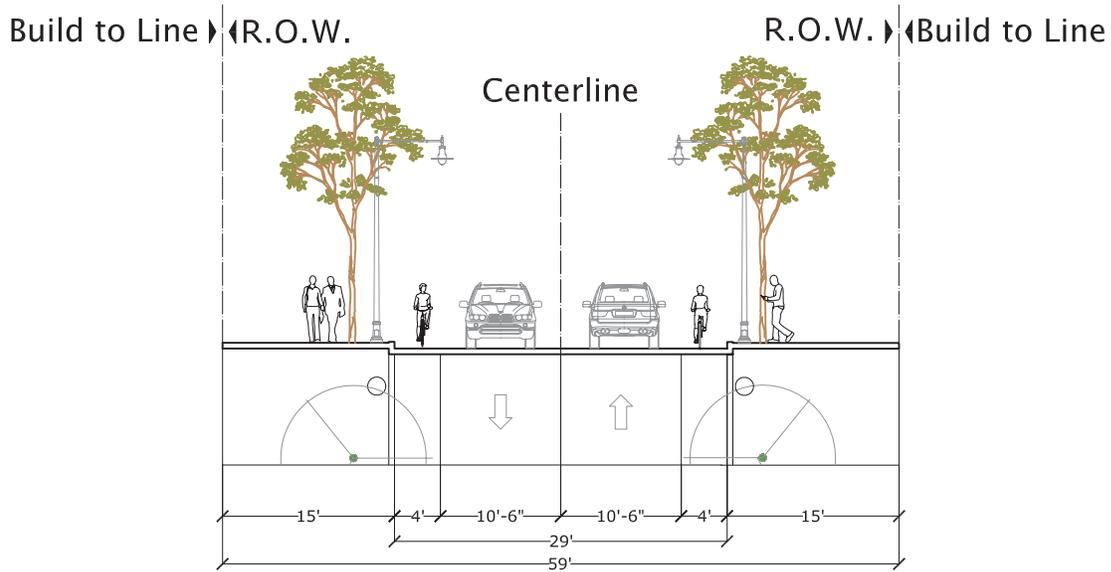


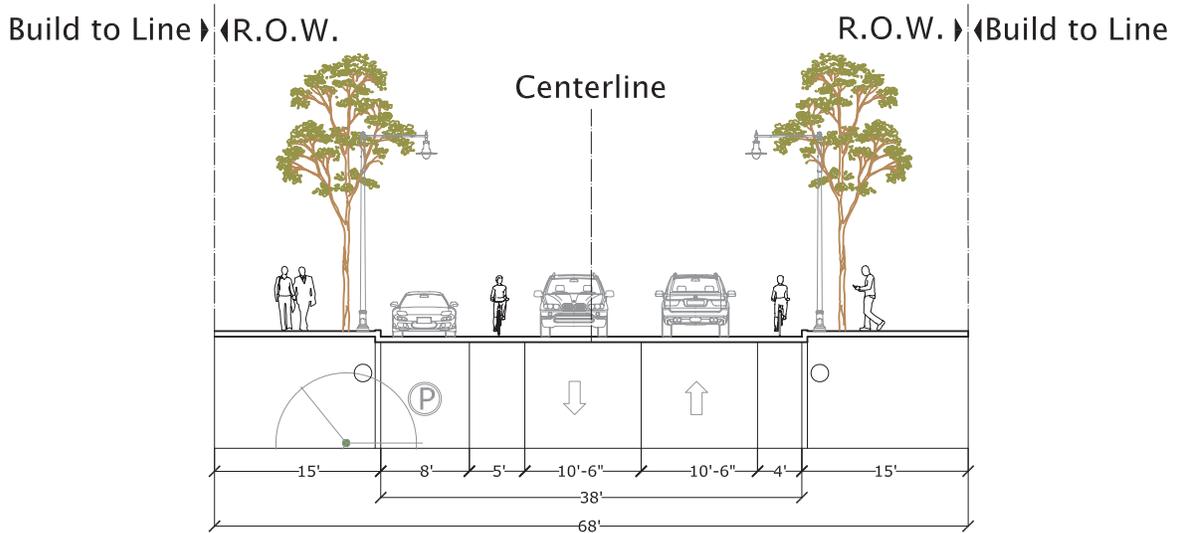


EXHIBIT 27 - SECONDARY STREET B ST:59:29:BL



Secondary B ST:59:29:BL	
Travel Lanes	2
Lane Width	10 feet 6 inches
Design Speed	30 mph
Bike Lane Width	4 feet
Sidewalk Width	15 feet
Light Type	Short Bracket
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	3.5 inches
Street Tree Type	Medium Shade Tree

EXHIBIT 26 - SECONDARY STREET C ST:68:38:BL



Secondary C ST:68:38:BL	
Travel Lanes	2
Lane Width	10 feet 6 inches
Design Speed	30 mph
Parking Lane	one side
Parking Lane Width	8 feet
Bike Lane Width	4 feet/5 feet next to parking
Sidewalk Width	15 feet
Light Type	Short Bracket
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	3.5 inches
Street Tree Type	Medium Shade Tree

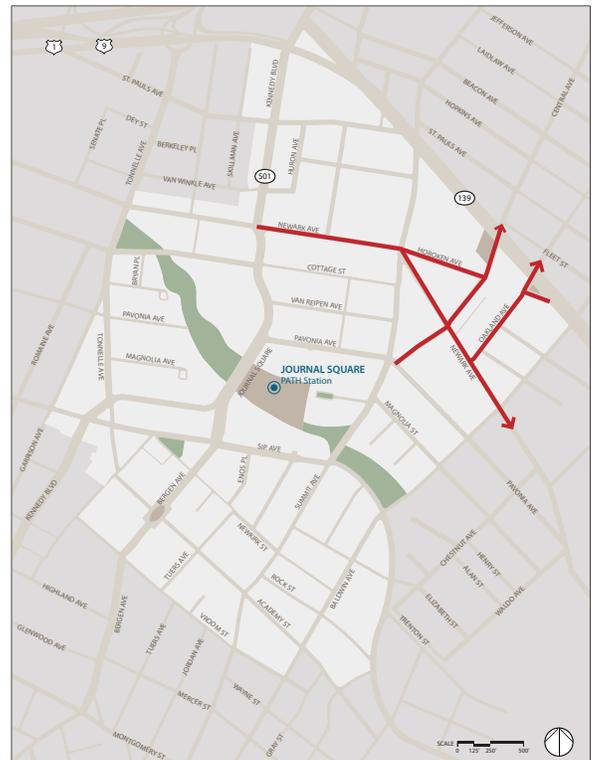
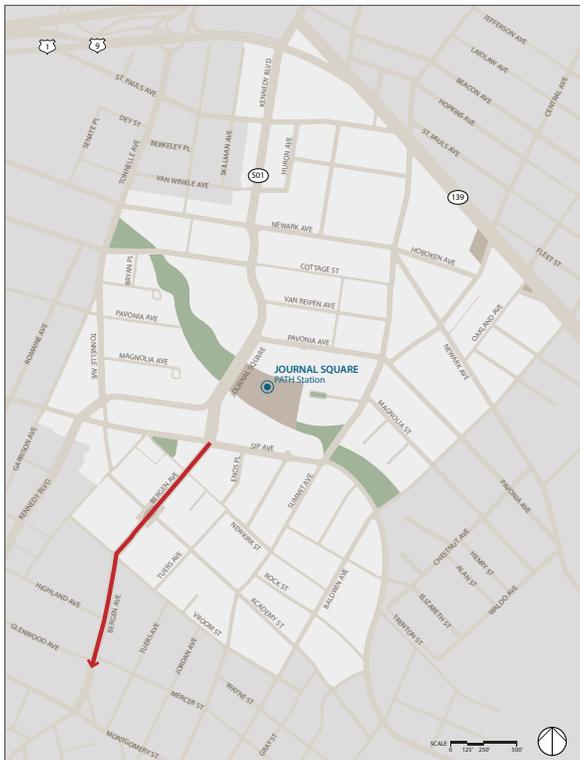
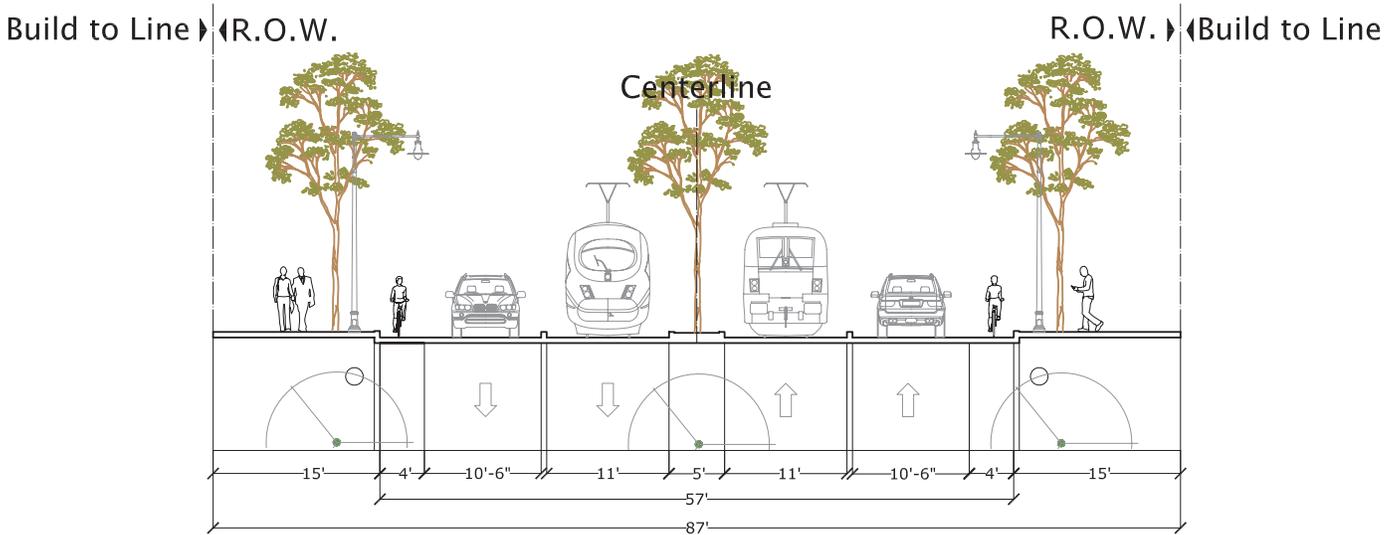




EXHIBIT 27 - SECONDARY STREET D ST:87:57:BL:TR



Secondary D ST:87:57:BL:TR	
Travel Lanes	2
Lane Width	10 feet 6 inches
Transit Type	Streetcar
Transit Lane Width	11 feet
Design Speed	30 mph
Bike Lane Width	4 feet
Sidewalk Width	15 feet
Light Type	Short Bracket
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	3.5 inches
Street Tree Type	Medium Shade Tree



An urban tertiary street system is designed to carry moderate volumes of traffic with the emphasis on land access and very low traffic mobility. It should be a link to other street systems and provide direct access to adjacent land uses. An alternative route is desirable for through traffic. This section provides specifications for all Tertiary Streets in the Journal Square Area.

STANDARDS FOR TERTIARY STREETS

- A. All Tertiary Street travel lanes shall be 10 feet in width. Vehicular travel lanes shall be constructed of asphalt or concrete.
- B. Sidewalks along Tertiary Streets must be at least 12 feet in width to provide adequate space for pedestrians. At a minimum, sidewalks shall be constructed in three foot scored blocks made of concrete with brick edges and dividers. Asphalt is strictly prohibited for sidewalks on Tertiary Streets. Alternative paving materials shall include, bluestone, and approved gray concrete pavers. No more than three (3) paving materials or colors shall be used per block.
- C. Crosswalks on Tertiary Streets shall be clearly striped at all intersections and wherever pedestrian traffic conflicts with vehicular traffic.
- D. All trees shall be a minimum of 3 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting.
- E. Small - Medium sized street tree species are required along Tertiary streets. Small - Medium size trees shall be spaced every 20 feet on center along streets and park edges.
- F. All Tertiary Street trees shall be planted within a planting well and shall be surrounded by vegetative ground cover protected with a raised curb or decorative fencing 6-10 inches in height. Tree pits shall be a minimum of 4-5 feet from curb to sidewalk and 4-6 feet in width.
- G. A unified standard for street lighting along Tertiary Streets must be used.
- H. Tertiary Streets shall have post top lamps that should be 12-14 feet in height. Street lights on Tertiary Streets shall be spaced 40 feet on center.
- I. Tertiary Streets A and C are subdivided by traffic direction as noted on the title block for each section.

Examples of Tertiary Streets

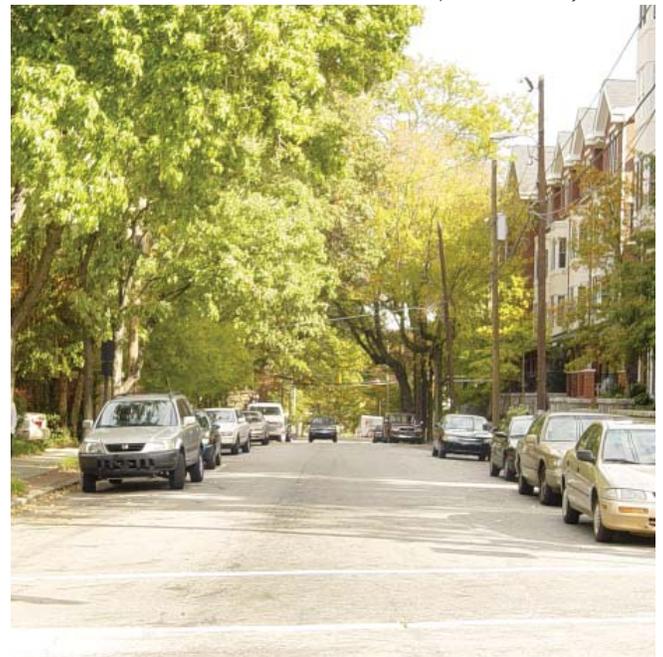




EXHIBIT M - TERTIARY STREETS

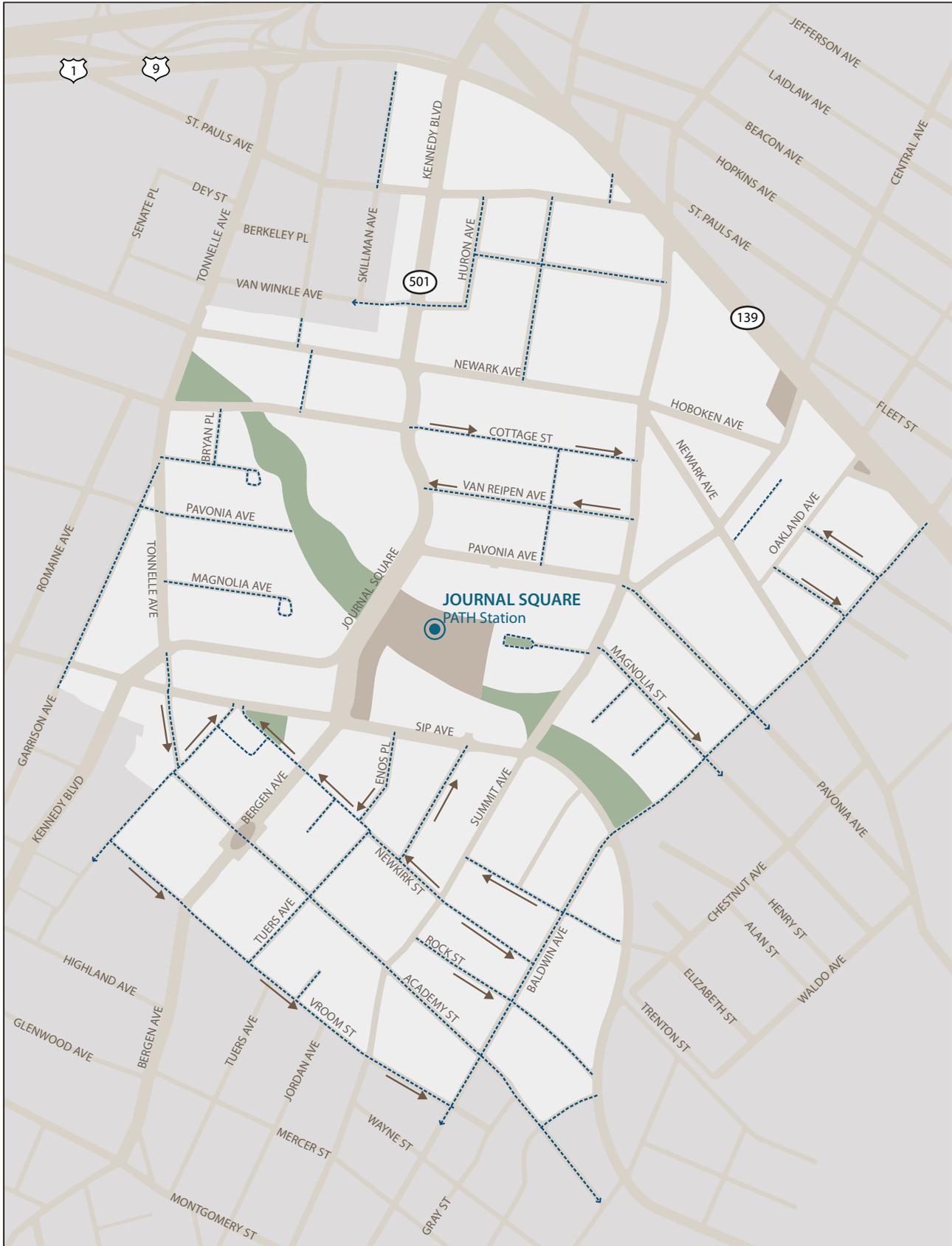
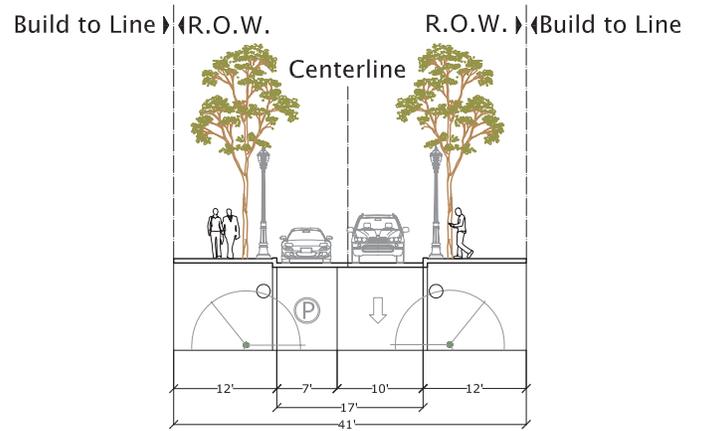
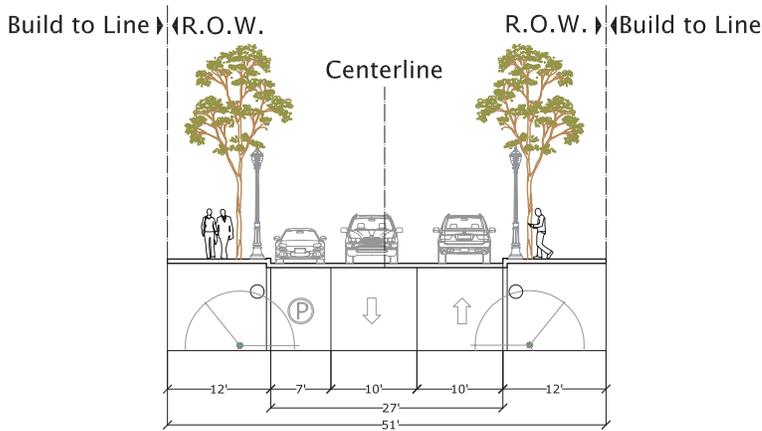


EXHIBIT 30 - TERTIARY STREET A
ST:51:27 - Two Way

EXHIBIT 31 - TERTIARY STREET A'
ST:41:17 - One Way



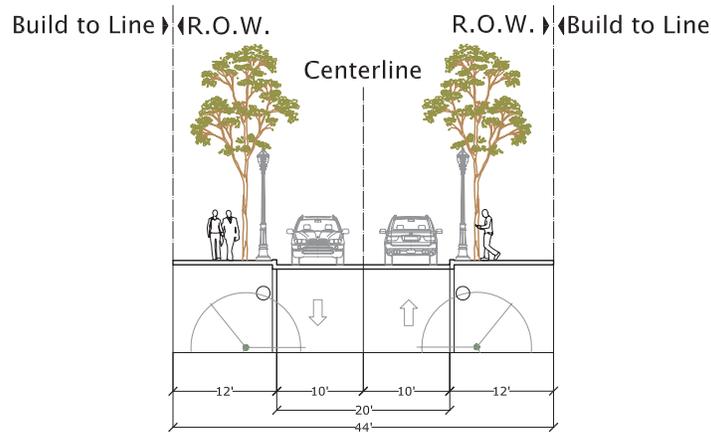
Tertiary A ST:51:27 (ST:41:17*)	
Travel Lanes	2 (or 1 one way*) lane(s)
Lane Width	10 feet
Design Speed	20 mph
Parking Lane	one side
Parking Lane Width	7 feet
Sidewalk Width	12 feet
Light Type	Post Top
Light Spacing	40 feet
Street Tree Spacing	20 feet
Street Tree Caliper	3 inches
Street Tree Type	Small-Medium Trees

*Section A'





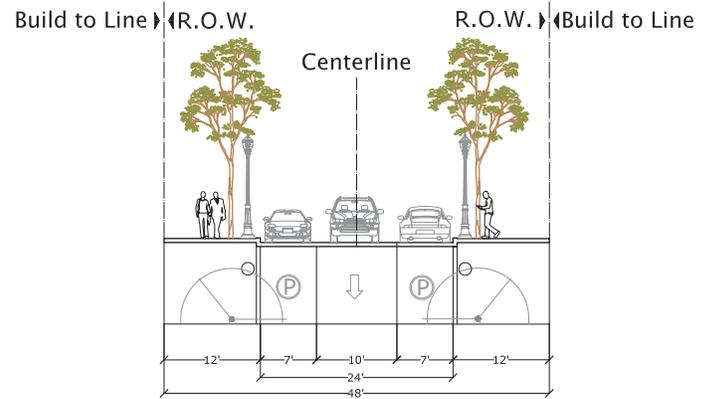
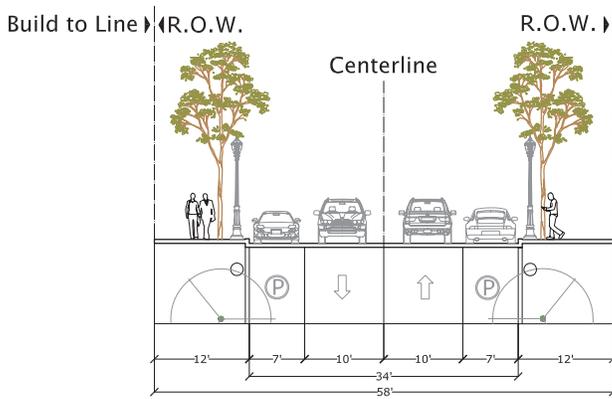
EXHIBIT 32 - TERTIARY STREET B ST:44:20



Tertiary B ST:44:20	
Travel Lanes	2
Lane Width	10 feet
Design Speed	20 mph
Sidewalk Width	12 feet
Light Type	Post Top
Light Spacing	40 feet
Street Tree Spacing	20 feet
Street Tree Caliper	3 inches
Street Tree Type	Small-Medium Trees

EXHIBIT 33 - TERTIARY STREET C
ST:58:34 - Two Way

EXHIBIT 34 - TERTIARY STREET C'
ST:48:24 - One Way



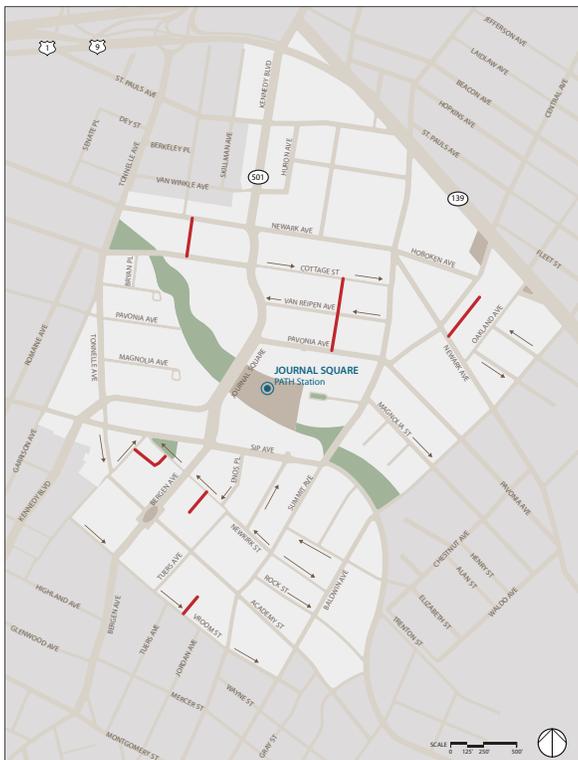
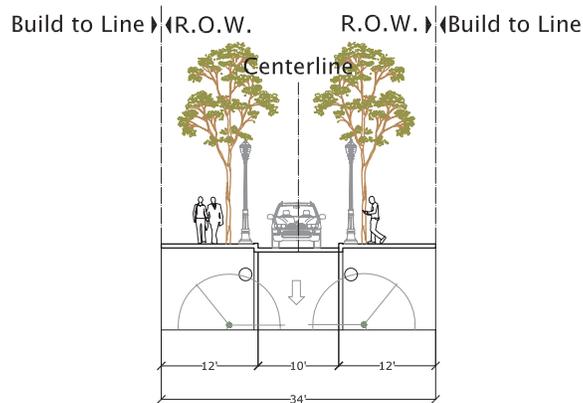
Tertiary C ST:58:34 (ST:48:24*)	
Travel Lanes	2 (or 1 one way*) lane(s)
Lane Width	10 feet
Design Speed	20 mph
Sidewalk Width	12 feet
Parking Lane	two side
Parking Lane Width	7 feet
Light Type	Post Top
Light Spacing	40 feet
Street Tree Spacing	20 feet
Street Tree Caliper	3 inches
Street Tree Type	Small-Medium Trees

*Section C'





EXHIBIT 35 - TERTIARY STREET D ST:34:10



Tertiary D ST:34:10	
Travel Lanes	1 one way lane
Lane Width	10 feet
Design Speed	15 mph
Sidewalk Width	12 feet
Light Type	Post Top
Light Spacing	40 feet
Street Tree Spacing	20 feet
Street Tree Caliper	3 inches
Street Tree Type	Small-Medium Trees



The Pedestrian Priority Street proposed for the Journal Square Area combines a Bus Plaza and Pedestrian/ Bus Street. Combining pedestrianism and transit in this central area will allow for greater flow of pedestrian traffic between both sides of the street. This section provides specifications for all Pedestrian Priority Streets in the Journal Square Area.

STANDARDS FOR PEDESTRIAN PRIORITY STREETS

- A. The proposed Pedestrian Priority Street is to function as the major retail corridor as well as provide pedestrian, bus, and taxi access from the Journal Square Plaza and PATH Station to Tonnelle Avenue.
- B. The Pedestrian Priority Street shall not provide any vehicular access, with the exception of NJ Transit Buses, Taxis, Emergency Vehicles, and light deliveries that support the day-to-day functioning of the building with which they are associated.
- C. The Pedestrian Priority Street must have a continuous paving that extends from building edge to building edge.
- D. The paving material used for pedestrian priority streets shall be complementary of materials used in the Journal Square Plaza and the City Center Park, such as granite, cobblestone or pavers, or stamped concrete. Asphalt is prohibited.
- E. Pedestrian Priority Street C shall have designated pedestrian crosswalks that must be clearly distinguishable. These crosswalks shall be paved with materials complementary of the materials used in the Journal Square Plaza as described in the previous regulation.
- F. Travel lanes for bus access shall be between 11 to 12 feet in width.
- G. All trees shall be a minimum of 4 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting. Trees shall be maintained to a minimum of 12 feet in height where they are adjacent to bus/ transit lanes.
- H. Medium-Large street tree species are required along Pedestrian Priority Streets. Medium-Large trees shall be spaced every 25 feet on center along streets and park edges.
- I. A unified standard for street lighting in the Pedestrian Priority Street area must be used.
- J. Pedestrian Priority Streets shall have post top lamps that should be 12-14 feet in height. Street lights on Pedestrian Priority Streets shall be spaced 50 feet on center.



Example of a Pedestrian Priority Street



EXHIBIT N - PEDESTRIAN PRIORITY STREETS

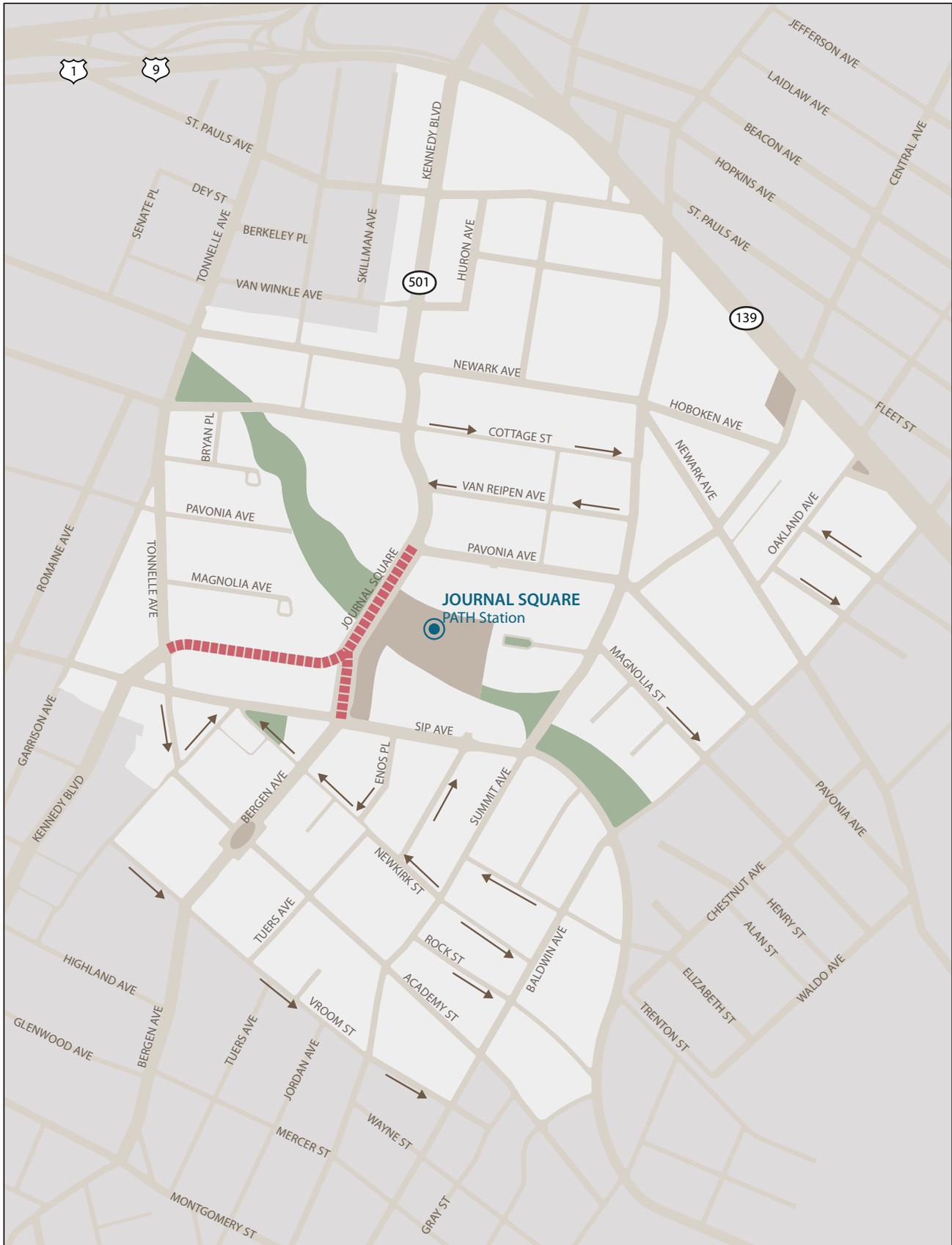
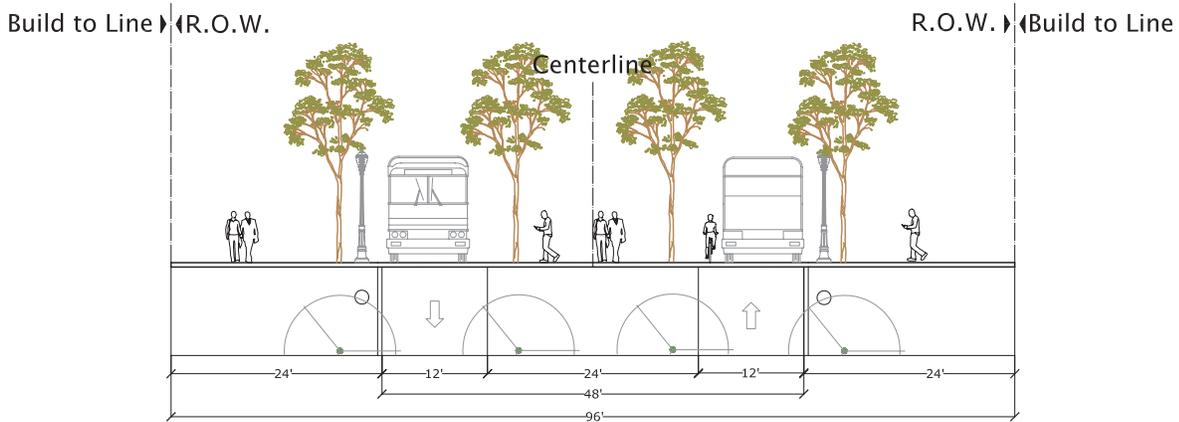
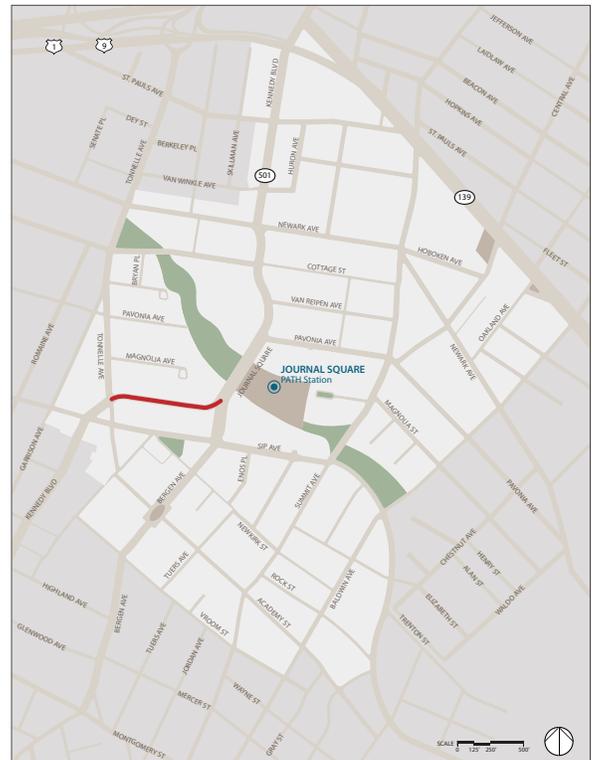




EXHIBIT 36 -PEDESTRIAN PRIORITY A ST:96:48:BR:TR



Pedestrian Priority A ST:96:48:BLVD:BR:TR	
Transit Type	Bus/Taxi/Bicycle
Transit Lane Width	12 feet
Design Speed	15 mph
Sidewalk Width	24 feet
Light Type	Post Top
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree





PEDESTRIAN PRIORITY A - PHASING SIMULATION



EXISTING CONDITIONS

Existing Street Section - two lanes of traffic in either direction with on-street parking/turning lanes. Sidewalks are approximately 16 feet wide from building edge to curb.



PEDESTRIAN PRIORITY A - PHASE ONE

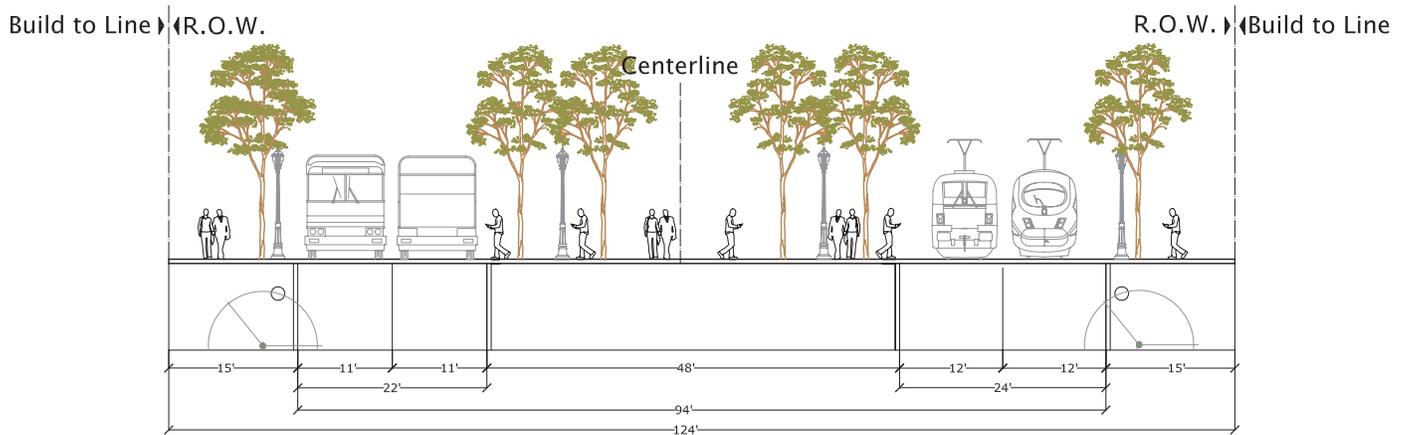
Phase One of converting this portion of Kennedy Boulevard into a Pedestrian Priority Street is to expand the sidewalks to 24 feet in width. Street trees will be planted along this new curb edge in accordance with the regulations for Pedestrian Priority Streets. The cartway will be reduced to two lanes of traffic in either direction. Each travel lane should be approximately 12 feet wide.



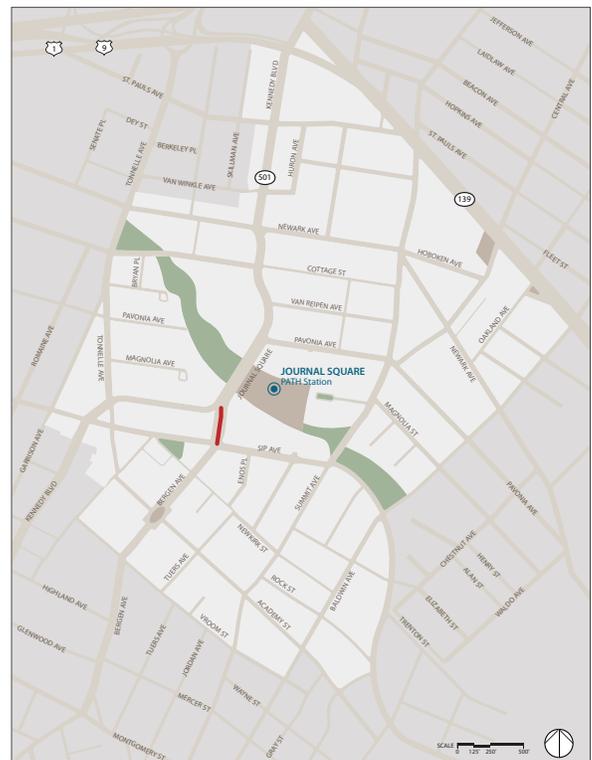
PEDESTRIAN PRIORITY A - COMPLETED

Completed Pedestrian Priority Street A will be a vibrant retail area with bus and taxi access. Pedestrians will be able to freely cross from one side of the street to the other without having to cross traffic.

EXHIBIT 37 - PEDESTRIAN PRIORITY B ST:124:94:BR:TR



Pedestrian Priority B ST:124:94:BLVD:BR:TR	
Transit Type	Bus/Streetcar/Taxi/Bicycle
Transit Lane Width (Bus)	11 feet
Transit Lane Width (Streetcar)	12 feet
Design Speed	15 mph
Sidewalk Width	15 feet
Light Type	Post Top
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree





PEDESTRIAN PRIORITY B - PHASING SIMULATION

EXISTING CONDITIONS



Existing Street Section - three lanes of traffic in either direction with turning lanes and a center median. Sidewalks are approximately 14 feet in width and the median is currently 18 feet in width.



PEDESTRIAN PRIORITY B - PHASE ONE



Phase One of converting this area of Journal Square into a Pedestrian Priority Street will be to convert one side of the median into a pedestrian and transit area with no vehicular traffic. The other side of the median will remain open to traffic with three travel lanes.

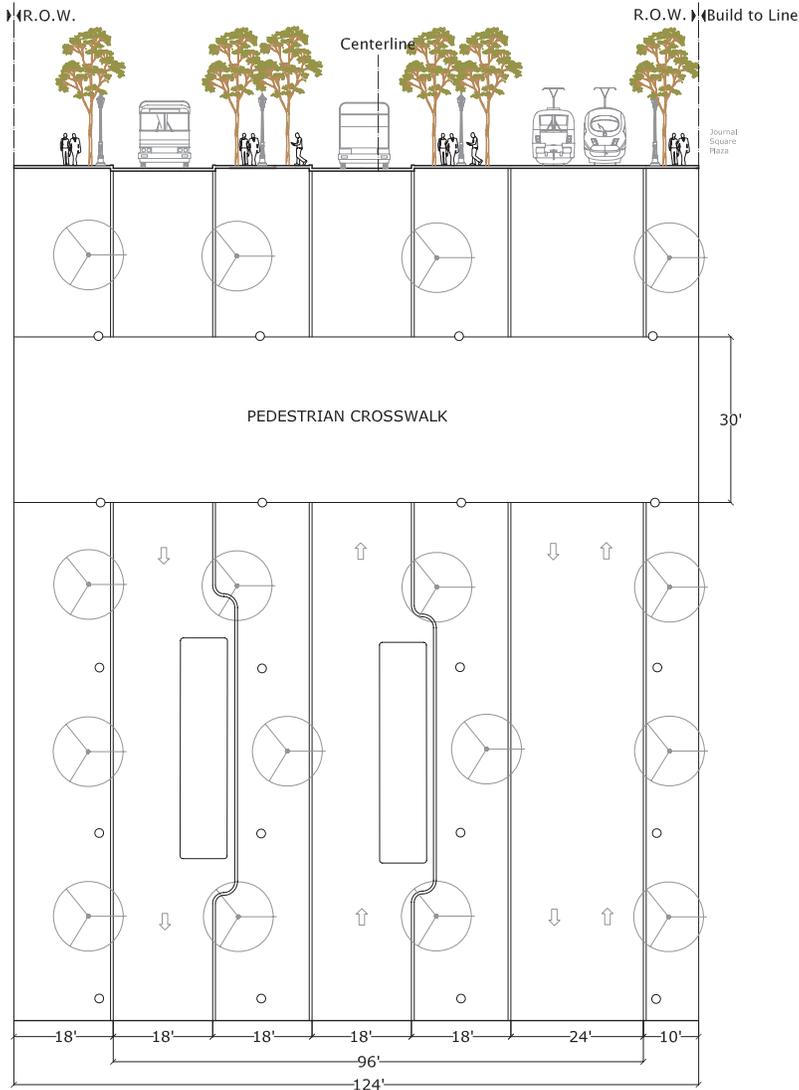


PEDESTRIAN PRIORITY B - COMPLETED

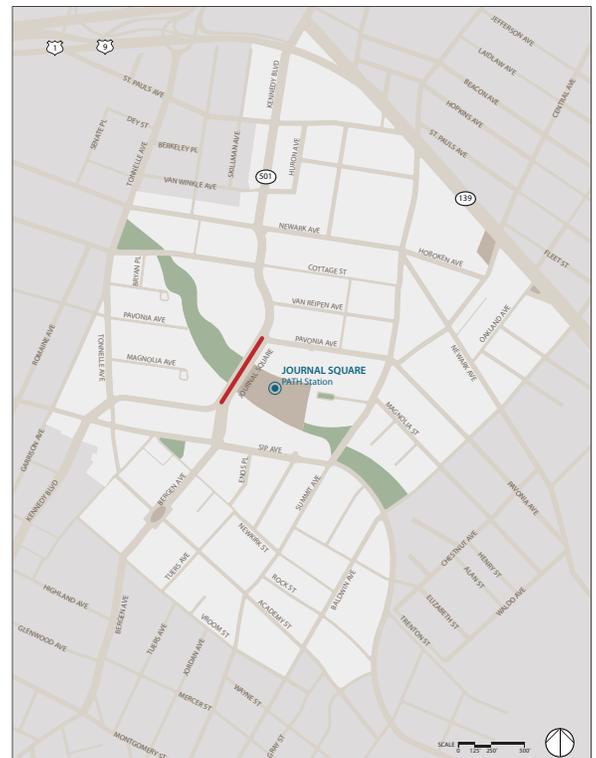


Completed Pedestrian Priority Street B will primarily be a pedestrian area. Transit in the form of streetcars, buses, and taxis will still have through access.

EXHIBIT 38 - PEDESTRIAN PRIORITY C ST:124:96:BR:TR



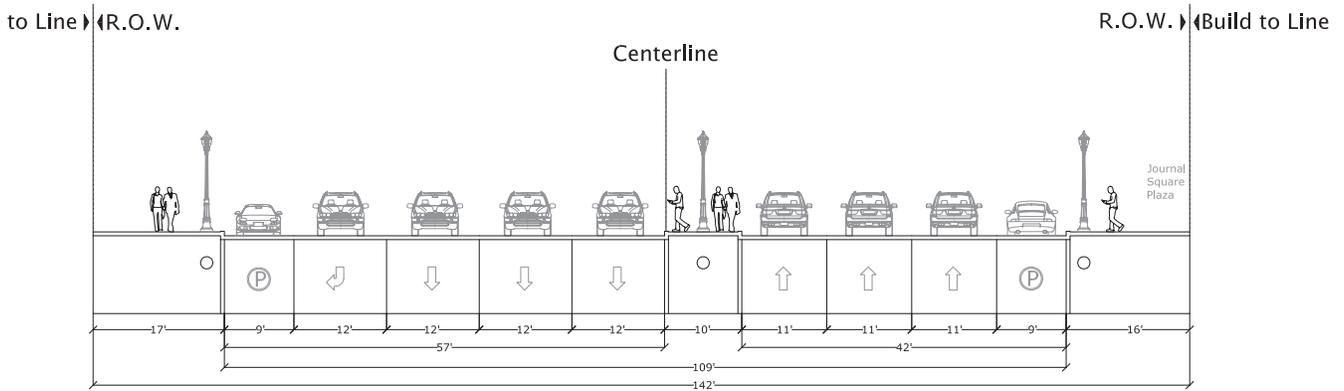
Pedestrian Priority C ST:124:96:BLVD:BR:TR	
Transit Type	Bus/Streetcar/Taxi/Bicycle
Transit Lane Width (Bus)	18 feet
Transit Lane Width (Streetcar)	12 feet
Design Speed	15 mph
Sidewalk Width	18 feet/10 feet
Light Type	Post Top
Light Spacing	50 feet
Street Tree Spacing	25 feet
Street Tree Caliper	4 inches
Street Tree Type	Medium-Large Shade Tree



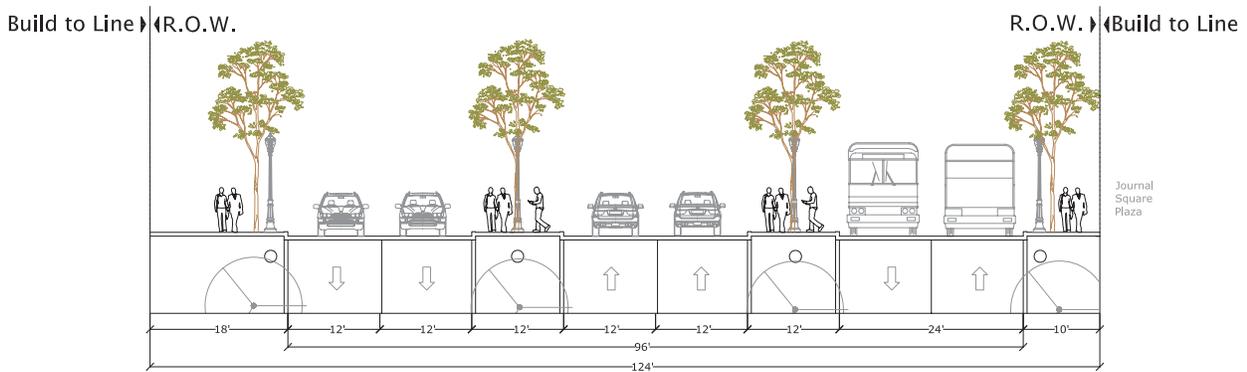


PEDESTRIAN PRIORITY C - PHASING OPTIONS

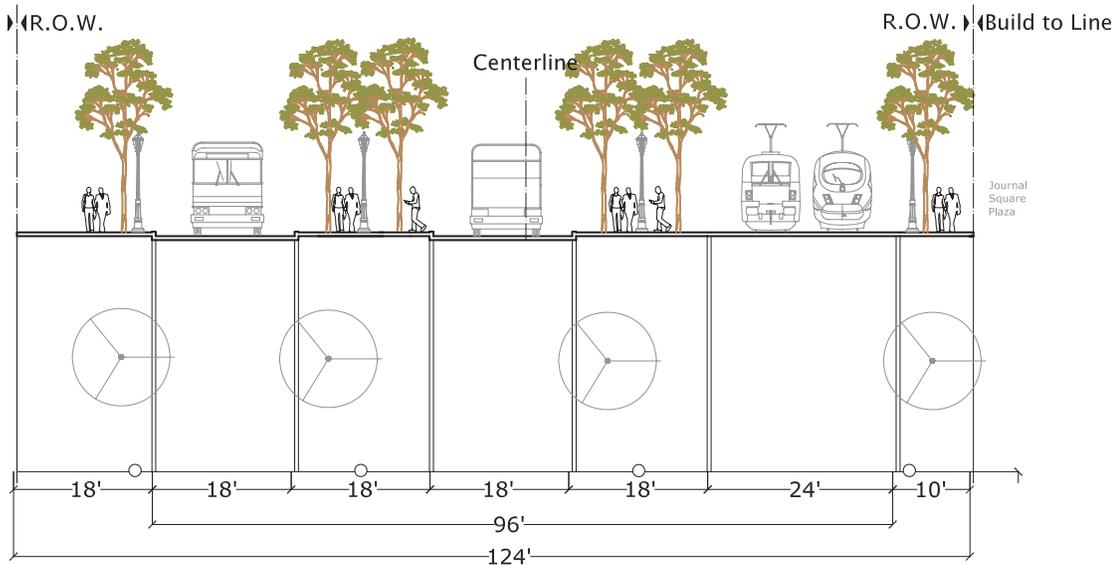
EXISTING CONDITIONS



PEDESTRIAN PRIORITY C - PHASE ONE



PEDESTRIAN PRIORITY C - COMPLETED



Redevelop Journal Square

Section 7 Landscape



View of the Statue of Liberty from Liberty State Park



Cityscape view from Liberty State Park's River Walk.



Lincoln Park - Hudson County Park

The City of Jersey City has approximately 1,550 acres of parks and Open Space that includes State, County, and Municipal parks. In addition to Liberty State Park, a national and international tourist destination, there are over 50 municipal parks of all sizes spread throughout the city. In this highly urbanized area green space provides a much needed human connection with the natural environment. Preserving Open Space, providing connections between existing parks, and adding additional green space to the Area should be a priority.

Proposed Garden State Greenway connector routes identify linear connections between natural resource hubs. Any future development should take these routes into consideration in order to build upon a statewide vision to provide access to parks and natural areas, farmland, and historic places. Another possible green connection through the Area can be provided by utilizing portions of the Bergen Arches and the Embankment that are shown in orange on the map to the right. This route has been identified by East Coast Greenways as a connection across Jersey City from the waterfront to Secaucus. The City Center Greenway Connection, shown as orange circles on the map, will tie City Center Park to this East Coast Greenway Route to the waterfront.

The purpose and intent of this section is to provide landscape standards for the Area that will help maintain and protect property values, enhance the appearance of the development, protect the aesthetic assets of the community, reduce erosion and storm water run off, reduce the urban heat island effect, and provide screening where necessary. The landscape requirements of this section are minimum standards; additional landscaping is encouraged.

The map to the right shows how landscape improvements in the Journal Square Area will relate to the current regional park and open space system. Streetscaping improvements will connect Journal Square to existing parks. The City Center Park, will contribute approximately 9 acres of green space to this system. Public plazas and pocket parks will also be incorporated with new development to provide a comprehensive set of amenities to residents and visitors.



EXHIBIT O - GREEN REGIONAL CONNECTIONS





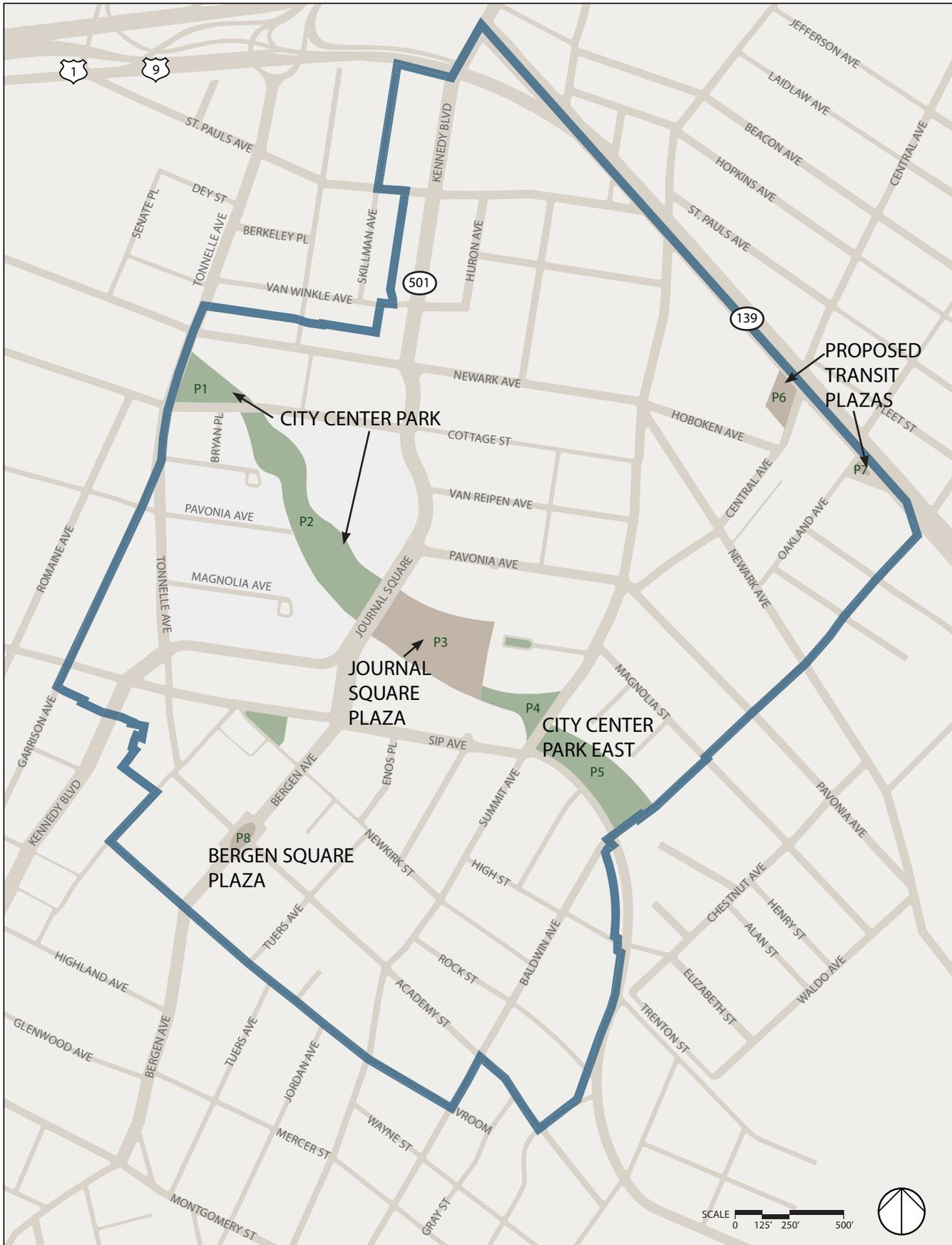
All Landscape Plans shall be subject to approval by the Planning Board. The Planning Board may forward the Plan to the City Engineer for review and comment. The Plan shall conform to the following requirements and guidelines:

GENERAL LANDSCAPE STANDARDS

- A. A detailed Landscape Plan in accordance with the unified Streetscape Plan shall be prepared by a certified landscape architect for all Site Plan proposals.
- B. The landscape plan shall include the highest quality materials and, at minimum, specify type and color of pavers and other hardscape materials, type and quality of decorative lighting fixtures, specific color and material of decorative site furnishings, as well as locations and quantities of each. The landscape plan shall also include species, sizes, and planting plans for all vegetation.
- C. Native plants should be used before other alternatives. A list of examples is provided at the end of this section.
- D. Within an overall Landscape Plan, a Streetscape Plan shall be required. The Streetscape Plan shall be submitted to the Jersey City Planning Board for its review and approval in conjunction with the project Site Plan application and implemented contemporaneously with the construction of the redevelopment project. All streetscape and semipublic edge landscaping must be completed before certificate of occupancy can be granted.
- E. All parks, greens, plazas, and pedestrian ways must be dedicated in perpetuity as their defined use and shall be permanently maintained by the owners of adjacent buildings. These open spaces shall be designed to become a focal point of the Area and be accessible to residents within and adjacent to the Area.
- F. Rain water management shall be integrated into the site design. Rain water should be captured and stored for site irrigation wherever possible.
- G. Public spaces should accommodate a variety of activities for both individuals and large groups. Within these spaces a balance of shaded and open sun seating shall be provided. Sun exposure upon the plazas and courtyards must also be considered to ensure the comfort of users.
- H. Views to, within, and beyond these spaces must be addressed during their final planning and designed to aesthetically enhance and benefit the plazas as well as the surrounding areas.
- I. Appropriate area for recreation must be provided based on the city regulations.
- J. Park space is required on development blocks P1, P2, P4, and P5 as designated in Exhibit 39 – Park and Plaza Block Designation shown to the Right. Plaza space is required on development blocks P3, P6, P7, and P8. Public Market Space must be provided on or adjacent to block P3.
- K. A public art and performance component must be developed in conjunction with the Landscape Plan and should include provisions for a public amphitheater on Block P2 using the side of the Historic Loews Theater.



EXHIBIT 39 - PARK AND PLAZA BLOCK DESIGNATION





PARK AND PLAZA DIMENSIONS

This map illustrates the location of the City Center Park and Journal Square Plaza. The edges of this spine are dimensioned and represent the minimum dimensions. The City Center Park space can be larger, but not smaller than what is dimensioned. Dimensions have been taken from the centerlines of adjacent streets. On block P6 must be continuous between Summit Avenue and Baldwin Avenue. The park space on block P5 shall be a minimum of 135 feet wide. The stated dimensions shall be met and shall not deviate more than 10 feet.

PLAZA AND OPEN SPACE STANDARDS

- A. Plaza spaces shall be paved with a combination of concrete, granite, brick, pavers, or bluestone. Asphalt is prohibited in plaza spaces and in the pedestrian priority areas.
- B. Pathways within the City Center Park are encouraged to be constructed of recycled materials and shall be no narrower than five feet in width.
- C. All plaza spaces shall have attractive drainage solutions that utilize clean runoff for irrigation.
- D. Site grading and permeable surfaces shall promote maximum return of clean rainwater within parkland and green spaces, with flat areas graded to a 2% minimum.
- E. Structures such as pergolas and awnings shall be incorporated to provide shade for visitors.
- F. A large water feature such as a reflecting pool is required on block P3.
- G. All street furniture shall be a consistent and uniform design vocabulary to establish visual continuity throughout the Area.
- H. Vending racks and sidewalk dining may encroach on the majority of the sidewalk providing that a clear aisle of five feet is maintained.
- I. Street Vendors are allowed in all parks and plazas with permission of the City.
- J. All Plaza space shall be lit with energy efficient lights that are dark sky compliant. The control of light pollution to surrounding areas shall be in accordance with the lighting requirements of the Jersey City Zoning Plan, Jersey City Ordinances, or Land Development Regulations.
- K. Low level lighting should be incorporated into plaza spaces that provides visual interest and addresses safety concerns at night.
- L. Metal Halide (or comparable light quality) lamps are strongly suggested because of their efficiency and light quality.
- M. Mercury Vapor and high pressure sodium lighting shall not be allowed.
- N. Cobra Head Light Fixtures are forbidden.
- O. A unified lighting standard must be used within each plaza and must be approved by the City.
- P. All open space areas in plazas and courtyards shall be landscaped with appropriate plant material including shade trees, flowering trees, evergreen and deciduous shrubs, and perennials and bulbs, unless said spaces are paved to allow for pedestrian activities.
- Q. The landscape plan for all parks and plazas shall consider sun exposure and provide adequate protection through year round vegetation.
- R. All landscaped areas within any park or plaza must be well maintained, cleared, clipped and pruned to provide a positive healthy visual character.
- S. All trees planted within parks and plazas shall be a minimum of 4 inches in caliper measured at 6 inches from ground level and have a minimum branch height of 8 feet at time of planting.



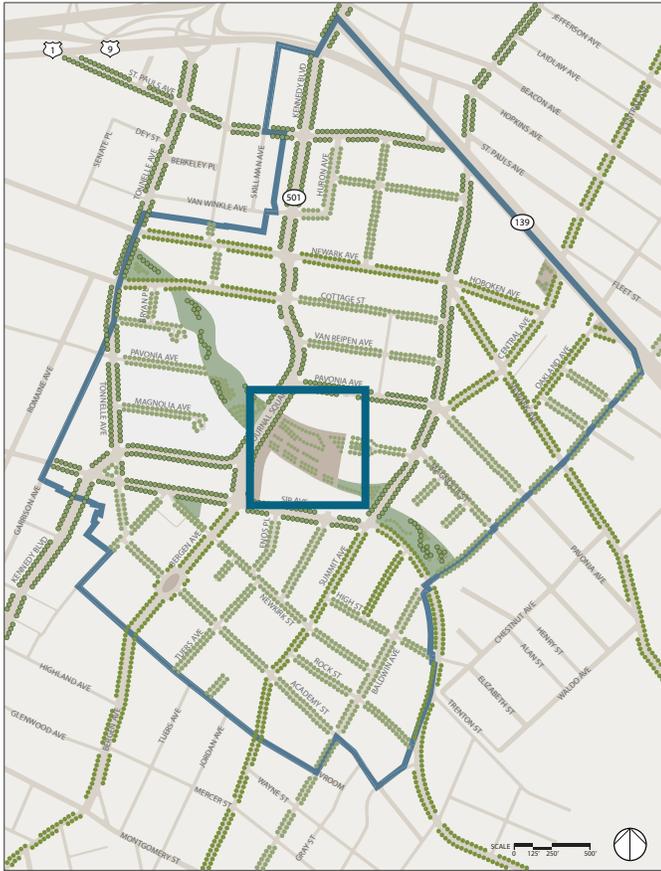
The illustrative landscape plan for the Area includes a large linear park, revitalized Journal Square Plaza, and an extensive streetscape network. Park space is required on development blocks P1, P2, P4, and P5. Plaza space is required on development blocks P3, P6, P7, and P8. Appropriate area for recreation must be provided based on the city regulations. The Landscape Plan to the right illustrates the parks, plazas, and street trees located within the Rehabilitation Area. For streetscape requirements see Section 6 – Streets and - Plant Materials at the end of this section.





EXHIBIT P - ILLUSTRATIVE LANDSCAPE PLAN





JOURNAL SQUARE PLAZA

A large public plaza space will be the centerpiece of the Journal Square Redevelopment Plan. This space will provide an attractive entrance and exit from a new day lit Journal Square PATH Station. Large walkways will accommodate high pedestrian flow from the new PATH station and adjacent Port Authority Bus Terminal. A large water element is incorporated into the design that will consist of a reflecting pool and fountain features. The addition of green space and trees to the Area will create a more pedestrian friendly environment.

This space will serve as a central meeting place for the entire Rehabilitation Area. From this state of the art public space pedestrians can easily access a pedestrian priority shopping area and light rail connections to other neighborhoods in the Area. A large public park will branch off this revitalized plaza that residents and visitors will be able to enjoy.

This plan provides space for a public market within its central core. A successful public market can become a catalyst for improving the life and foot traffic of an area. Farmer's Markets are real places that attract a broad diversity of people. The existing Journal Square market should be considered in any new development in the Area. The use should be preserved and space should be provided that appropriately serves the needs of the community.

Allee of trees leading out to main plaza

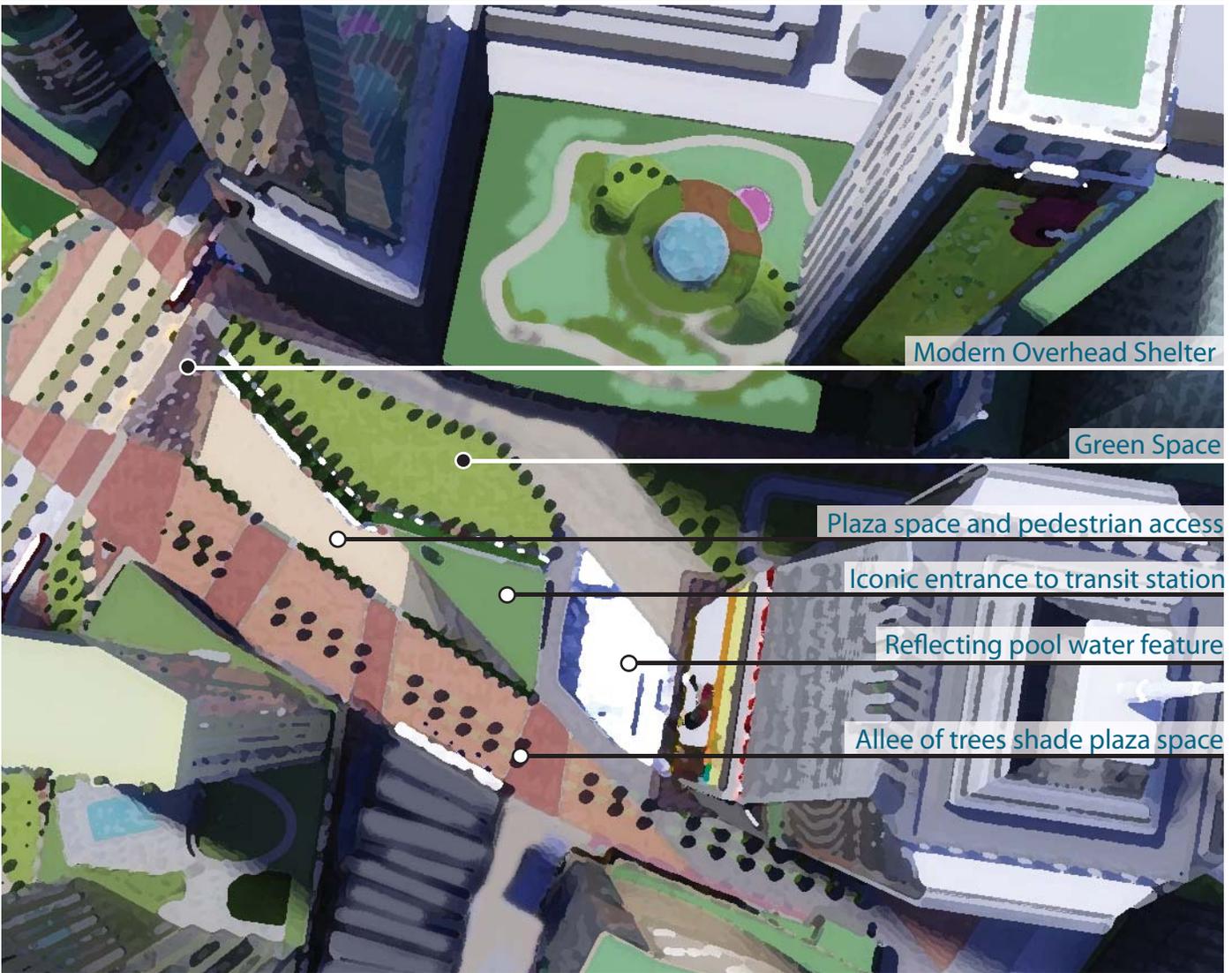


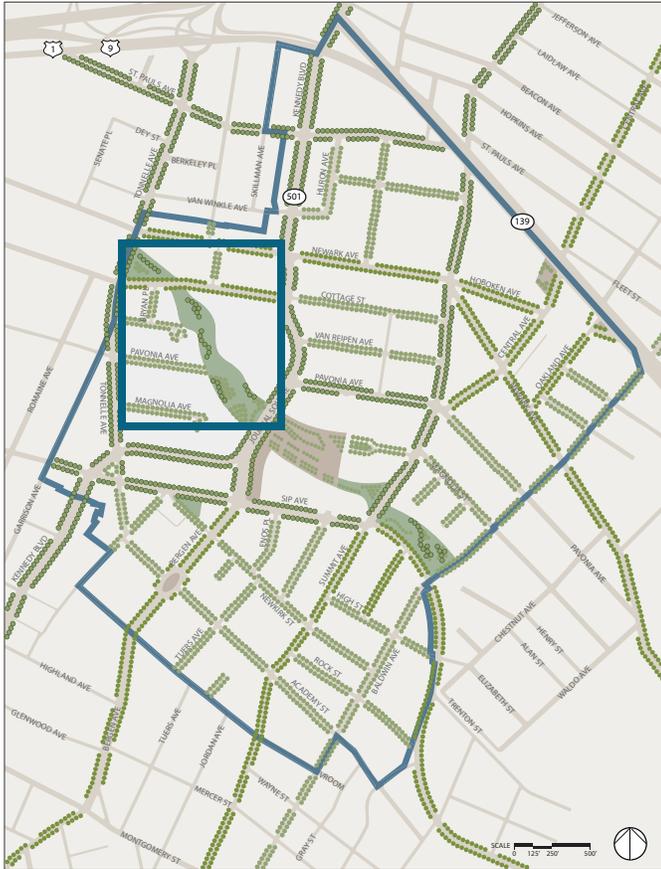
Modern Overhead Shelter in Journal Square Plaza





EXHIBIT Q - JOURNAL SQUARE PLAZA





CITY CENTER PARK

Reclaimed developable land that is currently rail tracks will be transformed into a beautiful public park. This new open space will provide residents and visitors with much needed green space in a highly urban environment. This linear space will also serve as a pedestrian connection to the PATH Station for residents of adjacent neighborhoods. The park will have a variety of pathways, seating, plantings, trees, and open lawn space that will be flexible to accommodate a diverse program of activities. The park will be framed by exciting new development consisting of residential towers, office space, and retail uses so there will be eyes on the space at all times. Residents, commuters, and visitors, will be able to enjoy this new park space that will be conveniently located near transit, offices, and a variety of shopping.

POCKET PARKS

The development of Pocket Parks is encouraged throughout the Area. Pocket parks are closely tied to the neighborhoods they serve. By nature, they tend to be scattered and disconnected because they are usually created opportunistically, but with some planning they can be connected to greenways or bike paths to attract more potential users. Pocket parks can meet a variety of needs such as small event space, play area for children, space for relaxing or meeting friends, taking lunch breaks, etc. Greenery within pocket parks can help regulate microclimates and act as the “lungs” of a city. Transforming a selection of vacant lots into pocket parks, and incorporating pocket parks into future development plans will help improve the character of the entire Area.

Grove of trees along the historic Loews Theater

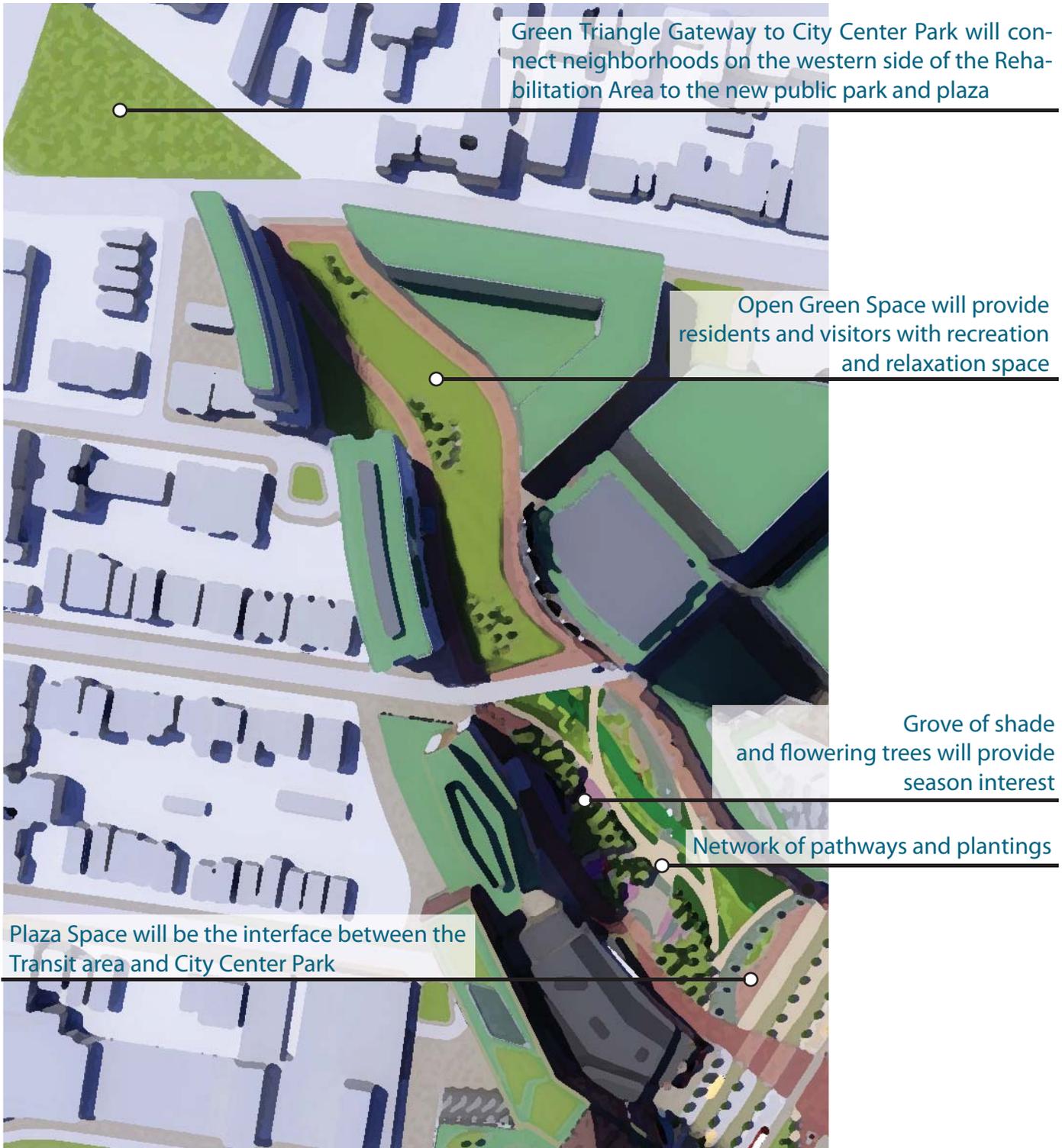


A series of pathways, plantings, and lawn space in City Center Park





EXHIBIT R - CITY CENTER PARK



Native tree species with proven success in urban areas shall be thoroughly searched by a plant broker before consideration of alternate species. Plants should be chosen that provide a variety of seasonal interests since these public spaces will be used year round. Larger tree species should be planted on Primary Streets, medium size trees should be planted on Secondary Streets, and smaller trees should be planted on Tertiary Streets. Street tree species shall be consistent within each block. See Exhibit 41 – Plant List Below and Exhibit 42 – Street Tree Plan (to the right) for planting locations and planting details, all trees must be planted in accordance with the Standards found in Section 6 – Streets, and the Landscape Standards found in this section.

EXHIBIT 41 - PLANT LIST			
LARGE TREES - Mature height greater than 50 feet tall			
TREE SPECIES		SHAPE	NOTES
SCIENTIFIC NAME	COMMON NAME		
<i>Faxinus pennsylvanica</i>	Green Ash	Pyramidal	
<i>Ginkgo biloba</i>	Ginkgo	Upright	Plant Male Only
<i>Gleditsia triacanthos inermis</i>	Honey locust	Irregular	
<i>Quercus phellos</i>	Willow Oak	Rounded	Plant Spring Only
<i>Styphnolobium japonicum</i>	Scholar tree	Rounded	
<i>Tilia x euchlora</i>	Crimean Linden	Rounded	Pest Resistant
<i>Zelkova serrata</i>	Japanese Zelkova	Vase-Like	
MEDIUM TREES - Mature height between 35 feet and 50 feet tall			
TREE SPECIES		SHAPE	NOTES
SCIENTIFIC NAME	COMMON NAME		
<i>Carpinus caroliniana</i>	American Hornbeam	Rounded	Plant Spring Only
<i>Ostrya virginiana</i>	American Hophornbeam	Rounded	Plant Spring Only
<i>Pyrus calleryana</i>	Callery Pear	Rounded	Do not plant 'Brandford' Cultivars 'Cleveland Select' is Structurally Superior
SMALL TREES - Mature height less than 35 feet tall			
TREE SPECIES		SHAPE	NOTES
SCIENTIFIC NAME	COMMON NAME		
<i>Amelanchier canadensis</i>	Serviceberry	Rounded	'Autumn Sunset,' 'Cumulus,' and 'White Pillar' are particularly suited for street trees
<i>Cercis canadensis</i>	Eastern Redbud	Irregular	
<i>Prunus cerasifera</i>	Purpleleaf Plum	Rounded	'Atropurpurea' and 'Thundercloud' Recommended
<i>Prunus virginiana</i> 'Schubert'	Schubert Cherry	Pyramidal	Leaves are Maroon Most of Summer
<i>Syringa reticulata</i>	Japanese Tree Lilac	Rounded	Disease-resistant, fragrant flowers
SHRUBS AND GRASSES - Parks, Plazas, Open Spaces			
SPECIES		Type	NOTES
SCIENTIFIC NAME	COMMON NAME		
<i>Aronia melanocarpa</i>	Black Chokeberry	Shrub	Native Species
<i>Calamagrostis canadensis</i>	Blue Joint Grass	Grass	Native Species
<i>Gaultheria procumbens</i>	Wintergreen	Shrub	Native Species
<i>Illex verticillata</i>	Winter Berry	Shrub	Native Species
<i>Illex opaca</i>	American Holly	Shrub	Native Species
<i>Schizachyrium scoparium</i>	Little Blue Stem Grass	Grass	Native Species
<i>Viburnum acerifolium</i>	Maple Leaf Viburnum	Shrub	Native Species
<i>Viburnum nudem</i>	Poosumhaw Viburnum	Shrub	Native Species
<i>Viburnum trilobum</i>	Cranberry Bush	Shrub	Native Species



EXHIBIT 42 - STREET TREE PLAN



7.6 Soil Handling and Compaction Standards



- A. All areas to be landscaped should receive topsoil that is friable, fertile, natural loam, free of subsoil, stone, roots, noxious plants and extraneous matter to a depth of 5 feet from finished grade as a subsoil cap and new planting root growth zone.
- B. Soil must be loosely compacted and protected from heavy compaction by equipment and stockpiling of materials by barriers during construction.
- C. An expanded slate soil admixture can be added to the sub-base for modular paving over planting trenches between trees in plaza space to compensate for compaction and promote root growth.
- D. The soil shall be thoroughly watered and lightly tamped as back filling progresses.
- E. A minimum depth of 6 feet of structure free zone must be provided for all trees planted in the Rehabilitation Area.
- F. Where heavy pedestrian circulation is expected, a concrete slab covered with decorative paving that bridges over a planting trench is required to provide a suitable pedestrian surface while preventing soil compaction in the tree trench.
- G. No planting well or strip shall be left exposed in order to prevent damage to tree and to prevent soil compaction.
- H. Where tree pits will be mulched, mulch must be 2"-4" deep. Mulch shall be kept 6" from the trunk of the tree.



7.7 Planting Standards

- A. Existing trees in the Area must be evaluated by an arborist and proper maintenance performed to ensure long term health of the tree.
- B. All plants shall be drought tolerant in order to reduce the need for irrigation.
- C. All landscape materials must have a one year maintenance guarantee. If any planting material dies within one year of planting, they must be replaced during the following planting season according to the planting guidelines called for in this Plan.
- D. All boulevard street tree plantings shall have surface watering/fertilizing access pipes and subsurface drainage outlets.
- E. All sodded areas, planting beds, and street tree planting strips shall have in ground irrigation systems.
- F. All plants within rooftop gardens can experience a high evaporation rate due to the drying effects of wind and sun. Irrigation, mulches and moisture-holding soil additives shall be added to help reduce this moisture loss.
- G. Trees should only be planted during appropriate spring and fall planting seasons to the highest arboricultural industry standards; appropriate root barriers shall be installed at the time of planting.
- H. All trees shall be monitored and treated annually by the developer for potential disease or decline in physical condition.
- I. Street tree planting should be strategically phased to ensure procurement of large quantities of uniform and consistently sized specimens of specifically selected species.
- J. Native tree species with proven success in urban areas shall be thoroughly searched by a plant broker before consideration of alternate species. (Suggested native tree species can be found at the end of this section.)
- K. Street tree species shall be consistent within each block.
- L. Suggested shrubs and grasses should be used in conjunction with other native, non invasive, vegetation.
- M. Trees in the suggested plant list have been categorized into small medium and large tree types that shall be located in accordance with the Street Tree Map, Street Tree Chart, and standards set forth in Section 6 - Streets.

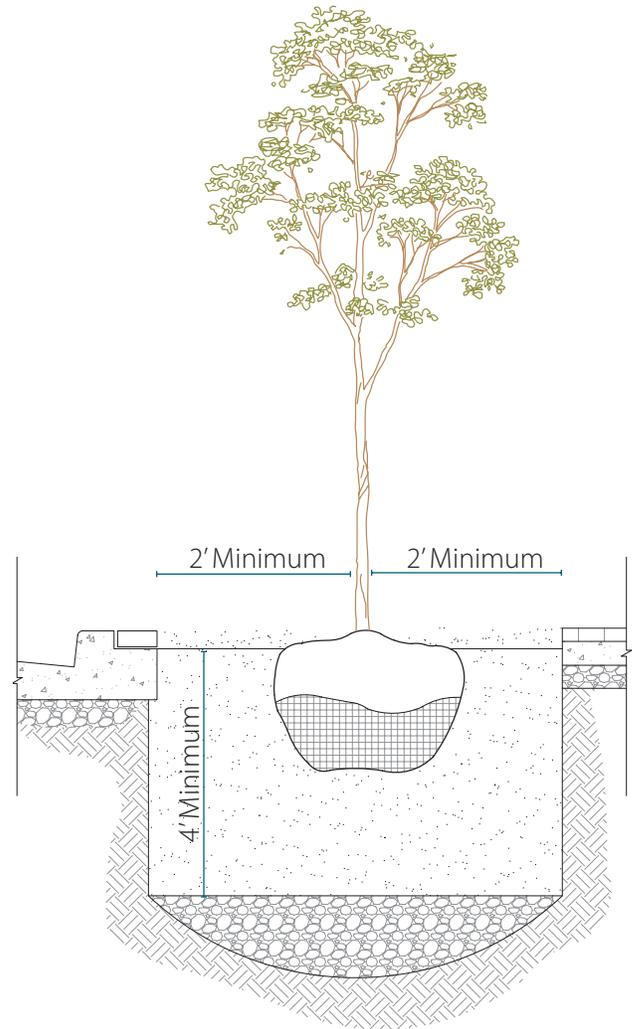


EXHIBIT 43- TREE PLANTING DIAGRAM

Redevelop Journal Square

Section 8 Design Standards



A place is defined not only by the character of its streetscape, but also by the quality of its buildings and the relationship of these buildings to the street, sidewalk, and public spaces. This section defines the basic design requirements that apply to all new and rehabilitated buildings in the Rehabilitation Area and defines a series of new building classes into which all new buildings must fit. Any building in which 51% or more of the structure is rebuilt or rehabilitated is considered a rehabilitated building and must meet the basic design requirements contained herein. The requirements set forth in this section of the document must be combined with the use and bulk requirements set forth in Section 3 – Land Use in order to gain a comprehensive picture of what type of development is permissible in the Rehabilitation Area.

The Design Requirements Section is divided into two primary subsections: Overall Design Requirements and Building Classes. Overall Design Requirements contains the requirements that apply to all new and rehabilitated buildings and is broken into the following requirements: build-to-line, building form, encroachments, facades, special buildings, and green buildings. Build-to-line and Building Form requirements deal with building placement, building base configuration, setbacks, and sky exposure planes, and loggia. Encroachment requirements contain awning, balcony, and sidewalk covering regulations. Façade Requirements addresses cornice lines, individual building widths, materials, and window requirements. Special Building and Green Building Requirements detail regulations pertaining to landmark buildings, preservation, and environmental building considerations.

The second subsection, Building Classes, sets forth specific requirements for six distinct building classes: extra small, small, medium, large, extra large, and signature. Every new building in the Rehabilitation Area must conform to one of these building classes. Each building class is briefly defined below; however the regulations relating to these building classes begin on page 184.

- Extra Small – A maximum height of 65 feet. See page 184.
- Small – A maximum height of 65 feet. See page 185.
- Medium – A height between 66 feet and 165 feet. See page 186.
- Large – A height between 166 feet and 440 feet. See page 187.
- Extra Large – A height between 441 feet and 1000 feet. See page 187.
- Signature – A height over 800 feet. See page 190.

Throughout the Design Requirements Section, photos and diagrams are used to illustrate the regulations as well as the visual and spatial characteristics that are envisioned for the Rehabilitation Area. The general locations of many of the buildings within the center of the study are illustrated in the Illustrative site plan and renderings in Section 2 – Overview.



8.2 Overall Design Standards

GENERAL REQUIREMENTS

- A. Buildings shall not exceed the MFAR requirements as defined in Section 3.7 MFAR Plan.
- B. Buildings shall not exceed the maximum heights as defined in Section 3.8 Heights Plan
- C. The maximum heights, as regulated in Section 3 – Land Use, does not include the mechanical penthouses, parapets or architectural embellishments for the “crown” of the building or antennas provided that they are in integral feature of the building design. There must be a complementary façade cover hiding mechanical penthouses. All mechanical penthouses, parapets and architectural embellishments must be approved by the Planning Board Site Plan Review Subcommittee.
- D. Developers proposing buildings over 166 feet tall must conduct an impact study that evaluate the potential of obstructed views, shadow casting, and wind tunnel effects along with other impact studies detailed in Section 9.3. The findings of this study must be presented to the Planning Board and will influence the project’s approval.
- E. The residential finished first floor must be raised off the sidewalk level a minimum of five (5) feet with a semi-public edge a minimum of five (5) feet deep.
- F. All retail entrances must be on-grade with the adjacent sidewalk and meet ADA requirements.
- G. The first floor of any building must be at least 1.5 times higher than the average floor height of the building.
- H. The top floor or floors must be at least 1.75 times higher than the average floor height of the building.
- I. There are no lot width maximums within the study area, except for the India Square Area defined as the area along Newark Avenue from Kennedy Boulevard to Tonnelle Avenue which shall have a maximum lot width of 50 feet.

BUILD-TO-LINE REQUIREMENTS

- A. A build-to-line is a line along which the front facades of a building must be located. Along this line, the building façade may deviate as stipulated in bullet E.
- B. All buildings must be built to the build-to-line [BTL] as regulated in Section 6 – Streets.
- C. The build-to-line is determined by measurement from the centerline of the existing streets as determined by the Governing Body controlling that right-of-way.
- D. The centerline of new street and build-to-lines are illustrated on each of the street sections in Section 6 – Streets.
- E. A building shall not be set forward of the BTL, but it may be set back from the BTL according to the following regulations: up to 10% of orthogonal facades may be set back from the BTL a maximum of 5 feet, up to 20% of facades with simple or compound curves may be set back from the BTL a maximum distance of 10 feet.
- F. Buildings may have more than one BTL. A BTL shall be determined for each side of a building that fronts onto a public right-of-way. In addition to right-of-way build-to-lines, some buildings will also be subject to Park/Plaza Build-To-Lines that are regulated in Exhibit 40.

BUILDING FORM REQUIREMENTS

Building Base

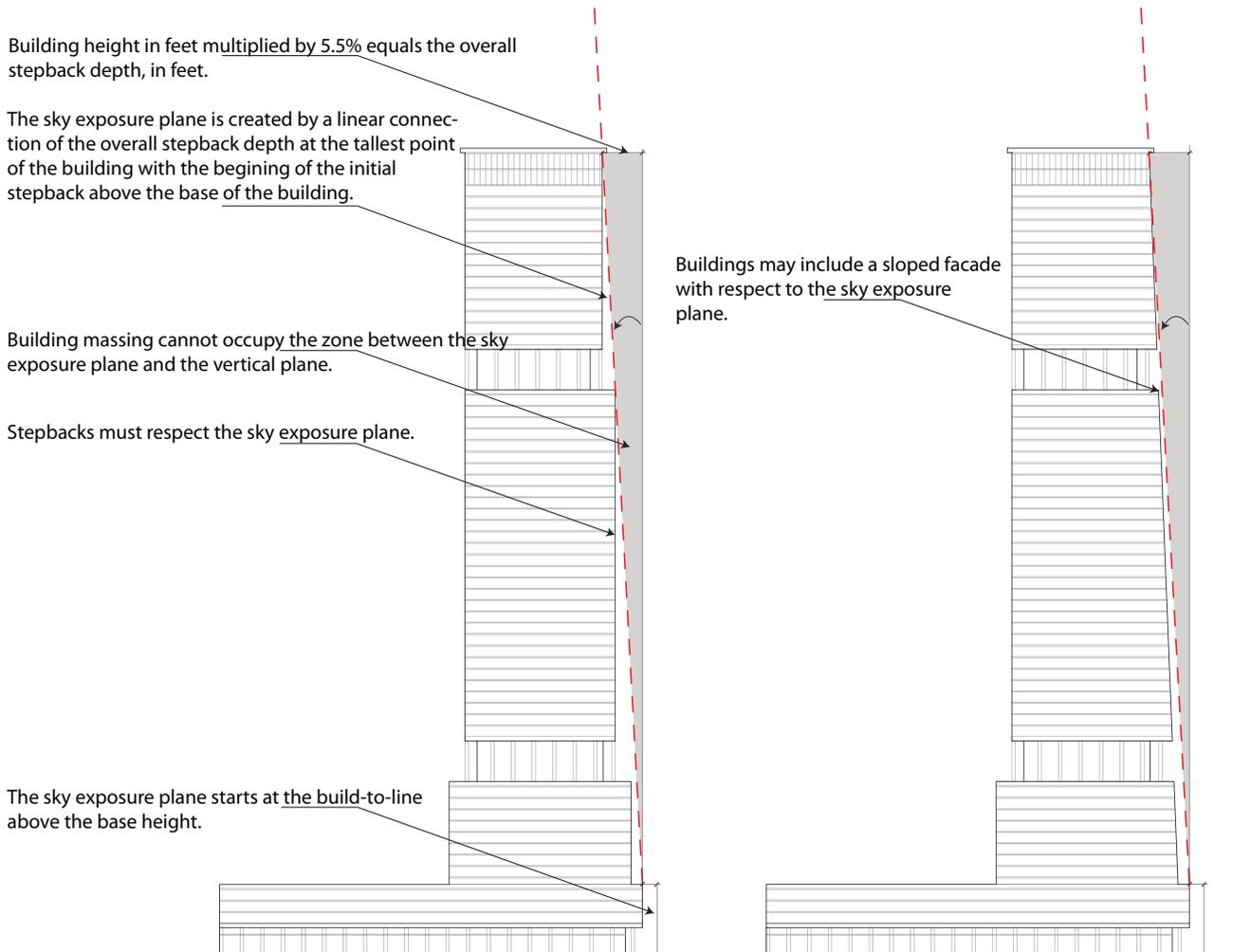
- A. In order to retain a human scale pedestrian realm while allowing for tall buildings, a minimum four (4) to a maximum six (6) story base is required on all new buildings. The streetwall created by a four (4) to six (6) story base is the most important visual element of the pedestrian realm.
- B. A maximum of 10% of the linear façade of any building above the first stepback may reach the sidewalk level on a public right-of-way or park.
- C. Any portion of a building that reaches the sidewalk level must respect the cornice line through the use of banding or continuation of the cornice line or other architectural feature.

Sky Exposure Plan - Stepbacks and Slantbacks

A sky exposure plan determines the location where the building must be stepped back or slanted back from the lower facade plane. Stepbacks or slantbacks are required in order to enhance the pedestrian experience at the ground level as well as diminish the visual impact of a high rise building as it become more vertical. A sky exposure plane is a virtual sloping line that begins above the build-to-line at the point of the first stepback and rises inwardly over a development parcel.

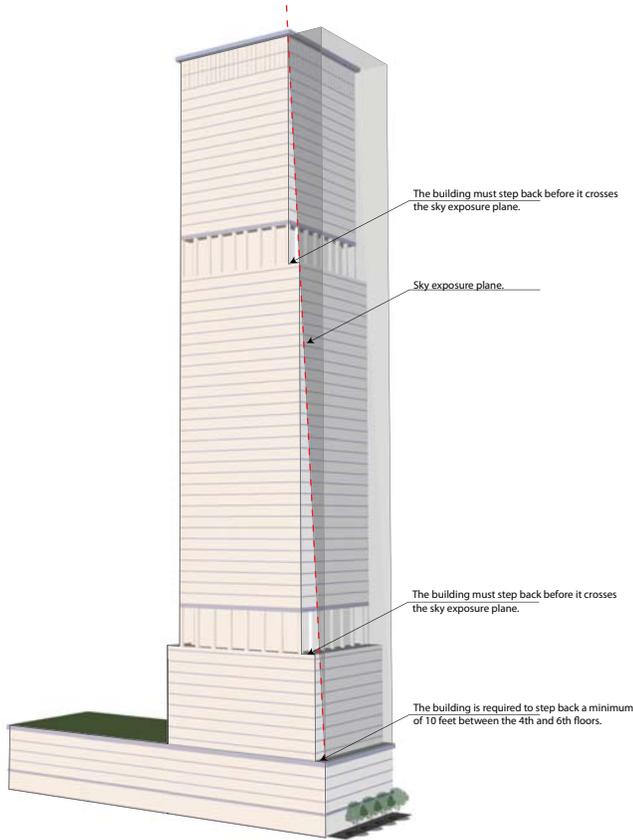
- A. All buildings must have one stepback between the 4th and 6th floor on sides of the building that front onto public right-of-ways. The depth of these stepbacks shall be determined by building class requirements (See Exhibits 52 - 56).
- B. The minimum depth of a front stepback is 10 feet.
- C. All buildings must stepback 15 feet on all sides not fronting a public right-of-way between the 4th and 6th floor.
- D. After the first stepback, no portion of any building fronting on a public right-of-way shall extend beyond the sky exposure plane. Specific sky exposure planes are expressed as a ratio of vertical distance to horizontal distance set forth in the Building Class regulations. Buildings may utilize stepbacks, slantbacks or some combination of the two to ensure that the building does not break the sky exposure plane.
- E. To the extent possible, the floor plate and tower design of Large and Extra Large Buildings should be designed to appear as narrow as possible by using a range of shapes, cuts, slopes, and changes in the façade plane.
- F. Other facades that do not front onto a public right-of-way may have stepbacks or slantbacks above the 4 to 6 story base, however these stepbacks or slantbacks should not exceed one-half of the sky exposure plane that is required on the side or sides of the building that front on a public right-of-way.

Exhibit S - Sky Exposure Plane Diagram

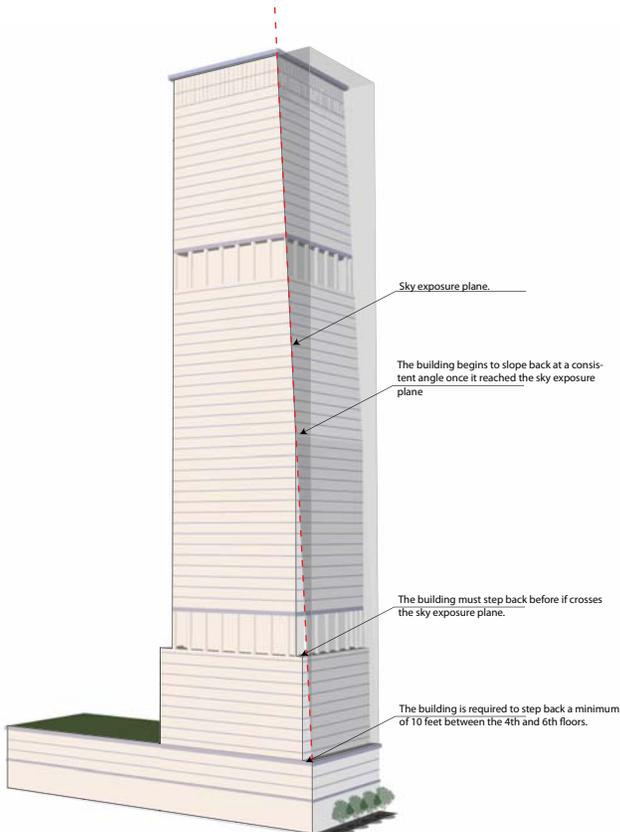
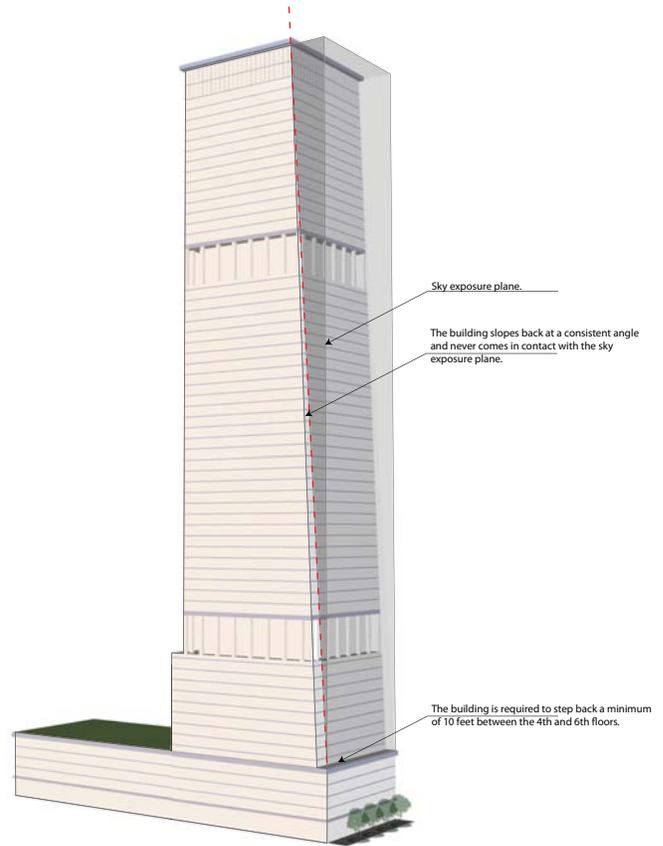




Example 1 : In this hypothetical building configuration, two stepbacks are used to meet the sky exposure plane requirements.



Example 2 : In this hypothetical building configuration, the building is slanted back along the sky exposure plane.



These buildings are for illustrative purposes and specific buildings should and will vary as different architects apply the sky plane regulations.

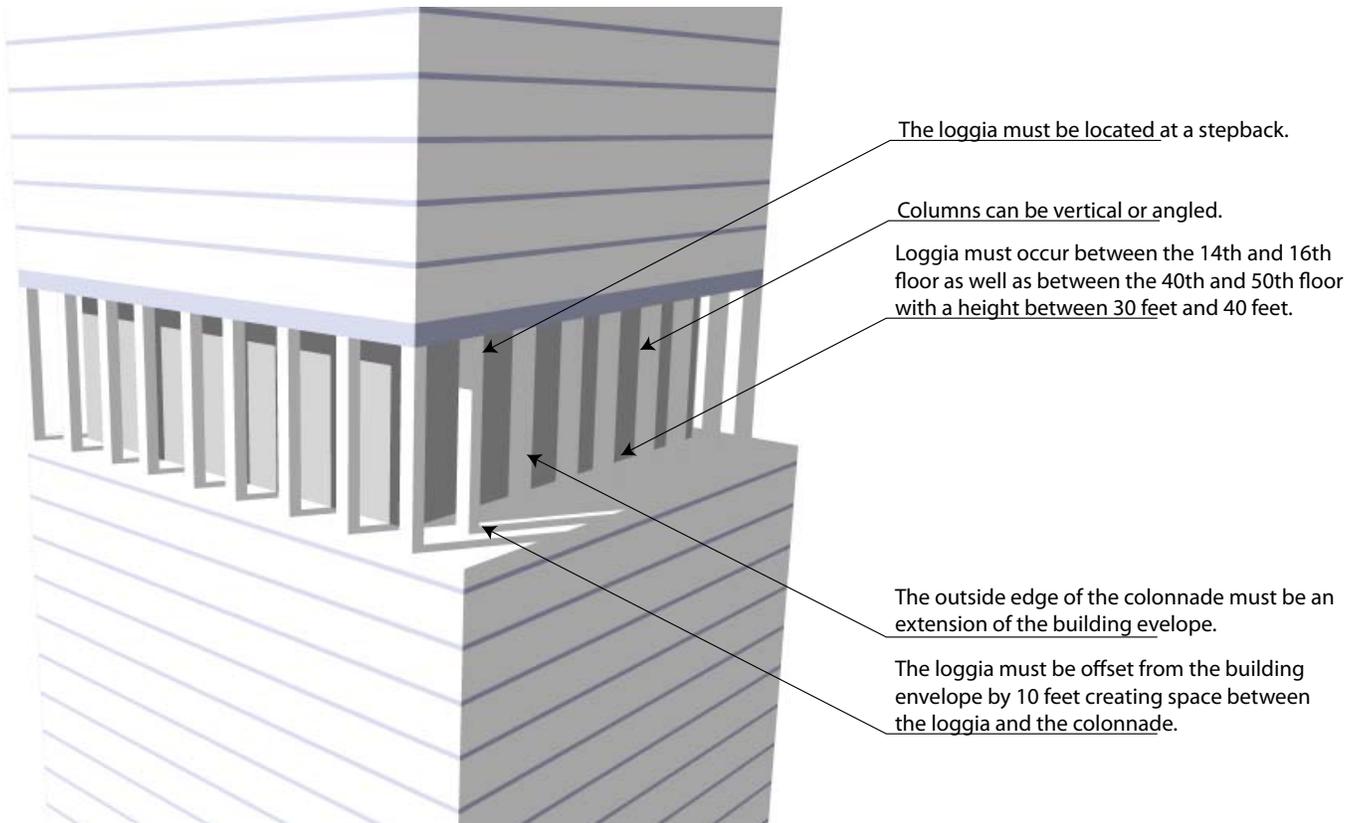
Example 3 : In this hypothetical building configuration, the building use a combination of a stepback and slanting the building back along the sky exposure plane.

Loggia

A Loggia is a gallery that can be opened to the air on at least one side. It is composed of multiple floors on the upper levels of a building over 165 feet high. Loggias create breaks in the vertical scale of a building and are surrounded by columns or created by a setback within the façade. Loggias create an interior and exterior space with views in all directions. In addition to reducing the vertical scale of high rise buildings, the placement of loggias in new buildings creates a horizontal architectural element that unifies the composition of buildings within the Rehabilitation Area.

- A. Loggias are required between the 14th and 16th floors and again between the 40th and 50th floor.
- B. In lieu of a loggia with columns a solid wall setback a minimum of 3 feet is allowed.
- C. The minimum height of a loggia is 30 feet, however loggias of 40 feet are recommended in larger buildings.
- D. 25% of loggia space must be accessible to all building tenants. These spaces can serve both residential and office tenants and be utilized as community spaces, outdoor terraces, conference rooms, dining facilities, reception areas, recreation areas, rentable spaces such as offices, workrooms and apartments, or mechanical operations space.
- E. Loggias shall be located in combination with any stepbacks above the first stepback or if a slantback is used the first floor of each loggia must start between the 14th and 16th floors and again between the 40th and 50th floor.
- F. Columns should have a maximum width of six feet and should be located to ensure that views from the interior of loggias are maximized.
- G. The outside edge of the colonnade must be an extension of the building envelope.
- H. The loggia must be inset from the building envelope by 10 feet creating space between the loggia and the colonnade or setback.

Exhibit T - Loggia Diagram





ENCROACHMENTS REQUIREMENTS

General

Encroachments are those building features that extend over the build-to-line.

- A. No building feature encroachment is allowed to extend over the BTL beyond four (4) feet of the front façade. Approved encroachments include cornice projections, fixed sun-shades, projecting signs, bow and bay windows, porticos, steps and stoops, recessed patios, and boxed-out windows.

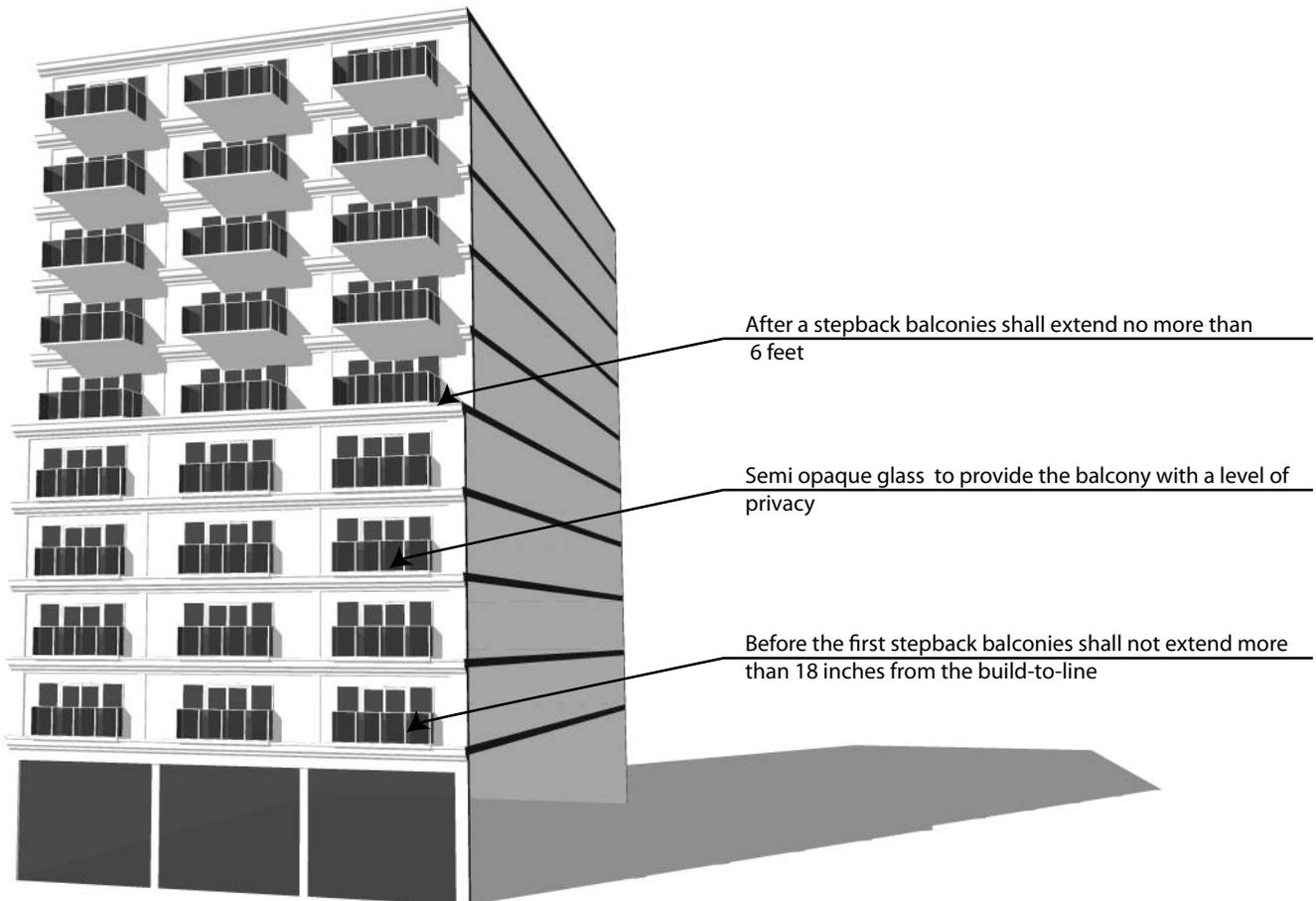
Balconies

Residential units should incorporate outdoor spaces where possible using balconies, patios, or recessed patios.

- A. Balconies occurring before the first step back (on the 4-6 story base) shall extend a maximum 18 inches from the façade.
- B. Above the first stepback, balconies shall extend a maximum of six (6) feet from the building façade, but shall not encroach on the BTL. Balconies shall be a minimum of four (4) feet.
- C. A railing or screen composed of a semi-opaque material is required for all balconies.
- D. No open storage is allowed on a balcony or a recessed patio.



Exhibit U - Balcony Diagram



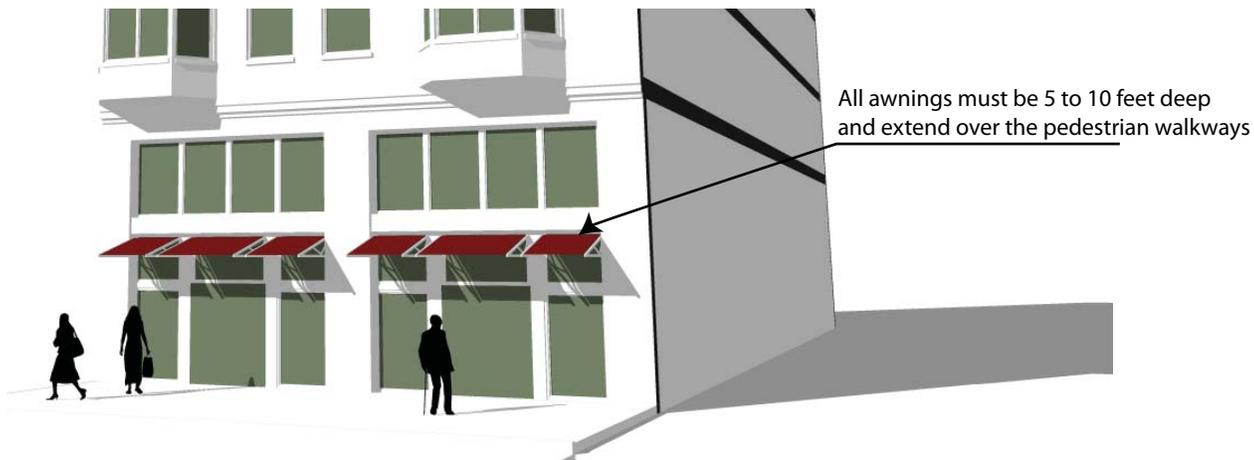


Awnings/Sidewalk Coverings

An awning is a shading device, typically constructed of fabric or metal, mounted over an exterior door or window.

- A. There are multiple sidewalk coverings that can be used to fulfill building class requirements for architectural features and retail frontages.
- B. Permitted sidewalk coverings fall into the following categories:
 - a. Retractable canvas awnings
 - b. Fixed awnings
 - c. Marquees
- C. The retail frontage along Pedestrian Priority Street A shall include awnings/sidewalk covering that extend a minimum of seven (7) feet beyond the build-to-line. These awnings/coverings must end at least two (2) feet before the cartway.
- D. Awnings/Sidewalk Coverings on all other streets must be 5 feet to 10 feet deep and shall not place supports upon the public sidewalk.
- E. Canvas awnings should be retractable and rectangular with no side panels.
- F. The front ribbon edge may have limited signing pursuant to Planning Board approval (see Exhibit V).

Exhibit V - Retractable Canvas Awning Diagram



- G. Fixed Awnings (see Exhibit W)
 - a. Fixed awnings are permanently attached to a building and are designed as an integral architectural feature of the building. They can be located either as an entrance feature or as a shade and covering along the entire length of a building.
 - b. Fixed awnings must be a combination of glass and metal
 - c. Fixed awnings should extend out between five (5) and ten (10) feet from a building.
 - d. Signs may be attached to the top edge of fixed awnings.
 - e. Fixed awnings shall place no supports on the sidewalk.

Exhibit W - Fixed Awning Diagram





- I. Marquee Entrance (see Exhibit X)
 - a. A Marquee is a large projection over the sidewalk typically associated with a theater or cinema. It is permanently attached to the building and is self supportive with no structural columns. It is typically decoratively lit, has the name of the theater as well as information on current or upcoming events.
 - b. Marquees may occupy the entire front of an entertainment use.
 - c. Marquees may extend to within one (1) foot of the curb edge.

Exhibit X - Marquee Entrance Diagram



- J. Prohibited Awnings
 - a. Dome Awnings, Convex Awnings, Elongated Dome Awnings, and Entrance Canopies illustrated in Exhibits 44, 45, 46, and 47 are not allowed anywhere in the Rehabilitation Area.

Exhibit 44 - Dome Awning

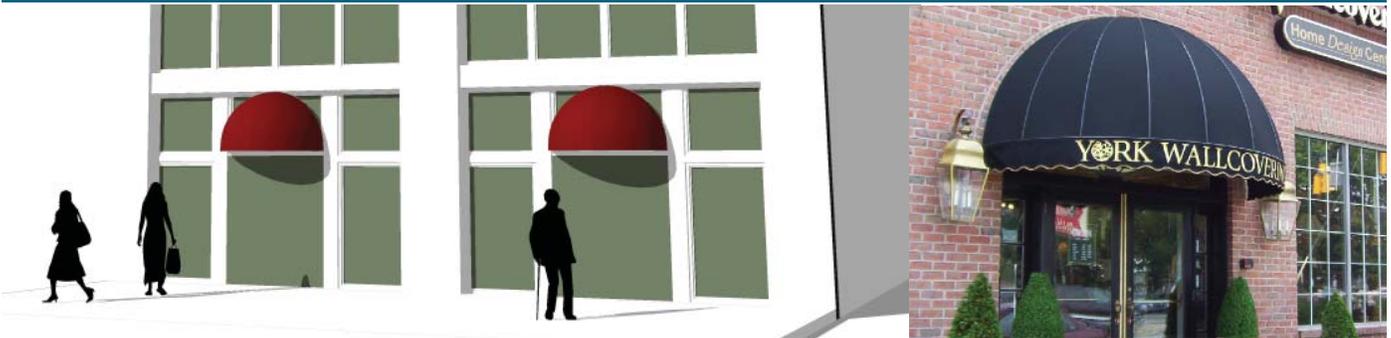


Exhibit 45 - Convex Awning



Exhibit 46 - Elongated Dome Awning



Exhibit 47 - Entrance Canopy Diagram



FAÇADE REQUIREMENTS

- A. Buildings must have an architecturally defined base, middle, and top.
- B. All sides of a building must be architecturally detailed.
 - a. However, the facades on the interior of the block are as important and shall share common elements and materials with the front façade. Interior facades do not have to express their Identifiable Building Widths (IBW).
- C. In order to create unique blocks within the Area, no specific façade on any block will be identical.
- D. Horizontal articulation, which includes cornices, must express the division of a building between the base and middle and the expression of the top or roof line.
 - a. A cornice (between one (1) and three (3) feet) of metal or masonry is required between the first and second stories.
 - b. Where a stepback is required, a two (2) to ten (10) foot cornice of masonry, glass or metal is required at the top of the story prior to the stepback.
 - c. Buildings without stepbacks must have a two (2) to six (6) foot cornice of masonry, glass or metal at the top for any façade facing a public right-of-way.
- E. Façades must not be blank and should have at least one window per structural bay, in a pattern that suggests habitation as retail, offices or residences.
- F. Exposed basement walls are required have at least one small window per structural bay as appropriate to an occupied foundation.



Identifiable Building Width (IBW) (see Exhibit Y)

- A. An Identifiable Building Width (IBW) is the division of a façade into what appears to be separate buildings sharing a common design vocabulary of elements, shapes and sizes, next to each other, but of sufficient architectural expression to visually show a difference.
- B. Each IBW must be distinguished by changes in material, colors, window and door treatment, expression of pilasters, vertical mullions, masonry pattern design, cornice treatments, on non-residential buildings.
- C. IBWs must be incorporated into the facade of the four (4) to six (6) story base of any building greater than 40 feet wide and along the façade above the stepback on medium class buildings.
- D. Common elements that can enhance the expression of IBWs in residential buildings include: projected window bays, stoops, balconies, patios, and portico treatments.
- E. Each IBW must consist of no more than three primary materials, textures, or colors (excluding windows and framing).
- F. IBWs can be classified as:
 - a. Narrow: 15 to 18 feet wide
 - b. Medium: 18 to 36 feet wide
 - c. Wide: 36 to 60 feet wide
- G. There must be a change in IBW width every 200 feet or less.
- H. The narrow IBW is most appropriate configuration for attract small retail businesses at the ground level.

Exhibit Y - IBW Diagram





Materials

- A. In order to create buildings with longevity, materials chosen for the interior and exterior of buildings are required to be durable and high quality.
- B. Masonry, glass, and metal are the preferred materials. Masonry can include precast concrete, synthetic stone, marble, granite, composite stone, and bricks.
- C. Jumbo or utility bricks, asphalt and asbestos shingles, and aluminum and vinyl siding are prohibited.
- D. Synthetic Stucco materials including Exterior Insulation Finishing Systems (EIFS) are also prohibited.
- E. Metal on a façade must only include anodized aluminum, stainless steel, coated galvanized steel, copper, zinc, and painted wrought iron.
- F. Where glass is used, clear or lightly tinted green or blue energy efficient coatings is permitted.
- G. Any exteriors venting must blend, complement, and enhance the façade through the use of color, composition and materiality.

Windows/Voids (see Exhibit Z)

- A. Windows shall be large to maximize natural day lighting.
- B. Windows shall respond to energy efficiency and respond to a LEED/green building principles.
- C. Some portion of all windows shall be operable to ensure natural ventilation and air circulation.
- D. Clerestory, transom, side light, and skylights windows are permitted and may be non-operable.
- E. The percentage of void area (windows and other openings) in a building façade shall be a minimum of 40%, except at street-level retail frontages, where it must not be lower than 75% (see Exhibit Z as an example of possible window/void façade treatments).
- F. Buildings surrounding Journal Square Plaza and City Center Park may have up to 95% of their facade composed of glass.
- G. The window pattern must form a rational composition and must align with the doors.
- H. Windows on primarily masonry facades must have headers and sills.
- I. For residential use, ground floor window sills shall be a minimum five feet above the sidewalk.
- J. Window glass must be clear or have lightly tinted green or blue energy efficient coatings.
- K. Glass curtain walls are permitted and encouraged.
- L. Mirrored glass is prohibited.
- M. Strip windows shall be prohibited.

Signage

It is the intent of these regulations to provide aesthetically pleasing, well-designed signage within the Rehabilitation Area. All signage shall be designed to complement the architectural design and character of the buildings, streets and uses it serves. It is recognized that certain uses, such as theaters, commercial, recreation, and entertainment uses have unique signage requirements. For these uses, marquees, animated signage, and signage integrated into the building's façade or architecture is permitted.

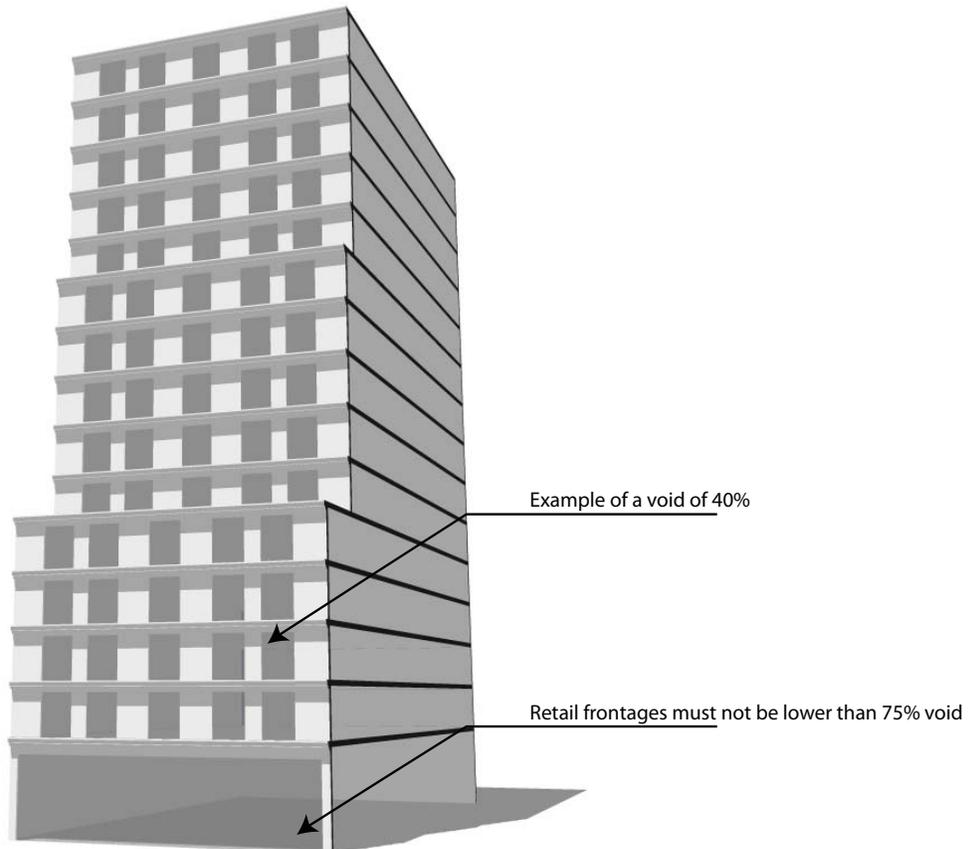
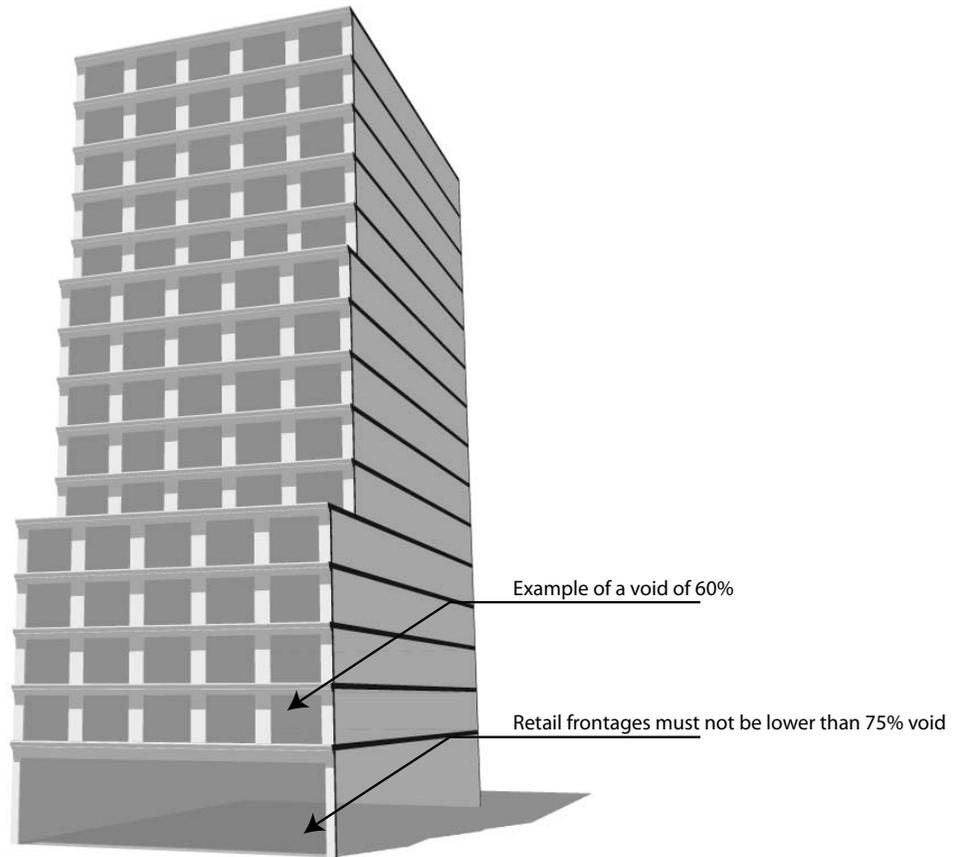
In order to regulate signage within the Rehabilitation Area shall submit a comprehensive signage package subject to review and approval granted through the Planning Board. The signage package shall address the design and size limitations for all signage within the Rehabilitation Area. This shall include: wayfinding and directional signage; identification signage for the overall project, and street signs. It shall not include the following freestanding signs: banners, animated, and kiosks. All signage shall be designed to complement the architectural design and character of the buildings.

The design and size of the permitted signage shall be guided by the following requirements:

- A. Residential: The multi-family community within the building shall be permitted identification signage not to exceed 20 SF. In addition, that community may provide any necessary signage required for proper mail delivery indicating the name(s) of the resident(s) of the building on the mailbox or doorbell. In the interior of the lobby, each residential building shall provide necessary signage required for proper mail delivery indicating the name(s) of the resident(s) on the building on the mailbox or doorbell.



Exhibit Z - Window Ratio Diagram





- B. Ground Floor Retail Uses: Each such use shall be allowed exterior signage not to exceed five (5%) percent of the area of the storefront (ground floor) to which it is attached.
 - a. Signs may be flush mounted and/or extend as a “blade” sign as illustrated in the images to the right.
 - b. Window signage shall not exceed twenty-five (25%) of the glazed area.
 - c. Blade signs and hanging arcade signs must not be more than 15 square feet.
 - d. Both a flush mounted sign and a blade sign may be utilized provided that the total signage does not exceed the maximum amount permitted.
 - e. Flush mounted signs must not exceed 40 square feet.
- C. Retail uses above the ground floor:
 - a. Exterior signage for uses above the ground floor shall be limited to one 4ft. x 6ft. directory sign flush mounted to the ground floor of the building.
 - b. Each retail use may also have one window sign, provided that the lettering is of a decorative style that compliments the architecture of the building and does not cover more than twenty (20%) percent of the window surface.
- D. Commercial Recreation and Entertainment Uses: These uses may have additional signage that is integrated into the architecture of the building. Theaters may also have a marquee as part of its signage package.
- E. Offices: Each building may have one sign indicating the name of the major tenant. Said sign shall be flush mounted to the façade of the building. In addition, the building may have a directory sign flush mounted to the interior of the lobby space within the building.
- F. Hotels: Each hotel use may have a sign indicated the name of the hotel. Said sign shall be flush mounted to the façade of the building. In addition, each hotel may have a directory sign flush mounted to the interior of the lobby space within the building. The directory sign may indicate the names of other uses located within the hotel such as restaurants or other facilities.
- G. Plaza: Neon, interactive, and animated signs are required for the plaza area. The terminal view must have an animated/lit wall sign type wall at least 20 floors high.
- H. Animated signs are encouraged along the corners of building facades. Said signs are permitted to project out from the façade up to 12 inches, starting at a minimum of 20 feet above the level of the sidewalk.
- I. Lighting levels for signage should be controllable to allow lighting intensity to be significantly reduced at times of low pedestrian or vehicular activity.
- J. The building is permitted to have up to four (4) signs located at an upper level that identifies the building and/or its primary occupant. No single side of a building shall have more than one sign located at an upper level.
- K. Wherever possible, public signage should be consolidated and affixed to lampposts.
- L. Key and up-lighting of the façade and upper story terraces is required.
- M. Wherever possible, public signage should be consolidated and affixed to lampposts.
- N. Information kiosks with directions and advertising are required. A complete pedestrian oriented kiosk and wayfinding system shall be developed by the City.
- O. During construction, one (1) sign for each project or development may be displayed indicating the name of the project, developer, design professionals, general contractor, sub-contractor, financing institution and/or public agency officials (as applicable and appropriate). The sign area shall not exceed three hundred (300) square feet. Said sign shall be removed upon completion of the project.



WAYFINDING AND KIOSK SIGNAGE



FLUSH MOUNTED RETAIL SIGNAGE



PEDESTRIAN ORIENTED BLADE SIGNS



GLASS SIGNAGE



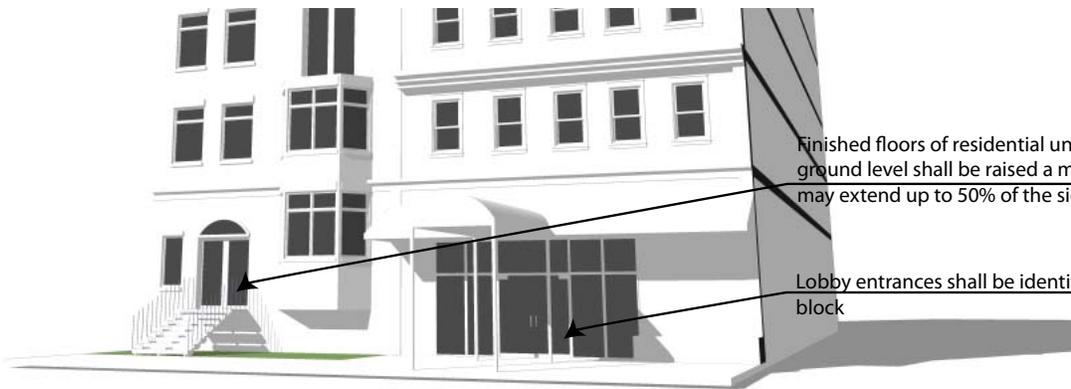


Lighting

- A. All entryways shall be appropriately lit at all times.
- B. Lighting fixtures attached to the building must be of a finish, style, and character appropriate to the architecture and details of the building.
- C. Interior lights, except for security lights, must be turned off after employees and maintenance/service personnel have left.
- D. All Large, Extra Large, and Signature buildings shall have special lighting for the top or “crown” of the building.
- E. Key lighting of important architectural features is highly recommended.
- F. Lighted signage must be controllable and significantly reduced after 3 am.
- G. All lighting must be dark sky compliant. The control of light pollution to surrounding areas shall be in accordance with the lighting of Jersey City Zoning Code, Jersey City Ordinances, or this Plan.

Building Entrances and Stoops

- A. Every use shall place their primary entrance on one street frontage or Plaza/Park frontage, although additional secondary entrances are permitted.
- B. Lobby entrances frontage onto a street must be of unique design expressed through the design of the doors, lights, color and material, railing design and coverings over the entry door.
- C. Each lobby entrance shall be clearly marked with a street number and the name of the building.
- D. No two lobby entries on a single block shall be identical.
- E. Stoops and stairs for lower units shall connect the building to the sidewalk and may extend up to 40% of the width of the sidewalk.
- F. Stairway details must vary from building to building.
- G. Risers shall be stone, capped masonry, concrete, or a decorative metal.
- H. Hand rails must be decorative and architecturally integrated with the design of the building.
- I. Stairs may be parallel with building façade.
- J. A building with 10 or more units must have an on-grade lobby entrance.



Finished floors of residential units with private entry at ground level shall be raised a minimum of 2 feet. Stairs may extend up to 50% of the sidewalk.

Lobby entrances shall be identifiable and unique to the block



SPECIAL BUILDING REQUIREMENTS

Landmark Buildings

- A. Exhibit 48 – Landmark Buildings denotes locations for a hierarchy of landmarks throughout the Rehabilitation Area: Signature Landmark, Major Landmark, and Minor Landmark.
- B. There are three (3) Signature Landmarks, seven (7) Major Landmarks, and 26 locations for minor landmarks in the Area.
- C. The three categories of Landmarks are defined and regulated as follows:
 - a. Signature Landmarks: These are the most prominent buildings in the Area and their architecture must be of a unique and iconic in nature. They terminate the vista along Kennedy Boulevard and define Journal Square Plaza. These buildings must be the tallest and incorporate contemporary building technologies and forms. Buildings in these locations must follow the Signature Building Requirements in the Building Class Requirements Section.
 - b. Existing Landmarks: These are landmarks, like the Loews Theater, that have defined Journal Square and Jersey City for decades and should be restored and preserved as architecturally important buildings.
 - c. Major Landmarks: Major landmark buildings provide focal points for both pedestrians and drivers within the neighborhood. Buildings which are major architectural landmarks must have additional height or embellishments which extend at least 20 feet from the building. Such expression must include but not be limited to projections, towers, roof forms, height increases, or other architectural appurtenances appropriate to the scale and architectural expression and style selected.
 - d. Minor Landmarks: These buildings provide focus and navigability of the city to the pedestrian. They must be accomplished by the use of the chamfered or rounded corners and under 5 feet changes in height.



SIGNATURE LANDMARKS



MINOR LANDMARKS



MAJOR LANDMARKS



EXISTING LANDMARKS





EXHIBIT 48 - LANDMARK BUILDINGS PLAN





Building Preservation

- A. Buildings designated as either a historic building, a neighborhood asset, or signature façade on the Preservation Plan (Exhibit 49) are reminders of Journal Square’s history and shall be preserved for the future as cultural assets. If any buildings are deemed of significant historic value by the local historical society and not indicated in Exhibit 49, this plan should be amended to include that building.
- B. Buildings that have been designated as signature façade may be redeveloped, but must retain the façade of the existing building.
- C. Buildings indicated on Exhibit 49 need not conform to the new height requirements if they are rehabilitated; however additional stories may be as added while preserving the existing structure or the street front façades.

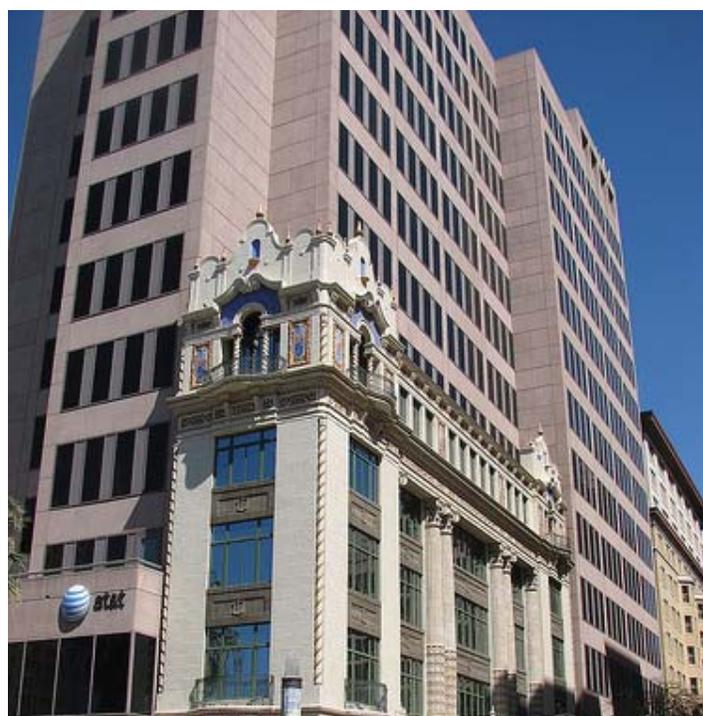
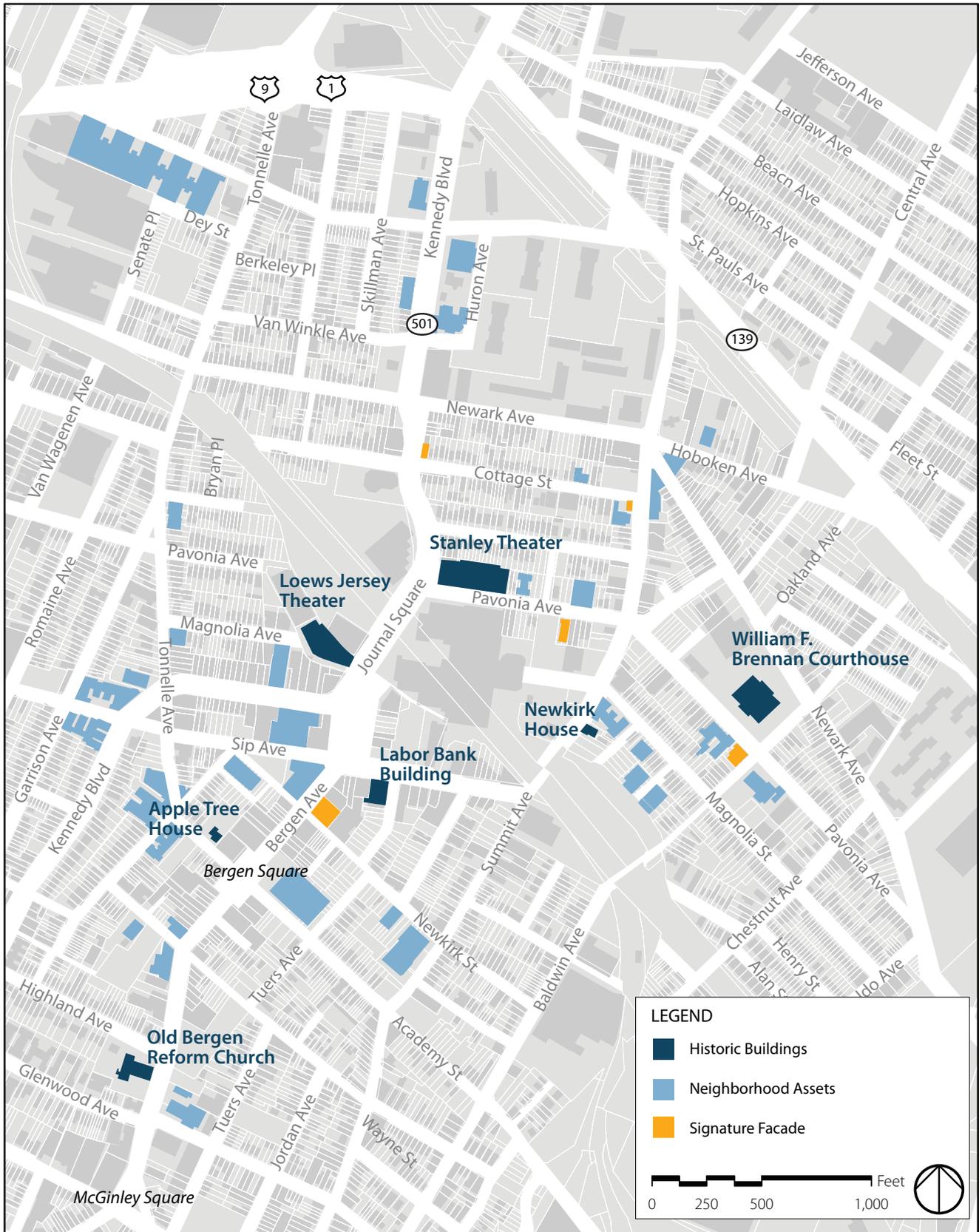




EXHIBIT 49 - HISTORIC BUILDINGS AND NEIGHBORHOOD ASSETS PLAN



Arcades

- A. Retail arcades are permitted along the Journal Square Plaza and Greenway in lieu of a fixed awning.
- B. The arcades shall range from 15 to 18 feet deep and from 18 to 20 feet high. (see Exhibit AA)
- C. Blade signs under the arcade are recommended.
- D. Exhibit 50 illustrates where arcades are recommended.

Exhibit AA - Arcade Diagram





EXHIBIT 50 - ARCADE PLAN





Signature Lighting/Signage

- A. Commercial recreation, entertainment, theaters, and hotels uses located on Journal Square Plaza may have marquee signs, animated signage and signage that is integrated into a building façade or its architecture.
- B. Digital signage and special lighting is encouraged and allowed only within the Journal Square Plaza as indicated in Exhibit 51. Within this area, it is limited to the first forty floors of buildings fronting onto the Plaza.
- C. Off-premise advertising is permitted only along the frontages indicated in Exhibit 51.
- D. Any building that employs digital signage must also incorporate architectural up-lighting in to the building.





EXHIBIT 51 - DIGITAL SIGNAGE PLAN





Security gates

- A. No roll down security gates are allowed on the exterior of any building.
- B. Open grill roll down gates are allowed on the interior of the building but must not obscure any window displays.
- C. It is highly recommended that commercial establishments employ alternative security measures instead of security gates such as glass and motion sensors.

GREEN BUILDING REQUIREMENTS

General

- A. All buildings should be LEED Certified as defined by the US Green Building Council or another nationally recognized green building certification system.
- B. At a minimum, all building interiors shall be designed with environmental considerations in mind.
- C. This includes designs to maximize the energy performance of the wall, natural day-lighting, passive solar orientation considerations, passive fenestration, and natural ventilation.
- D. Interior light shall as much as possible be generated from alternative energy source (i.e. wind or solar).

Green Roofs and Wastewater

- A. Green roofs are required on all parking structures with exposed roofs and are required on all other building roofs with less than a 30% pitch.
- B. Green roofs shall be comprised of an engineered roofing system that enables the growth of vegetation.
- C. The green engineered rooftop shall be comprised of the following components: an insulation layer, a waterproof membrane, a root barrier, a drainage layer, a geotextile or filter mat, a growing medium, and vegetation.
- D. Green roofs shall be either intensive or extensive in type.
 - a. Intensive green roofs are considerably more substantial in terms of the amount and type of vegetation supported and also in terms of cost and maintenance required. Intensive green roofs can support planting depths up to 8 inches but require higher load bearing structural roofs.
 - b. Extensive green roofs are covered with grasses, sedums, and wildflowers within a planting bed between 2 inches and 6 inches deep. Maintenance is minimal and little irrigation is required beyond rainfall.
- E. Green roofs should only be irrigated (if necessary) with water obtained from a rainwater collection system integrated into the building and/or block.
- F. A green roof strategy should be incorporated into the Jersey City Journal Square Stormwater Management Plan.
- G. Developers shall reduce potable water use for building sewage conveyance by 50% through the use of water-conserving fixtures or non-potable water, recycled greywater, and on-site or municipally treated wastewater.
- H. Buildings shall use 20% less water than the water used baseline calculated for the building (not including irrigation) after meeting the Energy Policy Act of 1992 fixture performance recommendations.

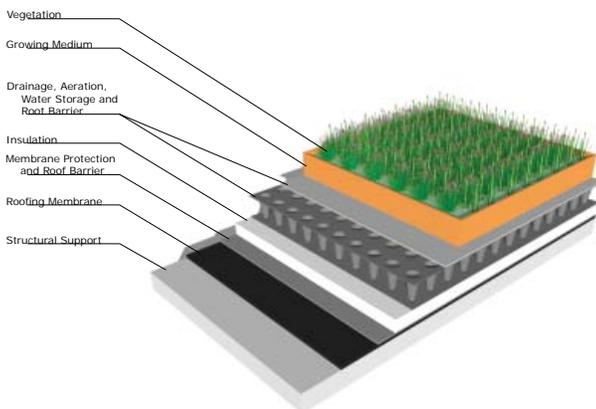
Energy and Recycling

- A. Developers shall design the building envelope, HVAC, lighting, and other systems to maximize energy performance.
- B. Considerations should be made for assessing the project for non-polluting and renewable energy potential including solar, wind, geothermal, low-impact hydro, biomass, and bio-gas strategies.
- C. Buildings in the Rehabilitation Area shall coordinate the size and functionality of the recycling areas with the anticipated collection services for glass, plastic, office paper, newspaper, cardboard, and organic wastes to maximize the effectiveness of the dedicated areas.



Sustainable Construction

- A. Development shall incorporate 5% of salvaged materials into building design.
- B. Development shall employ the use of 20% recycled materials (based on cost of the total value of the materials in the project) from a combination of post-consumer and pre-consumer sources.
- C. Development shall use materials or products that have been extracted, harvested or recovered, as well as manufactured, within 500 miles of the project site for a minimum of 10% of the total materials value.
- D. Development shall employ rapidly renewable materials (made from plants that are typically harvested within a ten-year cycle or shorter) such as bamboo, wool, cotton, agrifiber, wheatboard, strawboard, and cork.
- E. Development shall use low-VOC materials for adhesives, fire-stopping sealants, caulking, paint, and duct sealants.
- F. Development shall provide a lighting system controllable by individual occupants or by specific groups in multi-occupant spaces.



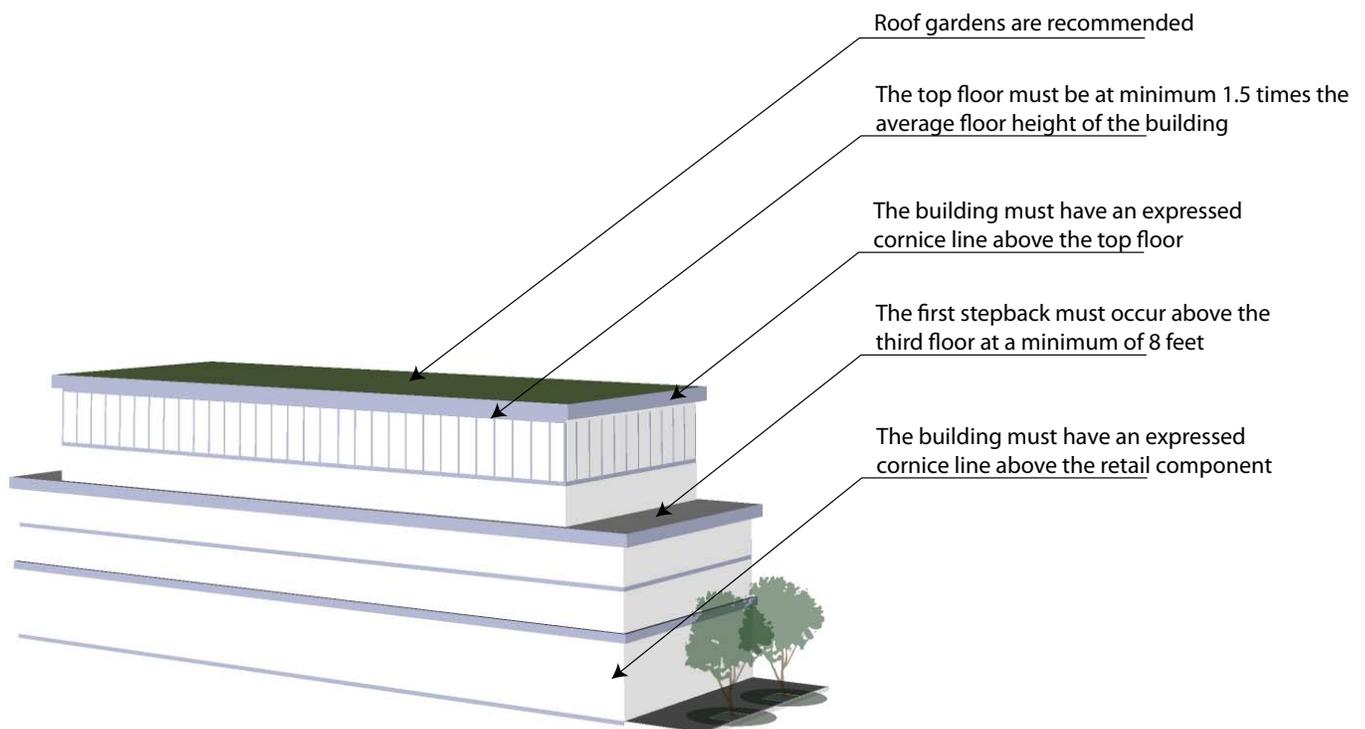


Building forms for new buildings in the Rehabilitation Area have been divided into five classes based on height as regulated in Section 3 – Land Use. For each building class specific requirements and architectural regulations are listed below.

EXTRA SMALL

- Maximum Height: 65 feet
- Stepback: required at the 3rd floor at a minimum depth of 8 feet
- Sky exposure plane: none required
- Loggia: none required
- Awning: retractable or fixed awnings required for retail frontages.
- Arcade: none required
- Window Ratio Ground Floor: must be a minimum of 75%
- Window Ratio Upper Floors: must be a minimum of 60%
- Minimum Ground Floor Height: 1.5 times average floor height of the building.
- Minimum Top Two Floor Height: 1.5 times than average floor height of the building.
- Side yard: none required when a blank wall abuts another blank wall. Side yards are required at a minimum of 10 feet or according to fire code requirements when windows are present on the proposed and/or abutting building(s), whichever is longer.
- Maximum Building Coverage: 90% lot coverage
- Rear Yards: not required where the building occupies the entire depth of the block. A minimum rear yard setback of 25 feet is required where it occupies a lot that is not the entire depth of the block and abuts existing habitable buildings.

EXHIBIT 52 - EXTRA SMALL BUILDING DIAGRAM

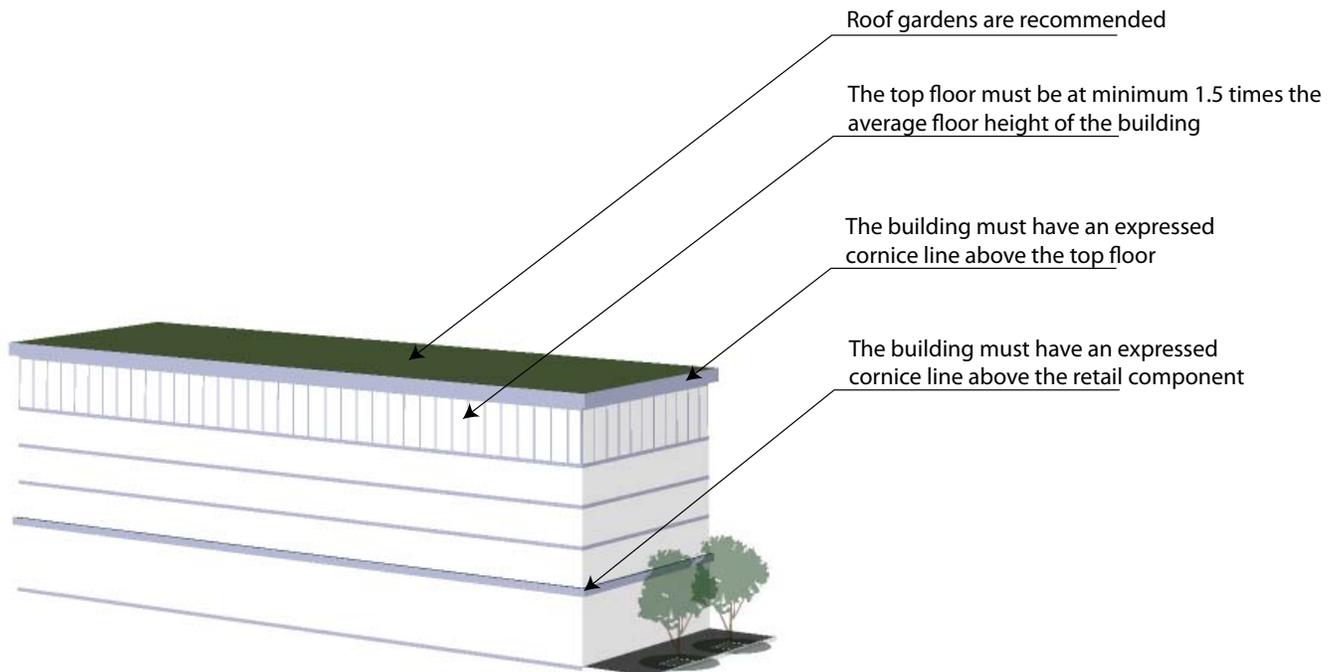




SMALL

- Maximum Height: 65 feet
- Stepback: none required
- Sky exposure plane: none required
- Loggia: none required
- Awning: retractable or fixed awnings required for retail frontages.
- Arcade: none required
- Window Ratio Ground Floor: must be a minimum of 75%
- Window Ratio Upper Floors: must be a minimum of 60%
- Minimum Ground Floor Height: 1.5 times average floor height of the building.
- Minimum Top Two Floor Height: 1.5 times than average floor height of the building.
- Side yard: none required when a blank wall abuts another blank wall. Side yards are required at a minimum of 10 feet or according to fire code requirements when windows are present on the proposed and/or abutting building(s), whichever is longer.
- Maximum Building Coverage: 90% lot coverage
- Rear Yards: not required where the building occupies the entire depth of the block. A minimum setback of 25 feet is required where it occupies a lot that is not the entire depth of the block and abuts existing habitable buildings.

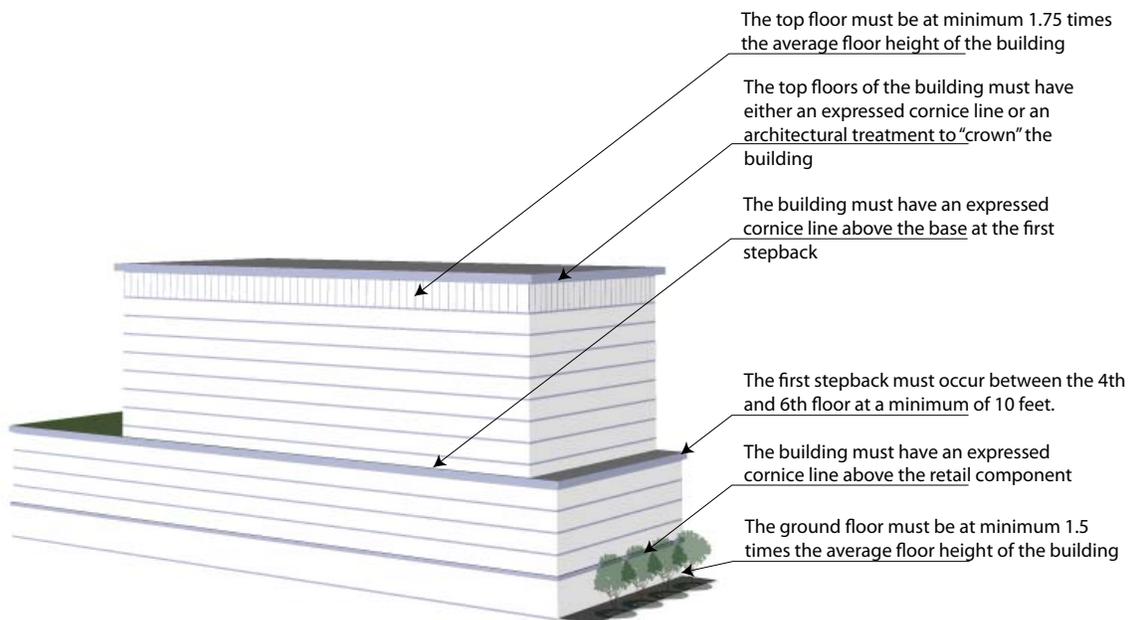
EXHIBIT 53 - SMALL BUILDING DIAGRAM



MEDIUM

- Maximum Height: 165 feet
- Stepback: required between the 4th to 6th floors at a minimum depth of 10 feet on the front façade that faces onto streets, plazas, or parks. Stepbacks are required on the side and rear of the building on the 4th to 6th floors at a minimum of 10 feet where the side or rear of the building abuts the side or rear of another habitable building or lot.
- Sky exposure plane: none required
- Loggia: none required
- Awning: retractable or fixed awnings required for retail frontages.
- Arcade: none required
- Window Ratio Ground Floor: must be a minimum of 75%
- Window Ratio Upper Floors: must be a minimum of 60%
- Minimum Ground Floor Height: 1.5 times average floor height of the building.
- Minimum Top Two Floor Height: 1.75 times than average floor height of the building.
- Side yard: none required when a blank wall abuts another blank wall. Side yards are required at a minimum of 10 feet or according to fire code requirements when windows are present on the proposed and/or abutting building(s), whichever is longer.
- Maximum Building Coverage: 95% lot coverage
- Rear Yards: not required where the building occupies the entire depth of the block. A minimum setback of 25 feet is required where it occupies a lot that is not the entire depth of the block and abuts existing habitable buildings.

EXHIBIT 54 - LARGE BUILDING DIAGRAM





LARGE

- Maximum Height: 420 feet
- Stepback: required between the 4th to 6th floors at a minimum depth of 10 feet on the front façade that faces onto streets, plazas, or parks. Stepbacks are required on the side and rear of the building on the 4th to 6th floors at a minimum of 10 feet where the side or rear of the building abuts the side or rear of another habitable building or lot.
- Sky exposure plane: 5.5 % starting at the outer edge of the first stepback.
- Loggia: one at the 15th floor, minimum 40 feet high.
- Awning: fixed awing required for retail frontage.
- Arcade: permitted for retail frontages, may substitute for awning.
- Window Ratio Ground Floor: must be a minimum of 90%
- Window Ratio Upper Floors: must be a minimum of 65%
- Minimum Ground Floor Height: 2 times average floor height of the building.
- Minimum Top Two Floor Height: 2 times than average floor height of the building.
- Side yard: none required when a blank wall abuts another blank wall. Side yards are required at a minimum of 15 feet or according to fire code requirements when windows are present on the proposed and/or abutting building(s), where a building front on more than $\frac{3}{4}$ of a block front.
- Lane Way: a 25 foot easement dedicated for pedestrian access and limited vehicular service.
- Maximum footprint: 180 feet by 180 feet above the first stepback for office use and 120 feet x 120 feet for residential use.
- Rear Yards: not required where the building occupies the entire depth of the block. A minimum setback of 25 feet is required where it occupies a lot that is not the entire depth of the block and abuts existing habitable buildings.
- Distance Between Buildings: Minimum distance of 50 feet must be maintained between any portion or extremity of any high rise building to another building.
- Maximum Building Coverage: 95% lot coverage

EXTRA LARGE

- Maximum Height: 1,000 feet
- Stepback: required between the 4th to 6th floors at a minimum depth of 10 feet on the front façade that faces onto streets, plazas, or parks. Stepbacks are required on the side and rear of the building on the 4th to 6th floors at a minimum of 10 feet where the side or rear of the building abuts the side or rear of another habitable building or lot.
- Sky exposure plane: 5.5% starting at the outer edge of the first stepback.
- Loggia: one required between the 14th and 16th floors and one required between the 40th and 50th, both must be 2 to 3 times the average floor height for the building.
- Awning: fixed awing required for retail frontage.
- Arcade: permitted for retail frontages, may substitute for awning.
- Window Ratio Ground Floor: must be a minimum of 95%
- Window Ratio Upper Floors: must be a minimum of 70%
- Minimum Ground Floor Height: must be 2 times average floor height of the building.
- Minimum Top Two Floor Height: must be 2 times than average floor height of the building.
- Side yard: none required when a blank wall abuts another blank wall. Side yards are required at a minimum of 15 feet or according to fire code requirements when windows are present on the proposed and/or abutting building(s), where a building front on more than $\frac{3}{4}$ of a block front.
- Lane Way: a 25 foot easement dedicated for pedestrian access and limited vehicular service.
- Maximum footprint: 180 feet by 180 feet above the first stepback for office use and 120 feet x 120 feet for residential use.
- Rear Yards: not required where the building occupies the entire depth of the block. A minimum setback of 25 feet is required where it occupies a lot that is not the entire depth of the block and abuts existing habitable buildings.
- Distance Between Buildings: Minimum distance of 50 feet must be maintained between any portion or extremity of any high rise building to another building.



Where property is located along a Primary Street that has been widened as a result of this Plan and the depth of such property is less than 100 feet as a result of such widening, the Sky Exposure Plane setback may be waived after the first stepback. Some form of articulation will be expected to create a visual cue of a pedestrian scaled facade.

EXHIBIT 55 - LARGE BUILDING DIAGRAM

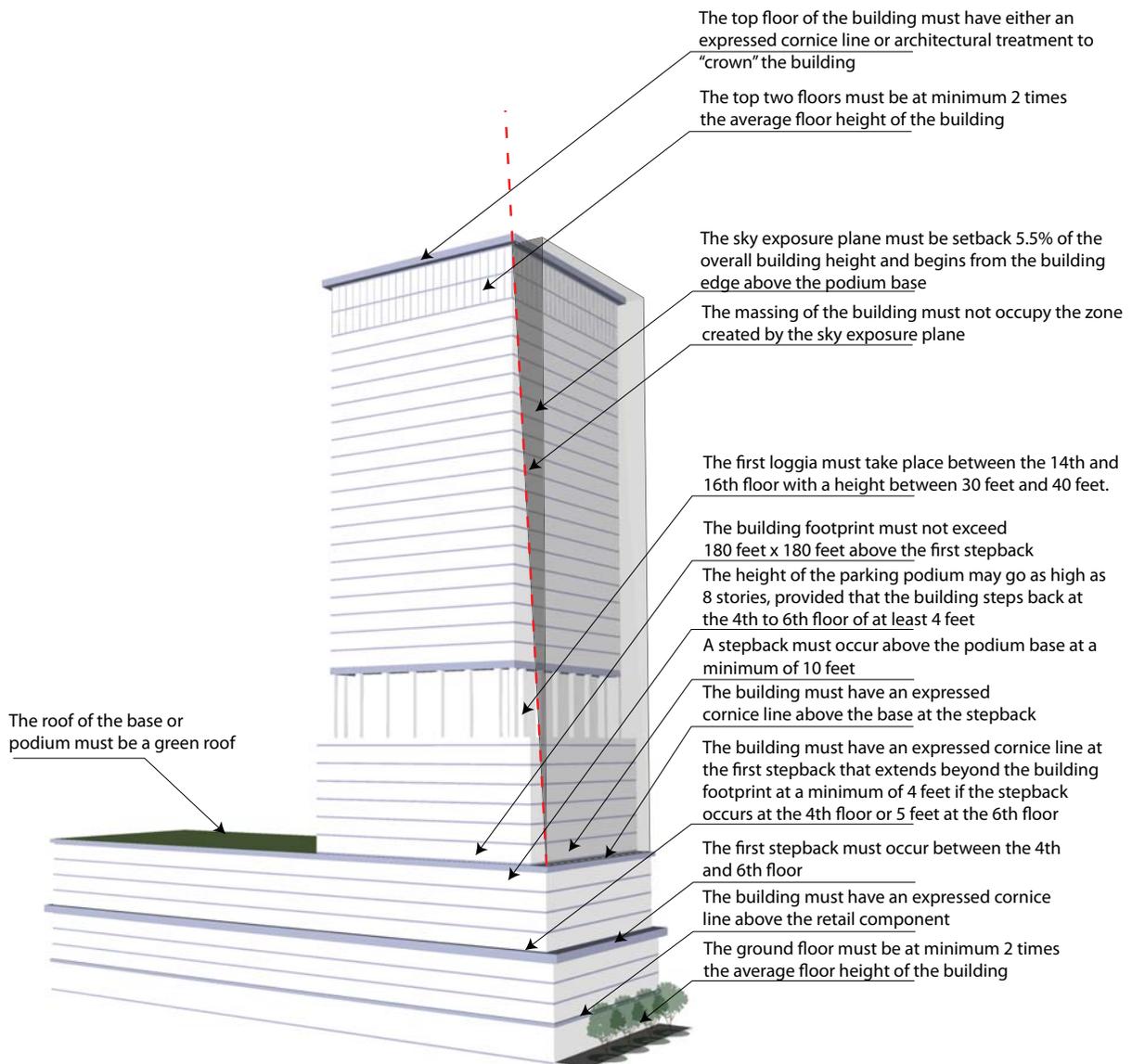
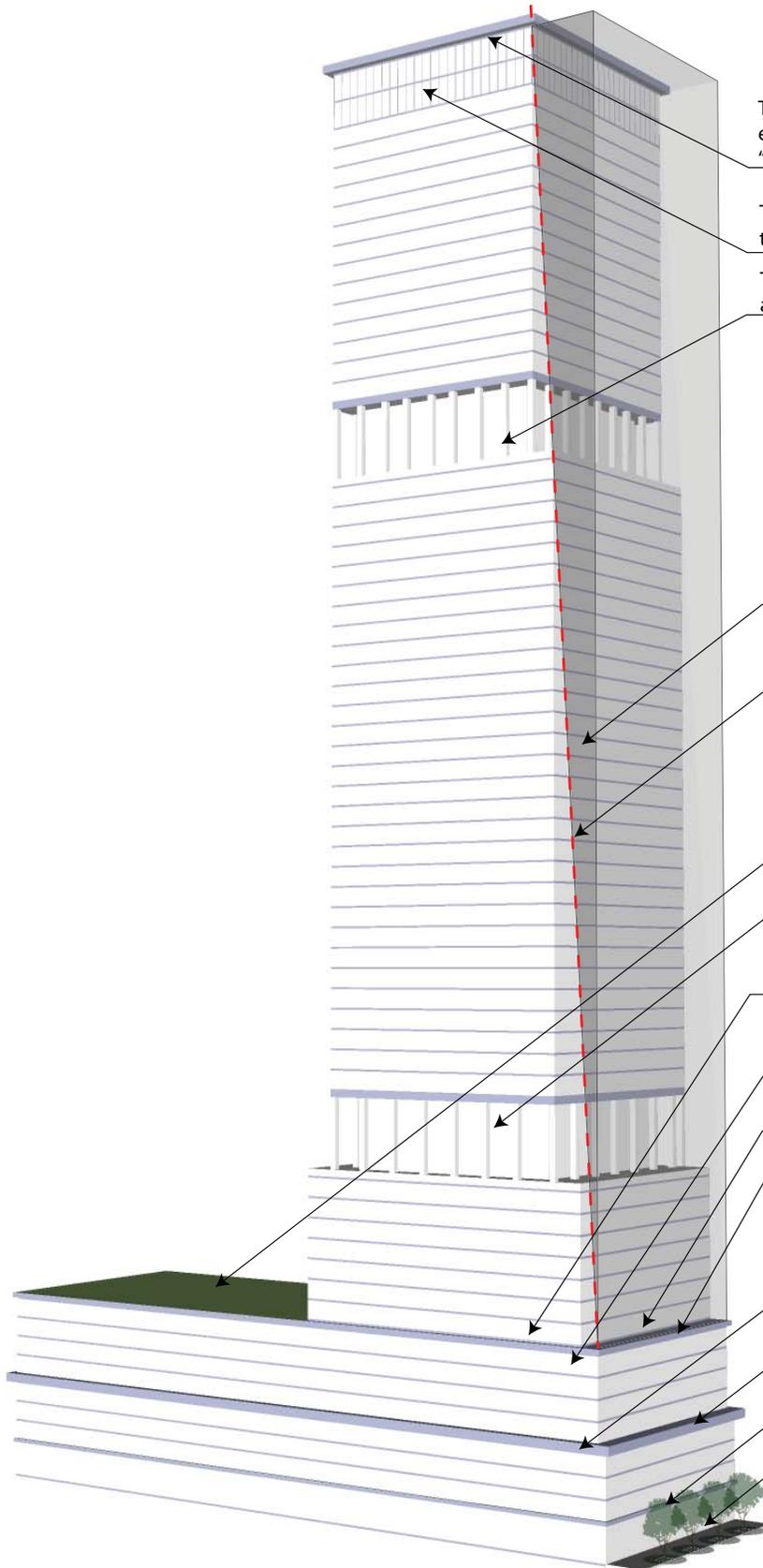




EXHIBIT 56 - EXTRA LARGE BUILDING DIAGRAM



The top floor of the building must have either an expressed cornice line or architectural treatment to "crown" the building

The top two floors must be at minimum 2 times the average floor height of the building

The second loggia must take place between the 40th and 50th floor with a height between 30 feet and 40 feet.

The massing of the building must not occupy the zone created by the sky exposure plane

The sky exposure plane must be setback 5.5% of the overall building height and begins from the building edge above the podium base

The roof of the base or podium must be a green roof

The first loggia must take place between the 14th and 16th floor with a height between 30 feet and 40 feet.

The building footprint must not exceed 180 feet x 180 feet above the first stepback

The height of the parking podium may go as high as 8 stories, provided that the building steps back at the 4th to 6th floor of at least 4 feet

A stepback must occur above the podium base at a minimum of 10 feet

The building must have an expressed cornice line above the base at the stepback

The building must have an expressed cornice line at the first stepback that extends beyond the building footprint at a minimum of 4 feet if the stepback occurs at the 4th floor or 5 feet at the 6th floor

The first stepback must occur between the 4th and 6th floor

The building must have an expressed cornice line above the retail component

The ground floor must be at minimum 2 times the average floor height of the building



SIGNATURE

Only three signature buildings will be allowed in the Rehabilitation Area. Locations for signature buildings are indicated on the Landmarks Plan. The design for a Signature Building requires an international design competition to determine the design of the building. The panel of judges for the competition must include at least three architects, two certified planners, one structural engineer, one transportation engineer, three property owner representatives, and three city representatives. The panel of judges must be selected at the time of request for competition entry is sent out or made available. If a competition is not held, the building will be held to all the design regulations and subject to the Extra Large Building Class requirements.

SPECIAL BUILDING REQUIREMENTS FOR PARKING STRUCTURES

- A. The following requirements apply to all parking structures developed within the Journal Square Rehabilitation Area.
- B. The following parking configurations are permitted within the Area:
 - a. Under Building (below grade) – one or more levels of underground parking connected by ramps with a discrete entrance that complements the architecture of the building.
 - b. Embedded – Parking structures that are wrapped by the outer edges of a building containing retail, residential or office uses. Wrapping the structure conceals any exterior exposure to parking. Embedded parking is the preferred on-site parking technique. It must be fully ventilated.
 - c. Podium – Lower floors within a building where parking extends to the build-to-line. The first and second floor of buildings utilizing podium parking must be dedicated to retail, office, or residential uses. The facades of parking floors must replicate the faced found on upper floors or utilize architectural screening to conceal the parked cars and interior lighting. In general, podium type parking is discouraged.
 - d. Walkable – A mixed-use parking structure within a short walking distance of a development. These neighborhood-scaled structures should be dispersed throughout the Rehabilitation Area to create a network of convention parking facilities within Journal Square.
 - e. Local Intercept – A mixed-use parking structure with a capacity of 2,000 to 5,000 vehicles located on the periphery of the Rehabilitation Area that is linked by transit service to neighborhoods or developments in Journal Square.
- C. Parking spaces should be laid out to accommodate the large number of cars with appropriate parking depth and length to accommodate a range of car sizes and parking layouts.
- D. Valet parking for some or all of a parking structure is allowed.
- E. All parking structures, except for regional remote facilities, must be mixed-use. Where parking structures front on a public right-of-way, park, or plaza. The structure shall contain commercial or mixed-uses on the ground floor. Where appropriate, second story commercial uses are recommended.
- F. On all facades, parking structures shall be designed to complement the architectural style, building scale, mass, building materials and colors of the principal or historic buildings in the adjacent area. Parking structures shall be indistinguishable from residential, commercial, or mixed-use buildings in the use of solids and voids. The scale and mass of parking structures shall be mitigated through wall offsets, openings and other distinctive design elements. Window openings shall be vertical or square in proportion. No long horizontal openings shall be permitted.
- G. Parking structures must step back between the 4th and 6th floor and have a distinctive cornice line at the stepback.
- H. Parking structures shall not express sloping floor decks in the facades of the structure. Architectural elements in the façade shall be expressed as horizontal and vertical architectural features and lines similar to other types of buildings.
- I. All parking façades should be designed to eliminate interior lighting from being visible from the outside of the structure. Various techniques, including double screening, can be used for this purpose.
- J. Where under building parking exists, the façade of the building must mimic a basement façade with real windows and must be fully ventilated.
- K. High pressure sodium lighting is not permitted in the interior of parking structures.
- L. Each floor of a parking facility shall be coded for the ease of locating vehicles.
- M. Automated parking is highly recommended.
- N. Green roofs are highly recommended for the tops of parking structures.



PLANNING BOARD SITE PLAN REVIEW SUBCOMMITTEE

Because of the architectural and site plan complications of this Plan, it is required that The City of Jersey City appoint a Planning Board Site Plan Review Subcommittee consisting of a minimum of five practicing professional architects, planning, urban design, or landscape architects. This subcommittee shall to review all the plans, sections, perspectives and reports at the workshop, pre-submission, and at submission. They shall meet with perspective developers in the initial phases and review plans and submitted information and prepare a report to the Redevelopment Agency and to the Planning Board for their review. This subcommittee can recommend waivers of certain architectural features where the new design will improve the overall visual and spatial character of the building as well as the function of the plan.

Redevelop Journal Square

Section 9 Legal Provisions



SEVERABILITY OF THE PLAN

If any section, subsection, paragraph, division, subdivision, clause or provision of this Plan shall be deemed by a court of competent jurisdiction to be invalid, such adjudication shall only apply to the particular section, subsection, paragraph, division, subdivision, clause or provision in question, and the balance of the Plan shall be adjudged valid and effective, provided that, if a court of competent jurisdiction invalidates the District Improvement Fund Bonus provisions, then only the Base Plans are defined herein may be utilized.

ZONING MAP REVISIONS

The Zoning Map of the City of Jersey City is hereby amended and shall be revised to show boundaries of the Journal Square Rehabilitation Area and identify the district as the “Journal Square Rehabilitation Area”. In addition, the Zoning Map of the City of Jersey City is hereby amended and shall be revised to show boundaries of the Area and all provisions of this Plan shall apply therein. This Redevelopment Plan shall supersede all provisions of the Jersey City Zoning Ordinance, except for the Palisades Protection Overlay discussed at Sections 3.2 and 3.3, herein. No variance from the requirements herein shall be cognizable by the Zoning Board of Adjustment. The Planning Board alone shall have the authority to grant deviations from the requirements of this Plan, as provided herein. This Plan shall also supersede preexisting redevelopment plans for preexisting redevelopment areas within the Journal Square Rehabilitation Area, except to the extent that all such preexisting redevelopment areas and the powers conferred upon them by such preexisting redevelopment plans shall remain intact.

PARCELIZATION

The diagrams, images, tables, plans, and other graphic representations provided in this Redevelopment Plan are intended to complement the written standards and regulations contained herein. Nothing in this Redevelopment Plan shall preclude the partial redevelopment of a block depicted in such diagrams, images, tables, plans, or other graphic representations, provided that such subdivision or re-subdivision and partial redevelopment of a block is fully in conformance with the written standards, diagrams and regulations contained herein. (Such parcelization of any disposition parcel shall be subject to approval by the Planning Board and shall be reflected within the Redeveloper Agreements as may be executed between the Redevelopment Entity and such Redevelopers as may be designated by the Redevelopment Entity.)

AMENDMENT TO THE JOURNAL SQUARE REDEVELOPMENT PLAN

The Journal Square Redevelopment Plan may be amended from time to time upon compliance with the requirements of the law. Any amendment to the Redevelopment Plan shall be made by ordinance, formally adopted by the City Council after public hearing, and shall comply with the provisions of NJSA 40:12A-7.

As development occurs within the Area, development priorities and market demands may change. This Plan should have the adaptability to meet the changing needs of market demand, the City of Jersey City and its citizens. Amendments may be required in order to accommodate these changes.

An application to amend this Redevelopment Plan shall include any required modifications to the following sections:

- A. Land Use Plans
 - 1. Land Use
 - 2. Retail Frontage
 - 3. Heights
 - 4. MFAR
- B. Transportation Plans
 - 1. Vehicular Circulation
 - 2. Bicycle
 - 3. Pedestrian
- C. Parking Standards
- D. Street Sections and Associated Map
- E. Landscape and Open Space Plan
- F. Building Typologies and Design Standards
- G. All related tables and charts



An application to amend this Redevelopment Plan shall include a Plan Consistency Review section, which examines the relationships between the development plan and the applicable regulatory documents, including the Master Plans of the City of Jersey City and surrounding municipalities, the Master Plan of Hudson County, and New Jersey State Development and Redevelopment Plan.

Any amendment to the Journal Square Redevelopment Plan should adhere to the street locations and typologies within the confines of the nature of the design character and intent set forth within this Plan, regardless of intended use.

This Plan may be amended from time to time upon compliance with the requirements of law. A fee of ten thousand dollars (\$10,000), plus all costs for copying and transcripts shall be payable to the City of Jersey City for any request from a private party to amend this Plan.

DEVIATIONS

The Planning Board may grant deviations from the regulations contained within this Plan, where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Plan, would result in peculiar and exceptional practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Planning Board may also grant a deviation from the regulations contained within this Plan related to a specific piece of property where the purposes of this Plan would be advanced by such deviation from the strict application of the requirements of this Plan; and the benefits of granting the deviation would outweigh any detriments. The Planning Board may grant exceptions or waivers from design standards, from the requirements for site plan or subdivision approval as may be reasonable and within the general purpose and intent of the provisions for site plan review and/or subdivision approval within this Plan, if the literal enforcement of one or more provisions of the plan is impracticable or would exact undue hardship because of peculiar conditions pertaining to the site. No deviations may be granted under the terms of this section unless such deviations can be granted without resulting in substantial detriment to the public good and will not substantially impair the intent and purpose of this Plan.

An application requesting a deviation from the requirements of this Plan shall provide public notice of such application in accordance with the public notice requirements set forth in NJSA 40:55D-12.a. & b.

No deviations may be granted which will result in permitting:

1. A use or principal structure in a district restricted against such use or principal structure;
2. An expansion of a non-conforming use;
3. An increase in height;
4. An increase in the height of the base of the building;
5. An increase in the permitted modified floor area ratio;
6. A reduction in the sidewalk widths, pedestrian ways, or lane ways;
7. An increase in the parking ratio;
8. Varying the minimum or maximum number of stories or their location as such story limitations are outlined and other requirements as outlined in the Height Plan and Frontage Plan sections of this Plan;
9. Varying in any way from the Use Standards Section of this Plan;
10. Varying the pattern of the Street Network Plan, as generally described in the text of the Mobility section of the Redevelopment Plan in relation to thoroughfare type, Right-Of- Way width, and pavement width beyond normal adjustments encountered during survey synchronization;
11. Non-completion of minimum open space, parks, or other type of phased improvements required to be implemented;
12. Deviation from the District Improvement Bonus Fund provisions set forth in this Plan; or
13. Non-compliance with the specific goals and objectives enumerated in the Plan.



PLAN CONSISTENCY REVIEW

In accordance with Section 40A:12A-7 of the Local Redevelopment and Housing Law, Adoption of Redevelopment Plan, this plan will include: (5) Any significant relationship of the redevelopment plan to (a) the master plans of contiguous municipalities, (b) the Master Plan of the county in which the municipality is located, and (c) the State Development and Redevelopment Plan adopted pursuant to the "State Planning Act," P.L. 1985, c.398 (C.52.18A-196 et al).

In accordance with NJSA 40:12A-1 et seq., Chapter 79, Laws of New Jersey 1992, known as the "Local Redevelopment and Housing Law", the following statements are made:

- A. The Plan herein has delineated a definite relationship to local objectives as to the appropriate land uses, density of population and improved traffic and public transportation, public utilities, recreation and community facilities, and other public improvements using general regulations and the specific form based codes required in this Plan.
- B. The Plan has laid out various strategies needed to be implemented in order to carry out the objectives of the Plan.
- C. The Plan has set forth proposed thoroughfare layouts and standards, land uses and building requirements for the Redevelopment Plan

RELATIONSHIP TO THE MASTER PLAN OF THE CITY OF JERSEY CITY, HUDSON COUNTY

The most recent Master Plan for Jersey City was adopted by the Jersey City Planning Board in August 2000. The Master Plan designated the center of the Study Area as the Central Business District and the immediately adjacent areas as Neighborhood Commercial, Office/Residential or Residential. The Master Plan states that, "Historically, the City of Jersey City's principal activity district was its central business district, located at Journal Square. This area has a mixed-use pattern of development in keeping with its historic function as a business, shopping, and transportation center".

The Master Plan recommends limiting uses to high density residential, government/public facilities, parking and transportation related facilities, commercial office and retail space, hotels and conference centers, restaurants and bars as well as theaters and other forms of entertainment. Based on the above and other reviews the Plan is in compliance with the Jersey City Master Plan.

The Plan is also in compliance with the Master Plan of the County of Hudson.

Consistency of the Redevelopment Plan with the New Jersey State Development and Redevelopment Plan

On March, 2001, the State Planning Commission ("SPC") adopted the new State Development and Redevelopment Plan ("SDRP"). The SDRP establishes a proposed statewide planning framework that is designed to maintain and revitalize existing cities and towns and organizing new growth in "centers" – compact, mixed-use communities that provide a variety of choices in housing, employment opportunities, entertainment, services, transportation and social interaction. The Rehabilitation Area is in Metropolitan Planning Area 1 (PA-1) in the SDRP. According to the State Plan, the intent of the Metropolitan Planning Area is to:

- Provide for much of the state's future development;
- Revitalize cities and towns;
- Promote growth in compact forms;
- Protect the character of existing stable communities.

The State Plan recommends adopting the principles of Smart Growth, Transit Oriented Development and maximizing Sustainability. The Plan for Journal Square is consistent with the intent of the Metropolitan Planning Area as enunciated in the SDRP. It is a model of smart growth and transit-oriented development.

The Redevelopment Plan will advance the SDRP's objectives for physical and economic redevelopment, and improved quality of life. Therefore, the Journal Square Redevelopment Plan is consistent with the goals and objectives of the 2001 SDRP.



RELATIONSHIP WITH ZONING ORDINANCE

The current Jersey City Land Development Ordinance was adopted in April of 2001. The Land Development Ordinance is consistent with the recommendations of the Master Plan. The center of the Area is zoned as the Central Business District and the immediate surrounding areas as Neighborhood Commercial, Office/Residential or Residential. The purpose of the district states, "The purpose of the central business district is to foster the development of a vibrant and accessible Citywide activity district that is a center of commerce and civic activity. It functions as a local and regional destination for business, retail, education, government services, entertainment and transportation".

CONSISTENCY OF THE REDEVELOPMENT PLAN WITH MASTER PLANS OF MUNICIPALITIES ADJACENT TO JERSEY CITY

No conflict is determined to exist between the Journal Square Redevelopment Plan and the Master Plans of adjacent municipalities.



SITE PLAN

All Redevelopers shall submit a Site Plan to the Planning Board identifying their specific project area by lot, block, and area within the overall Redevelopment Plan. The specific project submission for review and approval must identify bulk distribution, open spaces/parks and plazas, street improvements, building elevations, typical floor plans, typical building sections, site sections, and a program of uses. Utility location and capacity, detention, a landscape plan, a parking plan by type, a retail frontage plan, a sustainability plan, a streetscape plan, an illustrative site plan, a context plan of site plan, and elevation illustrating the proposed site plan in context with the surrounding existing and proposed buildings must be submitted to the Planning Board. In addition, two perspectives and a detailed three-dimensional massing model shall be submitted.

In addition, each developer shall provide a traffic impact analysis that must take in to account the alternative modes of transportation including, but not limited to, transit, walking, cycling, and the effects of intercept parking. Developers shall also provide a 30 year market analysis, fiscal impact analysis, and analysis of sewer and water infrastructure.

The Planning Board must be satisfied that the project meets the requirements and the goals and objectives of this Plan and the specific form based standards, providing for a satisfactory diversity of type, size and use, height of buildings, designated landscape features and other amenities and the projected timing of same.

40A:12A-15. IMPLEMENTATION OF REDEVELOPMENT PLAN

In accordance with the provisions of a Redevelopment Plan adopted pursuant to section 7 of P.L. 1992, c.79 (C.40A:12A-7), a municipality or Redevelopment Entity may exercise any of the powers permitted to be exercised in a redevelopment area, or a rehabilitation area, respectively, pursuant to the Local Redevelopment and Housing Law.

The power of condemnation for economic development purposes is not conferred upon a rehabilitation area under the Local Redevelopment and Housing Law. Nonetheless, the City of Jersey City and other government entities retain their condemnation powers with respect to traditional public uses, e.g. the construction of roads or schools.

REDEVELOPMENT AGREEMENT

No project shall be undertaken within the Area except pursuant to a redevelopment agreement approved by the Redevelopment Entity. The agreements will be undertaken on a project by project basis.

ACQUISITION PLAN

There will be no acquisitions for economic development in connection with the implementation of this Plan and therefore no acquisition plan has been prepared.

DEVELOPMENT REVIEW PROCESS

Preliminary Design Review

All projects in the area shall be submitted to the Planning staff and the Urban Design Review Committee for a preliminary design review prior to submission of an application to the Planning Board. Applicants are required to attend a workshop meeting with the Planning staff or its designated design review professionals. The meeting(s) will be used to determine if the proposed redevelopment project is consistent with the Redevelopment Plan and conforms to the core design concepts, building requirements, and architectural design guidelines of the Journal Square Redevelopment Plan and to provide an opportunity for comment and recommendations on the proposed project by the Redevelopment Entity and its professionals. Preliminary design review shall focus on how the proposed project relates to and coordinates other elements and phases of the Redevelopment Plan and how the streets, blocks, parks, plazas, building uses and typologies are integrated with and contribute to the quality and function of the layout and design as presented in the Redevelopment Plan. Prior to the approval of site plan for all or a portion of the redevelopment project by the Planning Board, the design review professionals shall provide recommendations to the Planning Board regarding the extent to which the proposed project is consistent with the Journal Square Redevelopment Plan and addresses the design standards contained in the Plan.



Planning Board Review

Pursuant to N.J.S.A. 40A12A-13, all development applications for development of sites governed by the Redevelopment Plan shall be submitted to the Planning Board of Jersey City for review and site plan approval. The following provisions shall govern review of any proposed redevelopment and rehabilitation projects for these sites:

1. Prior to the commencement of: (a) any new construction; (b) reconstruction; (c) rehabilitation; or (d) any change in the use of any structure or parcel, a site plan for such shall be submitted by the developer or property owner to the Planning Staff, the Urban Design Review Committee, and the Planning Board for review and site plan approval. No temporary or permanent Building Permit shall be issued for any work associated with (a) through (d) above, without site plan review and approval of such work by the Planning Board.
2. The Planning Board shall conduct site plan and subdivision review, if applicable, pursuant to N.J.S.A. 40:55d-1 et seq. and this Plan.
3. As part of the site plan approval, the Planning Board may require the Redeveloper to furnish performance guarantees for on-tract street improvements and appurtenant utility improvements pursuant to N.J.S.A. 40:55D-53. The performance guarantees shall be in favor of the City of Jersey City and the City Engineer shall determine the amount of any performance guarantees.
4. Any subdivision of lots or parcels of land within the Redevelopment Plan shall be in compliance with the Redevelopment Plan and reviewed by the Planning Board pursuant to the LRHL and N.J.S.A. 40:55D-1 et seq.
5. Once a property has been redeveloped in accordance with the Redevelopment Plan, it may not be converted to any use not expressly permitted in this Redevelopment Plan. A use or structure not conforming to the requirements of this Redevelopment Plan may not be reconstructed in the event of its destruction. The Planning Board shall determine the issue of whether the non-conforming use or building structure has been "destroyed."
6. No variances, deviations, or waivers may be granted by the Planning Board which will result in permitting a use prohibited or not expressly permitted within this Redevelopment Plan or which will result in a MFAR and height that exceeds the densities permitted in this plan.
7. The regulations and controls of this Redevelopment Plan shall be implemented, where applicable, by appropriate covenants, or other provisions, or through agreements for land disposition and conveyance between the redeveloper and municipality pursuant to N.J.S.A. 40A:12A-8 and 40A:12A-9.
8. Any and all definitions contained within the Redevelopment Plan shall control. In the absence of a definition, the definition found within the City's Land Development Ordinance may be used for guidance. Any and all definitions inconsistent with N.J.S.A. 40A:12-3 shall be invalid.
9. A redeveloper shall be required to pay all applicable escrow fees and other required charges (including District Improvement Bonus Fees) in accordance with applicable provisions of the City's Land Development Ordinance and New Jersey law. Additionally, a redeveloper may be required to pay for their proportional share of the cost of any studies, plans, reports, or analysis prepared by the City, the Agency or their respective professionals as part of the Redevelopment Plan review. Any such payments are required to reimburse the City or the Agency.



REVENUE ALLOCATION DISTRICT

Legislation is currently pending before the New Jersey State Legislature to amend the Revenue Allocation District Financing Act, N.J.S.A. 52:27D-459 et seq. (the "RAD Law") in order to allow the creation of a revenue allocation district in an area in need of rehabilitation, as defined by the Local Redevelopment and Housing Law. The City hereby designates the entirety of the Journal Square Rehabilitation Area as a revenue allocation district, such designation to take effect immediately upon the passage of the pending legislation described herein.



9.4 Schedule and Duration of the Plan

SCHEDULE FOR PERFORMANCE

Any site plan approved pursuant to this Redevelopment Plan shall contain a schedule for performance for the construction of the improvements. In addition to a schedule for commencement of improvements, such plans shall include a schedule that indicates the approximate time period for property acquisition, development approvals, construction permits, relocation, etc. required prior to construction as well as a projected date for project completion. The site plan shall also set forth the interim uses for the property and the duration of the period during which such interim uses will be in place.

DURATION OF THE PLAN

This Plan shall be in effect for a period of fifty (50) years from the date of approval of this Plan by City Council of the City of Jersey City.

PHASING

The Planning Board shall have the discretion to require a suitable mechanism to insure a balanced development of planned open space, commercial services and infrastructure to service the Area.

Phasing Plan:

Any applicant seeking to develop properties located within the Area shall be required to provide a Phasing Plan for review and approval by the Jersey City Planning Board, which shall establish the parameters under which public improvements which are the subject of the application for site plan approval shall be constructed in conjunction with permitted residential, retail and commercial development. "Public Improvement" is defined as any and all of the following: 1) the mandatory improvements identified in Landscape Section; 2) the mandatory improvements identified in the Streets Section; and 3) the mandatory improvements identified in the Mobility Section. The Plan shall set a schedule for completion of Public Improvements within the property owned or controlled by the applicant in which the percentage of completion of public improvements is approximately equal to or greater than the percentage of completion of all other development on property owned or controlled by the applicant.

Additional Requirements:

The Public open space improvements shall be constructed contemporaneously with any development of the surrounding Blocks.

CERTIFICATES OF COMPLETION AND COMPLIANCE

Upon the inspection and verification by the Redevelopment Entity that the redevelopment of a parcel has been completed, a Certificate of Completion and Compliance shall be issued to the redeveloper as such parcel shall be deemed no longer in need of redevelopment. At the discretion of the Redevelopment Entity, the redeveloper may submit a final Certificate of Occupancy for approval by the Redevelopment Entity to serve as the Certificate of Completion.

NON-DISCRIMINATION PROVISION

No covenant, lease, conveyance or other instrument shall be affected or executed whereby land within the Area is restricted upon the basis of race, creed, color, or national origin in the sale, lease, use or occupancy thereof. There shall be no restrictions of occupancy or use of any part of the Rehabilitation Area on the basis of race, creed, color or national origin.

AFFORDABLE HOUSING

Affordable housing and workforce housing shall be provided in accordance with the following guidelines:

(a) Ten percent (10%) of the residential units shall be affordable or workforce units, with no less than fifty percent (50%) of the affordable or workforce units to be provided on site;

(b) The remaining fifty percent (50%) of the affordable or workforce units required may be met by making a payment in lieu, which shall be set by the Agency on a per-unit basis at least once per year;

(c) Of the affordable or workforce units which are provided on site, the mix of low income, moderate income and workforce units, as well as the location, size, fixtures, number of bedrooms and other attributes shall be mutually agreeable to the City, the Agency and the Redeveloper;

(d) With respect to non-residential development, the redeveloper thereof shall make a payment in lieu equal to two and one half percent of the equalized assessed value of the land and improvements, for all new non-residential construction on an unimproved lot or lots; or a payment in lieu equal to two and one-half percent of the increase in equalized assessed value, of the additions to existing structures to be used for non-residential purposes, subject to the following exemptions:

1) All non-residential construction of buildings or structures on property used by churches, synagogues, mosques, and other houses of worship, and property used for educational purposes, which is tax-exempt pursuant to R.S.54:4-3.6, shall be exempt from the imposition of a non-residential development fee pursuant to this section, provided that the property continues to maintain its tax exempt status under that statute for a period of at least three years from the date of issuance of the certificate of occupancy;

2) Parking lots and parking structures, regardless of whether the parking lot or parking structure is constructed in conjunction with a non-residential development, such as an office building, or whether the parking lot is developed as an independent non-residential development;

3) Any non-residential development which is an amenity to be made available to the public, including, but not limited to, recreational facilities, community centers, and senior centers, which are developed in conjunction with or funded by a non-residential developer;

4) Non-residential construction resulting from a relocation of or an on-site improvement to a nonprofit hospital or a nursing home facility;

5) Projects that are located within a specifically delineated urban transit hub, as defined pursuant to section 2 of P.L.2007, c.346 (C.34:1B-208);

6) Projects that are located within an eligible municipality, as defined under section 2 of P.L.2007, c.346 (C.34:1B-208), when a majority of the project is located within a one-half mile radius of the midpoint of a platform area for a light rail system; and

7) Projects determined by the New Jersey Transit Corporation to be consistent with a transit village plan developed by a transit village designated by the Department of Transportation.



Standards: The regulations of the New Jersey Housing and Mortgage Finance Agency shall apply, but such regulations shall not apply to the number of affordable units required, to the mix of unit types, location, size, fixtures, number of bedrooms and other attributes of the on site affordable units, all of which shall be in accordance with the foregoing provisions.

Inventory: Pursuant to P.L.2008, c.46, any redevelopment plan is required to include an "...inventory of all housing units affordable to low and moderate income households,...that are to be removed as a result of implementation of the redevelopment plan,...[and]...a plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan." No acquisition of property for economic development purposes is anticipated pursuant to this Plan, therefore it is not anticipated that any affordable housing units shall be removed in connection with the implementation of this Plan.

PROJECT SIGNS AND ADVERTISING

In order to facilitate the overall rehabilitation of the Area, and to encourage further investment in the surrounding area and the City of Jersey City in general, all advertising, signage, renderings and other promotion of the development and redevelopment of the Study Area shall contain references to the proposed project's location in Jersey City, so as to promote the positive aspects of the project, the Journal Square Redevelopment Plan and the City of Jersey City.

Redevelop Journal Square

Appendices

Allee: a regularly spaced and aligned row of trees usually planted along the Pedestrian Priority street.

Apartment: a dwelling unit sharing a building and a lot with other dwellings and/or uses. Apartments may be for rent or for sale as condominiums.

Apartments, mid-rise and high-rise: Includes premises available for long-term human habitation by means of ownership or rental, but excluding periods of less than a month's duration; excludes all boarding houses and rooming houses. A building containing three or more dwelling units that share common horizontal and vertical separations.

Architectural Elements: The details and components that combine to create the architectural style of the building.

Area: shall mean the combination of the Journal Square Redevelopment Area, Newkirk Redevelopment Area, St. Johns Redevelopment Area, and the Journal Square Rehabilitation Area (see Exhibit 1)

Awning: An awning is a lightweight shading device, typically constructed of fabric or metal, mounted over an exterior door or window.

Balcony: A platform projecting from the wall of a building and surrounded by a balustrade or railing or parapet.

Bay: The distance between structural elements of a building that is reflected on the facade of a building. The division of a building façade of a building into distinctive vertical compositions. Typically bays are based on the structure of a building, but can also be created through design treatment of the façade.

Bicycle Lane (BL): a dedicated lane running within a moderate-speed vehicular thoroughfare, demarcated by striping.

Bicycle Route (BR): a thoroughfare suitable for the shared use of bicycles and automobiles moving at low speeds.

Billboard: off-premise advertising.

Block: the aggregate of private lots, passages, rear lanes and alleys, circumscribed by thoroughfares.

Block Area: the block area is measure from the inside edge of the sidewalk or lot line.

Block Face: the aggregate of all the building facades on one side of a block. The Block Face provides the context for establishing architectural harmony.

Brownfield: any former or current commercial or industrial site that is currently vacant or underutilized and on which there has been, or there is suspected to have been, a discharge of a contaminant (NJSA: 58: 10B-23.d)

Build-to-line: a line along which the primary facades of a building must be located. The build-to-line allows flexibility to the articulation of the facade allowing the facade to deviate in limited increments from this line.

Building Configuration: the form of a building, based on its massing, private frontage, and height.

Building Disposition: the placement of a building on its lot.

Building Frontage: building elevation that fronts on a public street where public access to the building is available.

Building Function: the uses accommodated by a building and its lot. Functions are categorized as Restricted, Limited, or Open, according to the intensity of the use.

Building Height: the vertical extent of a building measured in stories, not including a raised basement or a habitable attic. Height limits do not apply to masts, belfries, clock towers, chimney flutes, water tanks, elevator bulkheads, and similar structures. Building Height shall be measured from the average grade of the enfronting thoroughfare.

Building Type: a structure category determined by function, disposition on the lot, and configuration, including frontage and height.

By Right Permit: a proposal for a building or community plan that complies with this code and may thereby be processed administratively, without public hearing.

Cartway: the paved area of a street between the curbs, including travel lanes, medians, and parking areas but not including shoulders, curbs, sidewalks, or swales.

City: the City of Jersey City, NJ.

Civic: the term defining not-for-profit organizations dedicated to arts, culture, education, recreation, government, transit, and municipal parking.



Civic Building: a building designed specifically for a civic function. The particulars of their design shall be determined by Exception.

Civic Parking Reserve: parking structure or lot within a quarter-mile of the site that it serves. Space may be leased or bought from this Reserve to satisfy parking requirements.

Civic Space: an outdoor area dedicated for public use. Civic Space types are defined by the combination of certain physical constants including the relationship between their intended use, their size, their landscaping, and their enfronting buildings.

Clerestory: a windowed wall that rises above the roofed section of a building or portion of a wall.

Collector Street: an urban Collector Street system is designed to carry moderate volumes of traffic with an approximately equal emphasis on land access and traffic mobility. It should be the primary link between Minor Arterial Streets and Local Streets. These roads may, but do not necessarily, run through residential neighborhoods.

Commercial: the term collectively defining workspace, office, and retail functions.

Concrete Slab: a flat, reinforced-concrete structural member, relatively sizable in length and width, but shallow in depth; used for floors, roofs, and bridge decks.

Context: surroundings made up of the particular combination of elements that create specific habitat.

Corridor: a linear geographic area incorporating transportation and/or greenway trajectories.

Curb: the edge of the vehicular pavement detailed as a raised curb or flush to a swale. The Curb usually incorporates the drainage system.

Curtain Wall: an exterior building wall which carries no roof or floor loads and consists entirely or principally of metal or a combination of metal, glass, and other surfacing materials supported by a metal frame.

Density: The number of dwelling units within a standard measure of land area, usually given as units per acre.

Design Speed: the velocity at which a thoroughfare tends to be driven without the constraints of signage or

enforcement. There are four ranges of speed: Very Low (below 20 MPH); Low: (20-25 MPH); Moderate: (25-35 MPH); High (above 35 MPH). Lane width is determined by desired design speed.

Developable Area : the allowable building footprint of the ground floor of the building. It is within this designated area on the specific parcel that the building can be located.

Development: the division of a parcel of land into two or more parcels, the construction, reconstruction, conversion, structural alteration, relocation, or enlargement of any building or other structure, or of any excavation or landfill, and any use or change in the use of any building or other structure, or land or extension of use of land, for which permission may be required pursuant to the "Municipal Land Use Law," P.L. 1975, c.291 (C.40:55D-1 et seq.).

Driveway: a vehicular lane within a lot, usually leading to a garage.

Duplex: a unit within a multi-family building that has 2 or more floors stacked one above the other, accessed with a private internal stairway.

Elevation (drawing): the exterior walls of a building. An elevation drawing includes material, rendered window and door openings, height and facade details. See Facade.

Embedded Parking: Structured parking that is completely wrapped by a building so that the parking is not visible.

Encroachment: an area beyond the build-to-line that certain building elements can protrude. Typical encroachments may include overhangs, bow and bay windows, signing or other elements that commonly protrude over the main facade of a building.

Entrance, Principal: the main point of access of pedestrians into a building.

Façade: any vertical, exterior face or wall of a building.

Flat: a unit on one level in a multi-family building.

Floor Area: the sum of the gross area of all floors of a building or buildings, measured from the exterior face of exterior walls or from the centerlines of common walls separating two buildings.



Frontage Line: those lot lines that coincide with a public frontage. Facades along Frontage Lines define the public realm and are therefore more regulated than the elevations that coincide with the other Lot Lines.

GIS (Geographic Information System): a computerized program in widespread municipal use that organizes data on maps. Various municipal departments can input information including the location of wetlands, thoroughfares, water/sewer lines, boundaries, building footprints, schools, zoning, land-use, etc. GIS makes information available as layered databases. The protocol for preparing a Sector Plan should be based on GIS information.

Governmental Uses: Public institutions and uses, such as schools, community centers and government-owned or operated buildings, structures or land used for public purposes, not including houses of worship.

Green Roof: a green roof consists of vegetation and soil, or a growing medium, planted over a waterproofing membrane. Additional layers, such as a root barrier and drainage and irrigation systems may also be included.

Greenfield: a project planned for an undeveloped area outside the existing urban fabric.

Greenway: an open corridor in largely natural conditions which may include Trails for bicycles and pedestrians.

Greyfields: an area previously used primarily as a parking lot. Shopping centers and shopping malls are typical Greyfield sites.

Growth Sector: one of the three Sectors for New Communities or the Infill Sector, where development is permitted by right.

House of Worship: A building used for the assembly of members of a designated faith for religious instruction and worship of a deity such as a church, synagogue, mosque or temple.

Hotels: A building designed for occupancy as the more or less temporary place of abode for individuals who are lodged with or without meals, in which there are ten (10) or more guest rooms or suites and in which there may be kitchens in any individual room or suite.

Identifiable Building Widths (IBW): a subdivision of a linear facade to look like a series of adjoined buildings.

Independent Building: a building designed by a different architect from the adjacent buildings.

Infill: a project within existing urban fabric.

Inside Turning Radius: the curved edge of a thoroughfare at an intersection, measured at the inside edge of the vehicular tracking. The smaller the Turning Radius, the smaller the pedestrian crossing distance and the more slowly the vehicle is forced to make the turn.

Interim Uses: a temporary use before the final Redevelopment Plan implementation.

Lane Way: an easement along the mid-block property line of a development for pedestrian and limited vehicular access.

LEED (Leadership in Energy Efficient Design): green building standards developed by the U.S. Green Building Council (USGBC)

Liner Building: a building specifically designed to mask a parking lot or a parking garage from a frontage. A Liner Building, if less than 30 feet deep and two stories, shall be exempt from parking requirements.

Live-Work: A single, enclosed, private space of at least twelve hundred (1200) square feet, within a building that is designed to accommodate the two distinct functions of the living and working environment of residents who are creative in their professional work at home. This accommodation shall occur through the provision of appropriate ceiling heights, spatial arrangement, ventilation, sound attenuation and such other design considerations appropriate to a shared living and working environment; and the space/unit must also meet all appropriate codes for residential occupancy. Not more than one-half of the area of the total space may be dedicated to work space for the use of a person or persons engaged in a profession or other occupation in a business office setting. The remaining area within the space must be used for residential purposes and the person engaged in the profession or occupation must reside within the premises. Not more than two full time or full time equivalent employees not residing within the premises may be employed. The professions and occupations shall include:

1. Computer or information technology.
2. Computer graphics and computer aided design.
3. The graphic arts.



4. The offices of architects, planners, lawyers, accountants and other business professions.
5. Similar business occupations as determined by the Planning Board.

Local Streets: an urban local street system is designed to carry moderate volumes of traffic with the emphasis on land access and very low traffic mobility. It should be a link to other street systems and provide direct access to adjacent land uses. An alternative route is desirable for through traffic.

Lodging: premises available for daily and weekly renting of bedrooms. The area allocated for food service shall be calculated and provided with parking according to retail use.

Lot Line: the boundary that legally and geometrically demarcates a lot. Such lines appear graphically on Community and Site Plans. Codes reference lot lines as the baseline for measuring setbacks.

Lot Width: the length of the principal Frontage Line of a lot.

Manufacturing: premises available for the creation, assemblage and/or repair of artifacts, using table-mounted electrical machinery and including their retail sale.

Mandatory and Non-Mandatory Provisions: mandatory provisions are expressed by the words, including and limited to, shall, must, and required. Non-mandatory provisions are expressed by the words, including but not limited to, may, should, recommended and/or typical.

Minor Arterial: an urban Minor Arterial Street system is designed to carry high volumes of traffic with greater land access and less through traffic than a principal arterial system (such as Route 1). It should be the primary link between the principal arterials and collector roadways. Minor arterials should not intrude into residential neighborhoods.

Mixed-Use Building: a building with a variety of complementary and integrated uses, such as, but not limited to, residential, office, retail, public, or entertainment, in a compact urban form.

Mullion: a vertical strip dividing the pane of window.

Muntins: a horizontal strip dividing the pane of a window.

Neighborhood: a mostly residential area, often with a recognizable edge. For the purposes of this Plan, a “complete neighborhood” is further defined as consisting of one pedestrian shed (1/2 mile diameter) with a mixed-use center.

Office: a place for the transaction of business where reports are prepared, records are kept and services rendered, but where no retail sales are offered and where no manufacturing, assembly or fabricating takes place. General and Professional offices, including, by way of example, but not limited to, general offices including doctors, dentists, lawyers, accountants, architects, and financial institutions and government offices.

Open Space: any sidewalk, park, public plaza, water feature, courtyard, or similar area that is open and unobstructed from its lowest level to the sky.

Parks and Plazas: Open areas, parks, playgrounds, trails, paths and other recreational areas; scenic and historic sites. These sites will be open for the public but may be owned and maintained by either a public or private entities.

Parking, Garage: Buildings used exclusively for the parking or storing of motor vehicles. Parking includes stand alone structures with retail or office at the base. Embedded, under-building, in-building, podium, or any other type of parking primarily serving a primary use shall be designated an accessory use and shall not be included in the minimum number of uses required per building.

Parking Space: an area not to exceed nine (9) feet in width by eighteen (18) feet in depth (except handicapped spaces), either within a parking structure or a surface lot, for the parking of motor vehicles, exclusive of driveways, access drives, fire lanes and public rights-of-way. Parking spaces and lots must meet ADA requirements.

Parking Structure: a building containing two or more stories of parking. Parking Structure shall have Liner Buildings at the first story or higher, unless specified otherwise.

Parkway, Residential: the area between the curb and the sidewalk into which street trees are planted and which typically has a planted ground cover.

Parkway, Mixed-Use/Commercial: the area between the curb and building into which street trees, decorative lighting, and street furniture are located. The sidewalk extends from



the curb to the building. Typically, trees are planted in the first 4 to 6 feet. Planter grates are recommended around the bases of street trees.

Path (PT): a pedestrian way transversing a park or rural area, with landscaping matching the contiguous open space. Paths should connect directly with the urban sidewalk network.

Pedestrian Priority Street: A street type that allows buses, taxis, narrow gauge streetcar, pedestrians, and emergency vehicles, but has a specific emphasis on the pedestrian and pedestrian realm. It does not allow personal vehicles.

Pedestrian Realm: Areas primarily devoted to the congregation and flow of pedestrians. This area includes the sidewalk, the planting strip and street trees, lighting, and complementary street furniture.

Pedestrian Shed, Primary: an area defined as a five minute walk or approximately ¼ mile from a center or transit station.

Pedestrian Shed, Secondary: a distance defined as a 10 minute walk or approximately ½ mile from a center or transit station.

Plan: shall mean the Journal Square Plan.

Planter: the element of the public streetscape which accommodates street trees. Planters may be continuous or individual.

Podium Parking: A parking configuration where parking begins at the second or third story above a retail base with the primary building use above.

Porch: an open, elevated platform surrounded by a railing or low wall, immediately adjacent to an entry door with sufficient width to allow eating and sitting.

Portico: a smaller porch often leading to the entrance of a building with a roof supported by columns; extends no more than 2 feet from the building facade.

Principal Building: the main building on a lot, usually located toward the frontage.

Private Frontage: the privately held layer between the frontage line and the principal building façade. The structures and landscaping within the Private Frontage

may be held to specific standards. The variables of Private Frontage are the depth of the set back and the combination of architectural elements such as fences, stoops, porches, and galleries.

Property Line: edge of the right-of-way and/or edge of a lot on a block.

Public Frontage: the areas that are both used and seen by a person walking.

Public Realm: shall mean the areas that are both used and seen by a person walking.

Redeveloper: any person, firm, corporation or public body that shall enter into or propose to enter into a contract with a municipality or other redevelopment entity for the redevelopment or rehabilitation of an area in need of redevelopment, or an area in need of rehabilitation, or any part thereof, under the provisions of this act, or for any construction or other work forming part of a redevelopment or a rehabilitation project.

Redevelopment Entity: shall mean the Jersey City Redevelopment Agency.

Redevelopment Plan: the Journal Square Redevelopment Plan

Rehabilitation Area: shall mean the combination of the Journal Square Redevelopment Area, Newkirk Redevelopment Area, St. Johns Redevelopment Area, and the Journal Square Rehabilitation Area (see Exhibit 1)

Restaurant, Category One: A restaurant which is designed for and whose primary function and operation is the preparation and service by employees of meals to a customer or customers seated at the table at which the meal is consumed. A category one restaurant operates without substantial carry-out service; with no delivery service; with no drive-thru, drive-in, or service in vehicles; and without service at counters or bars unless the restaurant is licensed to serve alcoholic beverages.

Restaurant, Category Two: A restaurant whose primary function is the preparation and service by employees of food or drink to customers as part of an operation designed to include substantial carry-out service; delivery service; self-service, and which may also include on-premises consumption, except that no drive-in, drive-thru, or service in vehicles is permitted.



Retail: An establishment engaged in selling goods or merchandise to the general public for personal or household consumption and rendering services incidental to the sale of such goods. An establishment providing services, as opposed to products, to the general public for personal or household use.

Retail Frontage Line: Frontage Lines designated on a Community Plan that require the provision of a Shop front, causing the ground level to be available for retail use.

Right-of-Way: The area on, below, or above a public roadway, highway, street, cartway. Bicycle lane, and sidewalk in which a governmental unit has an interest, including other rights-of-way dedicated for travel purposes and utility easements of governmental units.

Semi-Public Edge: the yard area in front of a residential unit defined by a low fence and/or gate through which a person must pass in order to gain access to the front primary entrance. Semi-public spaces are also typically located in front of offices at street level.

Setback: the area of a lot measured from the lot line to a building façade or elevation. This area must be maintained clear of permanent structures with the exception of: galleries, fences, garden walls, arcades, porches, stoops, balconies, bay windows, terraces, and decks (that align with the first story level) which are permitted to encroach into the Setback.

Shared Parking Policy: an accounting for parking spaces that are available to more than one function. The requirement is reduced by a factor, shown as a calculation. The Shared Parking ratio varies according to multiple functions in close proximity which are unlikely to require the spaces at the same time.

Sidewalk: the paved layer of the public frontage dedicated exclusively to pedestrian activity.

Signature Building: These buildings must be the tallest and incorporate competition winning design.

Sky Exposure Plane: a virtual sloping line that begins at a specified height and rises inward over the zoning lot at a ratio of vertical distance to horizontal distance set forth in building class regulations. It is designed to provide light and air at street level and must not be penetrated by the building.

Stepback: the location where the building must be stepped back from the lower facade plane.

Story: a habitable level within a building of no more than 14 feet in height from finished floor to finished ceiling. Attics and raised basements are not considered stories for the purposes of determining building height. First floor retail may be granted a variance for a story height of up to 20 feet.

Stream Corridor: the zone within which a waterway flows.

Street (ST): a throughway that has emphasis on both vehicular and pedestrian movements.

Streetscape: the urban element that establishes the major part of the public realm. The streetscape is composed of streets (travel lanes for vehicles and bicycles, parking lanes for cars, and sidewalks or paths for pedestrians) as well as the visible private frontages (building facades and elevations, porches, yards, fences, awnings, etc.), and the amenities of the public frontages (street trees and plantings, benches, streetlights, etc.).

Streetwall: the elevations of buildings that when seen from the street or sidewalks form the space container.

Strip Windows: a series of windows that forms a horizontal band across the face of a building.

Substantial Modifications: alterations to a building that are valued to more than 50% of the replacement cost of the entire building, if new.

Suggestive Standards: sentences or phrases that include the following words: recommended, may, suggests

Thoroughfare: a vehicular way incorporating moving lanes and parking lanes within a right-of-way.

Tower: a building above 12 stories in a square or more rectangular shape with a central core for vertical circulation.

Transit Oriented Development (TOD): the development of housing, commercial space, services, and job opportunities in close proximity to public transportation. Reduces dependency on cars and time spent in traffic, which protects the environment and can ease traffic congestion, as well as increasing opportunity by linking residents to jobs and services.



Turnaround: a type of road junction at which traffic enters a one-way stream around a central island. Turnarounds allow traffic traveling in one direction on a road to efficiently reverse course and travel the opposite direction.

Utility: Water, sewage, telecommunication, gas or electric service from a private or public utility company or service provider.

Variance: an administrative technique granting relief from the provisions of a code.

Work/Live Artist Studio: a single, enclosed, private space of nine hundred (900) square feet or more, where at least one-half of the volume of the total space is devoted to work space for the creation, display and sale of art, and the remainder is used for living purposes. A minimum of one hundred fifty (150) square feet of living space per person occupying such work/living space shall be required. Nothing in this definition shall prohibit the use and occupancy of a "work/live artist studio" in a setting where shared kitchen and/or bath facilities are available, provided that applicable health and safety codes are met and maintained.

Workforce housing: household income equal to or less than 120% of regional median income



JOURNAL SQUARE REDEVELOPMENT PLAN

June 1989	
January 1988	
November 1987	
April 1987	
June 1999	
March 8, 2000	
August 21, 2001	
June 14, 2006	
March 14, 2007	Ord: 07-042
June 13, 2007	Ord: 07-098
August 22, 2007	Ord: 07-142

CITY OF JERSEY CITY
DIVISION OF CITY PLANNING



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INTRODUCTION

The standards and guidelines included within this redevelopment plan are intended to foster the redevelopment of Journal Square. This redevelopment plan is only one component in the City’s effort to further improve the Journal Square district and return it to a flourishing central business and shopping destination. The goals of this plan are coupled with the goals of the Special Improvement District members, including; owners, merchants, business persons and neighborhood groups, all of whom are working to positively contribute toward the continued improvement of Journal Square.

I. BOUNDARY DESCRIPTION

Property within the Boundary of this redevelopment plan area includes the following Blocks and Lots:

Block: 586.5 Lots: PL.A, PL.B, D21, C2, B, A2, A3
 1862 N26, N27
 1863 N24
 1864 36A, 38E, 38D, B2, 22
 1866 25H, 25J, C1, 20, 19, 18A, 17A, 16, B3, B4, 27C
 1867 1.B, 5

Concourse East (a.k.a. Block 581, Lot C5), and portions of the Right’s of Way of Journal Square, Sip Avenue and Kennedy Boulevard

A map of the boundary, entitled, BOUNDARY MAP, dated April 26, 2007 is attached. If there is any discrepancy between the properties listed above and the map, the map shall govern.

II. REDEVELOPMENT PLAN OBJECTIVES

Renewal activities for Journal Square (herein after referred to as the Project Area) will be undertaken in conformity with, and will be designed to meet, the following objectives of the Redevelopment Plan:

- A. There will be acquisition of private property in the Journal Square Redevelopment Area.
- B. The elimination of vacated, deteriorated and obsolete structures which affect the feasibility of amenable neighborhood physical change and the further development of an emerging commercial and residential area.
- C. The improvement of the functional and physical layout of the project area for contemplated new development and the removal of impediments to land disposition.
- D. The overall improvement of traffic circulation through the development of new and improved vehicular and pedestrian circulation systems which provide for separation of vehicular and pedestrian traffic and the maximum use of public transportation



- E. Coordination of redevelopment activities, reinforcing already existing adjacent renewal programs and in accordance with the Master Plan for the City overall.
- F. Preservation and adaptive reuse of any existing structures shall be encouraged. Said structure's pre-existing height, bulk and density shall be accepted in instances where the zoning regulations contained herein are exceeded.
- G. Encourage the conservation and preservation of selective structures, especially those with historical or architectural significance; and provide opportunity for adaptive reuse which will produce an economic life comparable to newly developed structures.
- H. Provide land in parcels of sufficient size and configuration so as to permit comprehensive, economically sound redevelopment projects.
- I. Provide for redevelopment with minimum relocation of residents and business concerns.
- J. Provide site improvements for the beautification of the project area and surrounding areas.
- K. To promote balanced development in accordance with the Fair Housing Act, (NJSA 52:27D-311), and the Housing Element and Fair Share Plan of the City of Jersey City Master Plan.
- L. To preserve, promote, and accentuate, view corridors created by public streets to accentuate views.
- M. Develop a network of public open space throughout the redevelopment area which shall be designed to improve pedestrian circulation, act as a catalyst for retail and commercial development, and help to preserve important vistas.
- N. Creation of major new employment and housing opportunities for the residents of Jersey City.
- O. Creation of a well planned project area which will provide opportunities for permanent employment, residential, college, recreational, commercial, and retail facilities within an area which is currently underdeveloped, contains substandard and obsolete structures, and has physical impediments.
- P. Promote sound development on a broad and comprehensive scale.
- Q. Encourage and provide for a variety of land uses which will generate and encourage community activity.
- R. Coordinate redevelopment activities to provide a uniform and consistent attack on blight within the physical structure of the City.



- S. Protect, recognize, and preserve historic properties by providing opportunities for design relationships consistent with the redevelopment plan.
- T. To promote the principles of “Smart Growth” and “Transit Village” development. i.e. sustainable economic and social development, including a variety of housing choices, providing pedestrian friendly streets and public rights-of-way, minimize automobile use by maximizing the appeal of mass transit, encourage reduced parking and shared use parking solutions, and creating a livable community with convenient access to commercial facilities.
- U. Maintain and improve pedestrian access to the Journal Square PATH Station.
- V. To encourage maximization of improved and landscaped roof top open space.

III. TYPES OF PROPOSED REDEVELOPMENT ACTIONS

A combination of redevelopment actions is proposed to substantially improve and up-grade the Journal Square area. These will include, but not be limited to:

- A. Acquisition and demolition of structures determined to be impediments to sound and comprehensive redevelopment.
- B. Retention and rehabilitation of sound compatible structures, if any.
- C. Assemble into redevelopment parcels the vacant and underutilized land now in scattered and varied ownership.
- D. Make provisions for a full range of public infrastructure necessary to service and support the new development.
- E. Rehabilitate dilapidated, deteriorated, obsolete or underutilized structures where necessary.
- F. Construct new structures and complimentary facilities.
- G. Provide for public infrastructure necessary to service and support the new development.
- H. Acquisition of air rights over the Conrail/PATH rail cut.

IV. DESIGN REQUIREMENTS

This redevelopment plan authorizes the Planning Board to identify, review and approve the site improvements and building including the façade design of all building in the redevelopment plan area. All applicants shall meet with the Planning Board Technical Review Subcommittee, present their design, and work with them to receive their endorsement prior to a hearing of the full Board.

A. Building and Structure Design

- 1. Building design shall be guided by the general characteristics of the best surrounding properties. All new structures shall incorporate brick as the main material. Building facades shall be constructed of masonry including



but not limited to stone, brick, textured concrete, etc. Materials such as EIFS and concrete block, and/or metal panels are prohibited. High rise structures may incorporate additional glass and other contemporary materials into the tower element provided that the façade of the tower elements also contain the more traditional masonry materials and architectural character of the surrounding area.

3. All structures within the project area shall be situated with proper consideration of their relationship to other buildings, both existing and proposed, in terms of light, air and usable open space, access to public rights of way and off-street parking, height and bulk.
4. Groups of related buildings shall be designed to present a harmonious appearance in terms of architectural style and exterior materials and shall be encouraged to incorporate historic elements found throughout the surrounding area.
5. Buildings shall be designed so as to have an attractive, finished appearance when viewed from all vantage points within and outside of the project area. Each façade shall be treated as being of equal importance in terms of material selection and architectural design. Material shall remain the same on all sides.
6. Buildings shall provide a top and a distinct base with a cornice line. The base which shall serve to finish the building with decorative features and materials to accent the base such as a water-table, to provide detail and interest to the pedestrian levels of the building. The top shall provide for a significant cap to the building and shall be consistent with the design, proportions, materials and character of the building.
7. Access by the elderly, physically handicapped and/or disabled shall meet barrier free design regulations as specified in the New Jersey and Federal ADA Standards Uniform Construction Code.
8. Chain link fencing, barbed wire and/ or razor wire shall not be permitted except during construction.
9. All utility distribution lines, including multi-media telecommunication lines, and utility service connections from such lines to the project area's individual use shall be located underground.
10. Urban design and streetscape elements shall be consistent throughout the Redevelopment Area. All projects shall continue the established design, color, materials and street furniture of the Journal Square Streetscape improvements.
11. Bergen Avenue, Kennedy Boulevard and Sip Avenue shall be maintained as important view corridor by the use of stepbacks, architectural design



elements, and building massing.

12. All structures within the project area shall be designed, and maintained so as to improve the visual impact of the Jersey City skyline as viewed from within and beyond the Cities borders.
13. All buildings shall have a base, which shall be designed according to the following:
 - a. The base façade shall be 15 to 40 feet in height. The height of a base located in District 5 may exceed 40 feet in order to accommodate a multi-level commercial / retail facility and structured parking, provided that the design of the base utilizes design feature within the base in the area between 15 and 40 feet in height; such as changes in materials, texture, ratio of voids to solid surfaces, building stepback or other similar architectural cues in order to better relate the scale of the base to the pedestrian environment and surrounding buildings.
 - b. The base façade shall be constructed of masonry, including but not limited to stone, brick, textured concrete, etc. Glass shall constitute 70 % of the first 2 stories of the base and at least 40 % of any stories above the second within the base façade area.
 - c. A visual cue or indicator such as a cornice, belt coursing, a significant change in the glass to solid ratio, or any other indicator consistent with the design, proportions, and materials, of the base shall be provided, at a minimum of 18 feet and a maximum of 75 feet from grade on the base.
 - d. All measurements specifying heights shall be taken from the average finished grade level for the perimeter of the building.
14. Stepbacks - All primarily non-residential buildings exceeding 150 feet shall be required to have one stepback or a combination of stepbacks to meet one of the following options:
 - a. Stepbacks on 4 or more sides with a minimum total of 30 feet per side
 - b. Stepbacks on two (2) sides with a minimum total of 40 feet per side, non-rectangular buildings are encouraged and may use the average of two (2) or more adjacent sides when calculating stepback requirements,
 - c. Stepbacks occurring within the top ten percent (10%) of any building exceeding 150 feet shall not count toward meeting the requirements listed above,
 - d. Buildings under 150 feet are not required to use stepbacks, but must include architectural articulation at or within 10 feet of all street facing cornices.

Stepbacks for primarily residential buildings within District 5 are specified within the Specific Land Use Provisions of District 5, Section IX of this Plan.



15. Roof treatment, Mechanical Screening and Electrical Equipment
 - a. All buildings exceeding 75 feet shall have a significant top designed to gracefully cap the structure. The Planning Board may permit the cap requirement to be replaced by the use of articulated elements that create the perception of a lower scale structure. The articulated elements such as material changes, material setbacks and use of cornices, are to be located no more than seven (7) stories below the highest point of the building. Lighting of the building top shall be required.
 - b. All mechanical equipment located on any roof of a building shall be screened from view from all vantage points with a material harmonious to that used in the façade of the structure. The screening shall not resemble a utility or rooftop elevator or stair tower. It shall instead resemble an upper level extension of the building and be designed to contribute to the building top design as is required in this plan.
 - c. Wherever roofs can be seen or looked down upon from adjacent buildings a roof scape plan must be developed and submitted for approval. Roof scapes should include mechanical equipment, trellises to obscure view, colored roof patterns and landscaping. Parking decks shall be designed to obscure the view of broad expanses of parking with the use of landscaping, screens, trellises and roofs.
 - d. All electrical communications equipment shall be located in such a way that it does not negatively impact the appearance of the building nor create objectionable views as seen from surrounding structures.
 - e. Transformers and primary and back-up generators shall be located interior to the building or within the pavement area of an adjacent street. Location within the sidewalk or outside between the sidewalk and the building is not permitted.
 - f. The burial of all new or reconstructed signal boxes is required.
 - g. The screening of all new or reconstructed telecom equipment is required.

16. Streetscape
 - a. All buildings shall be designed to front on a public street to encourage create a street wall and a pedestrian environment at a within human scale.
 - b. Main entrances into buildings shall be located on all public streets. Secondary entrances shall also be provided from parking areas and/or as necessary according to the design of the structure.
 - c. Entrances shall be designed to be attractive and functional. Indicators such as awning, changes in sidewalk paving material or other indicator consistent with the design, proportions, material and character of the surrounding area shall be provided.

17. Parking - All surface parking and parking structures shall meet the following requirements:
 - a. Any parking garage level equal to street level shall not contain any



- parking or mechanical floor area adjacent to the sidewalk/street frontage. Atrium, Lobby, and/or retail space shall occupy these areas.
- b. The parking structure shall be designed to eliminate headlight glare by the provision of opaque screening for head lights and placement of interior garage lighting to be directed into the structure and mounted on the interior side of columns. Light Fixture detail and location shall be included within the Garage floor plan at the time of site plan application.
 - d. The facade of the parking levels in the building shall be of a compatible material to that used throughout the development and shall be designed to provide visual interest.
 - e. The exterior material and design shall be the same or similar, to the greatest extent possible, as the exterior walls of the principal building. Where there is no principal building, the exterior materials and design shall be as attractive as a principal building.
 - f. Any openings in the garage facade must be of the punched type and relate in design to the fenestration pattern of the principal building. All openings must be screened with glass or decorative metal grills of small openings. Any openings shall preferably be in a vertical proportion. Perforated metal panels shall be used to screen the parking structures of District 5.
 - g. Exterior lighting of the screening materials on a parking structure façade is required in order to provide additional visual interest in terms of light and shadow and to further mask the interior lighting of the parking structure and headlight glare.
 - h. Open horizontal bands along the façade of any parking structure are prohibited.
 - g. Blank walls on any facade are prohibited.
 - i. All building and garage access points shall be provided at street level and designed to encourage street activity. Overhead or elevated pedestrian or vehicular connections are prohibited.

B. Improved Open Space

- 1. All new development shall provide improved publicly accessible open space on all parcels as required in the appropriate district.
- 2. Where possible, new structures surrounding or enclosing open space should be designed and sited to allow penetration of sunlight onto open space areas during peak user time (11:00 am to 2:00 PM).
- 3. Open space shall provide visual and functional elements such as benches, low walls, drinking fountains, refuse containers and planters, and public fountains. Open space amenities shall include decorative material such as: stone pavers, brick pavers, asphalt pavers, stamped and tinted concrete, and decorative lighting and detailing.
- 4. Adequate lighting shall be provided to encourage active usage and a sense



of security in the open space.

5. Open space shall be distributed so as to provide for maximum usability.
6. Through creative design, open space features shall address the need for human comfort and enjoyment and provide both active and passive leisure uses for secure and pleasant outdoor and indoor settings to meet public and private use requirements. Open space and plazas shall be designed at a human scale to invite and attract the public.
7. Open space shall be oriented to focus on areas to maximize views.
8. As a general guide, one (1) linear foot of seating for each linear foot of plaza perimeter shall be provided. Seating space may include planters, benches, fountains, etc.
9. Twenty-five (25%) of the total site involved in an application shall be required to be improved open space unless otherwise specified or exempted in a particular district. A minimum of 10% of this space shall be provided on the ground floor. This open space shall be divided into two (2) categories:
 - a. Totally Accessible Open Space – which shall be calculated at 100% of the actual area and be open to the public 24 hours per day shall include:
 - Sidewalks with trees
 - Landscaped medians
 - Plazas
 - Arcades, defined as partially covered
 - Public walkways
 - Storefront promenades or malls with ceiling heights at least 30' in height
 - Landscaped buffer areas greater than 10 feet in width
 - Student and public commons which include natural light, air, and landscaped areas capable of supporting natural plant material.
 - Elevated plazas and leisure space
 - Outdoor cafés at ground level and within elevated plaza space.
 - b. Limited Access Open Space – which shall be calculated at 50 % of the actual site area and opened to the public a minimum of twelve (12) hours per day shall include:
 - Student commons
 - Atriums or a large open public space areas enclosed primarily by glass
 - Enclosed commercial arcades or malls
 - Elevated plazas or roof top recreational opportunities

C. Landscaping and Lighting Guidelines

1. Landscaping shall be required for any part of any parcel not used for



buildings off-street parking, or loading space. The developer’s plan shall include proposals for landscaping indicating the location, size and quantity of the various species to be used.

2. Greenspace (trees, shrubs, flowers, etc.) shall be used as buffers and to accent entrances, arcades and sidewalks.
3. All plant material used must be able to withstand an urban environment. All screen planting shall be a minimum of 4 feet high and shall be planted, balled and burlapped as established by the American Association of Nurserymen. A planting schedule shall be provided by the developer and approved by the Planning Board. Ground cover shall be used in place of mulch.
4. Any landscaping which is not resistant to the environment or dies within 2 years of planting shall be replaced by the developer.
5. Underground watering facilities shall be encouraged for all landscaped areas. Hose bibs shall be provided immediately adjacent to planting areas abutting a building.
6. Trees and shrubs shall be planted along curblines of streets at a maximum of (35) thirty-five foot centers or groupings, in a regular pattern or spaced alternately on either side of streets, to further enhance the aesthetic quality of the redevelopment area. All deciduous trees shall be a minimum of four (4) inches in caliper.
7. Lighting within the site shall sufficiently illuminate all areas, including those areas where buildings are setback or offset to prevent dark corners.
8. All lighting sources must be adequately shielded to avoid any off-site glare. The area of illumination shall have a fairly uniform pattern of at least one-half (0.5) foot candles.
9. Lighting fixtures shall be in scale with the street and size of project. And shall incorporate the pole and fixture of the new decorative Journal Square street lighting fixture.

D. Rehabilitation of Existing Structures

1. Through the use of selective building materials, all buildings to be rehabilitated shall be encouraged to preserve the area’s prevailing character. All structures shall be rebuilt to their original profile.
2. Any upper level additions, i.e. additional floors, penthouses, etc. should maintain a strong relationship to the basic design elements as well as to texture, color and scale of materials of the existing building.



3. Existing structures to be rehabilitated shall be exempt from area, yard and bulk requirements except any additions to existing structures shall not cause the total building area to exceed the area, yard, bulk, height, coverage, and floor area ratio specified in the appropriate district. In the event that an existing building exceeds these parameters, that structure shall not be permitted any additional floor area.
4. All rehabilitation to historic buildings shall conform to the design standards and guidelines articulated in the Historic Preservation Ordinance of Jersey City.
5. Buildings shall be restored and rebuilt to their original profile.

E. Signage

1. Office

The total exterior sign area shall not exceed the equivalent of five (5) percent of the first story portion of the wall to which it is attached. In no case shall a sign on any structure exceed 50 square feet. One (1) use shall be permitted one sign. Buildings with multiple uses shall have not more than one (1) sign per use provided the aggregate area of all signs does not exceed the maximum permitted.

2. Residential

One (1) sign may be allowed, not to exceed 20 square feet. It shall be non-illuminated and identify the building name and address

3. Parking Garages and Lots

One (1) sign shall be provided per entrance to garages or lots indicating the parking facility by the international parking symbol and direction arrow. The sign area shall not exceed twenty (20) square feet. If applicable, one (1) sign per entrance may be allowed indicating parking rates, not to exceed eight (8) square feet.

4. Retail, Restaurants, Bars and Nightclubs

Each such use fronting on a public street may be permitted one (1) exterior sign, not to exceed 5 percent of the storefront (ground floor) to which it is attached.

5. Temporary Construction Signs

One sign shall be permitted for each project or development indicating the name of the project or development, general contractor, subcontractor, financing institution and public agency officials (where applicable). The



sign area shall not exceed 20 square feet and shall be attached (where there is an existing structure) or freestanding (where there is new construction).

6. Additional Sign Standards

- a. Additional signage regulations may be provided within Section IX Specific Land Use Provisions for a particular District within this Plan. If there is a conflict between any provision found in this subsection, and the specific regulations found in Section IX; the regulations found in Section IX shall prevail.
- b. Window signs are prohibited above the ground floor level and shall not exceed 20 percent of the window surface to which the display appears.
- c. Freestanding signs, except for those indicating direction, transportation, circulation and parking are prohibited.
- d. Internally illuminated signs shall be allowed and shall be subject to review and approval by the Planning Board.
- e. All signs are subject to site plan review.

V. TRAFFIC CIRCULATION REQUIREMENTS

- A. Parking and service access should be separated from the main traffic oriented streets. These access areas shall be clearly designated and designed so as to avoid the backing in and out of vehicles onto the street right-of-way.
- B. Shared use of accessory parking facilities shall be encouraged.
- C. The use of public transportation by employees, residents and visitors of the Redevelopment Area shall be strongly encouraged.
- D. All traffic impact studies shall incorporate, as part of the study, all projects approved or proposed in the immediate area. A listing of the projects may be obtained from the Division of City Planning.
- E. Traffic associated with office development shall be circulated in a manner as to not negatively impact the neighboring residential areas.
- F. The pedestrian circulation system shall be integrated with the roadway circulation network and shall encourage safe and improved pedestrian circulation through the following:
 - The focus of the streetscape improvements along primary pedestrian corridors;
 - Encourage design features, materials and activities at the street level which create an attractive and interesting pedestrian environment;
 - Insure the safety of pedestrians by providing adequate sidewalk space and clearly defined pedestrian crossings;
 - Direct new development to minimize pedestrian and traffic conflicts.
 - Provide for a conformable pleasant pedestrian friendly environment.



- Direct underground access from District 2 and District 5 to the Journal Square PATH Station is encouraged.
 - Overhead walkways connecting buildings and or parking above streets or Rights of ways are prohibited.
 - The current public pedestrian access easement that traverses the site and provides access between Sip Avenue and Concourse East shall be maintained at grade level. Its location may be shifted to the eastern edge of Lot 25H, parallel to the Hudson County College walkway. The existing Collage pedestrian walkway along the western side of Lot 27C on Block 1866 shall be maintained and widened a minimum of 5 Feet.
 - Improvement shall be provided to Concourse East with the Re-development of District 5 to insure a seamless connection between the projects and usable shared sidewalk.
- G. Sidewalk area shall be equivalent to the sidewalk width on either side of the property for in-fill projects. Where there is not an established sidewalk width, the Minimum shall be Fifteen (15) feet and shall be durably paved and smoothly surfaced and continue a pattern the same as and consistent with the implemented streetscape design of the Journal Square Special Improvement District.

VI. MAXIMUM OFF-STREET PARKING AND LOADING REQUIREMENTS

- A Residential uses shall provide up to a maximum of 1 off-street parking space per dwelling unit.
- B Office and other commercial uses shall provide up to a maximum of 0.9 space per 1000 square feet of gross floor area. This parking ratio can be averaged for the entire redevelopment area.
- C Retail, restaurants, bars, nightclubs and health clubs shall provide up to a maximum of .5 space per 1000 square feet of gross floor area.
- D Shoppers and Shop Employee Parking- Additional spaces in any garage may be set-aside for shoppers parking provided there is an approved validation procedure for shoppers and an identification card or similar procedure for employees. Parking garages providing shoppers and shop employee parking may utilize valet parking pursuant to site plan approval from the Planning Board.
- E Theaters shall provide up to a maximum of .5 space per 8 seats.
- F Hotels shall provide up to a maximum of 1 space per room up to 100 rooms; .5 space per room for 101 to 250 rooms; .3 space per room for 251 to 500 rooms.
- G Public/semi-public uses shall provide up to .9 space per 1000 square feet of gross floor area.
- H Colleges and Universities shall provide a maximum of 1 space per faculty and administrative member per 8 hour shift.



- I Stand alone commercial parking structures with ground floor retail, when permitted in their respective Zoning District of this plan, shall be permitted provided the Planning Board finds that there is an established need. These structures must conform to all design requirements, and area, yard, bulk, and height requirements of this ordinance and, as per the Permitted Use section, all commercial garage structures, whether attached or detached, shall incorporate retail along all street frontages of the ground floor unless otherwise specified in a particular district.
- J All required parking spaces must be a minimum of 8.5 feet wide by 18 feet deep. Compact parking spaces (8 x 15), may be provided, up to fifty percent of approved parking spaces.
- K Aisle widths shall conform to the following standards:
 - 90 degree parking 22' wide two-way aisle
 - 60 degree parking 18' wide one-way aisle
 - 45 degree parking 15' wide one-way aisle
 - 30 degree parking 12' wide one-way aisle

All one-way aisles shall be clearly designated.
- L Off-street parking and loading areas shall be coordinated with the public street system serving the project area in order to avoid conflicts with through traffic or obstruction of pedestrian walks and thoroughfares. Developers shall demonstrate that sufficient off-street parking and loading will be provided to meet the needs of the proposed use.
- M Parking lots for more than 10 vehicles, and all loading areas, shall provide a screen planting of dense evergreens not less than three (3) feet high along any street line and along all property lines except those instances where a building intervenes or where the proposed planting may interfere with sight triangles. Within the parking area, a minimum of three percent (3%) of the parking area shall be landscaped and maintained with shrubs no higher than three (3) feet and trees with branches no lower than six (6) so that the landscaping is dispersed throughout the parking area.
- N All peripheral planting areas shall provide decorative low fencing within the planting area to prohibit pedestrians from walking through the planting strips. Frequent pedestrian crossings with breaks in the fence shall be provided to accommodate convenient circulation needs. All outdoor, at grade parking lots shall contain perimeter landscaping and perimeter trees and, in addition, contain interior landscaping of at least one tree planting per 10 parking stalls.
- O All parking lots and garages shall provide pedestrian ingress and egress directly to all streets for which they have frontage.



- P Lighting used to illuminate off-street parking and loading areas shall be arranged and shielded to prevent the spillage of light off the premises and shall be in accordance with the lighting requirements of the zoning ordinance.
- Q All required parking areas shall be provided off-street. All such parking and loading areas shall be graded and paved with a durable dust free surface and adequately drained. All parking and loading areas shall be designed in accordance with the requirements of the Jersey City Zoning Ordinance.
- R Required parking may be provided on-site or within five hundred (500) feet of the property line of the development to which it will serve.
- S The number and design of off-street loading spaces shall be demonstrated by an applicant according to an anticipated need. All loading activities should be encouraged to be restricted to early morning and/or late evening hours. The design and number of off-street loading shall be subject to review and approval of the Planning Board.
- T All loading for newly constructed offices, schools, and mixed use facilities shall be internal to the building and provide sufficient space to secure complete enclosure of all trucks that will be servicing and visiting the building.
- U All developments which propose valet parking shall submit a parking management plan. Such plan shall include but not be limited to: number of vehicles to be parked, number of rows of cars to be stacked, all parking stall and aisle widths and any other information deemed necessary to effectively evaluate the management plan. All parking management plans shall be subject to review and approval of the Division of Traffic Engineering, the Division of City Planning and the Planning Board.

VII. INTERIM USES

Interim uses may be established, subject to agreements between the developers, the Jersey City Redevelopment Agency, and the Planning Board, that such use will not have an adverse effect upon existing or contemplated development during the interim use period, which may include surface parking provided there is no Kennedy Boulevard ingress or egress and also provided that an interim use of surface parking shall be limited to the State Theatre Site, referred to in this plan as District 6. Interim uses must be approved by the Planning Board which may establish an interim use period of between one (1) year and three (3) years in duration, subject to the Planning Board’s discretion. Additional renewals of an interim use may be granted by the Planning Board, subject to the same interim period limitations specified above.

VIII. GENERAL PROVISIONS

- A The regulations and controls in this section may be implemented where applicable by appropriate covenant, or other provisions, or agreement for land disposition



- and conveyance executed thereto.
- B There shall be no restriction of occupancy or use of any part of the project area on the basis of race, creed, color, age, gender, marital status or national origin. No lease conveyance or other instrument shall be executed by a developer or any of his successors or assignees, whereby land within the project area is restricted upon the basis of race, creed, color, age, gender, marital status or national origin in the sale, lease, use or occupancy thereof.
- C No building shall be constructed over public streets in the project area with the exception of freestanding structures ancillary to public plazas and/or pedestrian walkways, which shall be subject to review by the Planning Board.
- Given the unusual lot dimension of District 5, an applicant may propose to utilize portions of the Journal Square R-O-W below grade; this redevelopment plan shall not be interpreted as precluding this potential. This would be especially applicable to the utilization of below grade portions of Journal Square in order to provide additional area for off-street parking or commercial use levels below grade. However, the extent of the encroachment must be found to be acceptable by the Planning Board and City Staff in conjunction with a favorable site plan application. The applicant must request, receive a City Council franchise approval in order to proceed. The cost and construction, re-designing and maintenance of the R-O-W and streetscape to accommodate these encroachments shall be bore by the applicant and be reviewed by the Planning Board and completed in conjunction with and at the same time as the private project improvements.
- D. Prior to commencement of construction, site plans for the construction and/or rehabilitation of improvements to the project shall be submitted by the developer to the Planning Board of the City of Jersey City for review and approval, so that compliance of such plans with the redevelopment objectives can be determined.
- E. No use or reuse shall be permitted, which, when conducted under proper safeguards, will produce corrosive, toxic or noxious fume, glare, electromagnetic disturbances, radiation, smoke, cinders, odors, dust or waste, undue noise or vibration, or other objectionable features so as to be detrimental to the public health, safety or general welfare.
- F. All residential redevelopment proposals and construction plans shall meet or exceed applicable F.H.A. and/or H.F.A. minimum room size requirements prior to approval by the Planning Board.
- G. The provisions of this plan specifying the redevelopment of the project area and the requirements and restrictions with respect thereto shall be in effect for a period of forty (40) years from the date of approval of this plan by the City Council of the City of Jersey City, provided however that any development or redevelopment projects that are commenced and/or completed within said forty



(40) year period shall be deemed to comply with all applicable laws, so long as they comply with the provisions of this Redevelopment Plan. At the end of this forty (40) year period, the zoning regulations contained herein shall be incorporated into the zoning ordinance of the City of Jersey City in accordance with the appropriate State statutes.

- H. Site plan review shall be conducted by the Planning Board pursuant to NJSA 40:55D-1 et. seq. Site plan review shall consist of preliminary site plan application and final site plan application. Applications may be submitted for the entire project or in any number of phases.

No permanent Certificate of Occupancy of any type shall be issued for any construction until the Planning Board has conducted a final site plan compliance review for the phase in which such construction is located.

As part of any site plan approval, the Planning Board may require a developer to furnish performance guarantees pursuant to NJSA 40:55D-53. Such performance guarantees shall be in favor of the City in a form approved by either the Corporation Counsel or the Planning Board attorney. The amount of any such performance guarantees shall be determined by the City Engineer and shall be sufficient to assure completion of site improvements within one (1) year of final site plan approval.

- I. Any subdivision of lots and parcels of land within the Redevelopment Area shall be in accordance with the requirements of this plan and the land subdivision ordinance of Jersey City.
- J. Upon demolition of existing structures, the site shall be graded and planted or sodded, with a durable dust free surface in the interim period prior to construction of new buildings.
- K. **Deviation Requests**
The Planning Board may grant deviations from the regulations contained within this Redevelopment Plan, where, by reason of exceptional narrowness, shallowness or shape of a specific piece of property, or by reason of exceptional topographic conditions, pre-existing structures or physical features uniquely affecting a specific piece of property, the strict application of any area, yard, bulk or design objective or regulation adopted pursuant to this Redevelopment Plan, would result in peculiar and exceptional practical difficulties to, or exceptional and undue hardship upon, the developer of such property. The Planning Board may also grant such relief in an application relating to a specific piece of property where the purposes of this Redevelopment Plan would be advanced by a deviation from the strict requirements of this Plan and the benefits of the deviation would outweigh any detriments. No relief may be granted under the terms of this section unless such deviation or relief can be granted without substantial detriment to the public good and will not substantially impair the intent and purpose of the Redevelopment Plan.



IX. SPECIFIC LAND USE PROVISIONS

The Journal Square Redevelopment Area shall consist of the following districts:

DISTRICT 1	Block 586.5	Lot PL.A, PL.B, C2, D21
DISTRICT 2	Block 586.5	Lot B (partial, air rights), A2 (partial), A3
	Block 1863	Lot N24
DISTRICT 3	Block 1862	Lot N26, N27
DISTRICT 4	Block 586.5	Lot B (partial), A2 (partial)
DISTRICT 5	Block 1866	Lots 27C, 25H, 25J, C1, 20, 19, 18A, 17A, 16, B3, B4 and Portions of the R-O-W of Concourse East (a.k.a. Block 581, Lot C5), Portions of Journal Square and portions of Sip Avenue sharing frontage with the lots included within this district.
DISTRICT 6	Block 1864	Lots 36A, 38E, 38D, B2, 22, and a Portion of Kennedy Boulevard and Sip Avenue sharing frontage with these lots.
DISTRICT 7	Block 1867	Lots 1.B and 5

A. DISTRICT 1

- 1. Permitted Principal Uses**
 - a. Offices
 - b. Retail Sales
 - c. Restaurants, Bars, Night Clubs
 - d. Health Clubs
 - e. Public/Semi-Public Uses
 - f. Residential Uses
 - g. Appropriate mixed-use of the above

- 2. Accessory Uses**
 - a. Off-street parking and loading
 - b. Fences and low walls
 - c. Improved open space

- 3. Maximum Height**



All buildings in this district shall have a maximum height of 280 feet which shall include parking, mechanical floors and rooftop equipment. Antennas shall be exempt from height calculations.

4. Area, Yard and Bulk Requirements

a. Floor Area Ratio

The maximum floor area ratio for non-residential uses throughout this district shall not exceed 6.00 (exclusive of parking and areas used for mechanical equipment).

The maximum floor area ratio for residential uses shall not exceed 4.8 (exclusive of parking and areas used for mechanical equipment.)

b. Minimum Lot Size

Minimum lot size shall be 45,000 square feet. The overall area for District 1 shall be 177,690 square feet (4.08 acre).

c. Maximum Lot Coverage

Maximum lot coverage shall not exceed eighty percent (80%) for both buildings and parking structures (inclusive of on-grade parking). The remaining twenty percent (20%) shall be improved open space.

d. Setbacks

1. For all new construction, front yard setbacks shall be 0 feet from the property line.
2. All new construction shall be integrated with existing structures, (where applicable). In such cases, side yard setbacks may be permitted in accordance with contiguous structures and upon the review by the Planning Board.

e. Parking

There shall be no additional parking provided in District 1.

- 5. Design Requirements** – Except as they relate to the building façade materials set forth in Section IV, Design Requirements, Subsection A Building and Structure Design (1) all of the design requirements set forth in the Redevelopment Area shall apply to District 1. With regard to Block 586.5, Plot A, however, building facades shall be of a design and material consistent with those that currently exist on this parcel. Any additional development on the District 1 Block 386.5 Lot A will require screening and up-grades to the existing parking garage as may be required by the



Planning Board.

6. Driveway Access

The driveway access to any new buildings and the existing building on Block 386.5 shall be designed to function and look like a public street connecting into the John F. Kennedy Blvd. There shall be sidewalk on both sides of the street, parallel parking and only a minimal drop-off and turn around area.

7. Recreational Amenities shall be provided to adequately address the needs of the residents of this location. Recreation and exercise facilities shall be provided.

B. DISTRICT 2 Preservation and rehabilitation of the Loews Theater is required.

1. Permitted Principal Uses

- a. Offices
- b. Retail Sales
- c. Restaurants, Bars, Night Clubs
- d. Health Clubs
- e. Hotels
- f. Convention Center
- g. Cultural Facilities
- h. Theater/Studio
- I. Public/Semi-Public Uses
- j. Appropriate mixed-use of the above

2. Accessory Uses

- a. Off-street parking and loading
- b. Fences and low walls
- c. Improved open space
- d. Signs

3. Maximum Height

All buildings in this district shall have a maximum height of 280 feet which shall include parking, mechanical floors and rooftop equipment. Antennas shall be exempt from height calculations.

4. Area, Yard and Bulk Requirements

a. Floor Area Ratio

The floor area ratio in this district shall not exceed 6.5 (exclusive of parking and area used for mechanical equipment). If any lot in this district does not use the entire FAR permitted by the Plan, the FAR not used may be transferred to another lot within this district.



b. Minimum Lot Size

Minimum lot size in this district shall be 20,000 square feet.

c. Maximum Lot Coverage

Maximum lot coverage shall not exceed seventy-five percent (75%) for both buildings and parking structures. The remaining twenty-five percent (25%) shall be improved open space.

d. Setbacks

1. For all new construction, front yard setbacks shall be 10 feet from the property line.
2. All new construction shall be integrated with existing structures, (where applicable). In such cases, side yard setbacks may be permitted in accordance with contiguous structures and upon the review by the Planning Board.
3. Rear and side yard setbacks shall be determined by lot design and shall be subject to review and approval by the Planning Board.
4. Any new freestanding parking structure shall be a maximum of five (5) stories and not exceeding fifty-five (55) feet in total height.

f. Parking

All surface parking and parking structures shall meet the following requirements and the Design Requirements of this plan.

1. All parking structures (contained within a permitted use building or freestanding structure) shall be designed, setback and adequately screened so as not to be visible from Kennedy Boulevard, Pavonia Avenue or Magnolia Avenue.
2. Parking requirements can be found in the Parking/Loading section of this plan.

C. DISTRICT 3

1. Permitted principal Uses

- a. Residential
- b. Retail (restricted to the ground floor)
- c. Office (restricted to the ground floor)

2. Accessory Uses

- a. Off-street parking and loading
- b. Improved open space



- c. Fences and low walls

3. Maximum Height

All buildings in this district shall have a maximum height of four (4) stories not to exceed forty-five feet in total height which shall include mechanical floors and rooftop equipment.

4. Area, Yard and Bulk Requirements

a. Floor Area Ratio

The floor area ratio in this district shall not exceed 3.

b. Density

The maximum density shall be limited to 120 dwelling units per acre.

c. Minimum Lot Size

Minimum lot size shall be 5,000 square feet.

d. Maximum Lot Coverage

Maximum lot coverage shall not exceed seventy-five (75) percent for both buildings and parking structures (inclusive of on-grade parking). The remaining twenty-five percent (25%) shall be improved open space.

e. Setbacks

1. Buildings shall be setback a minimum of five (5) feet from the front property line.
2. Side yard setbacks may be provided in accordance with existing structure which surround the intended use.
3. Rear yard setbacks shall be a minimum of ten (10) feet from the rear property line.

f. Parking

1. Parking areas shall be located and adequately screened so as not to be visible from the street.
 - The ground level of all parking structures (contained within a permitted use building or freestanding structure) shall be designed, setback and adequately screened so as not to be visible from Sip Avenue, Bergen Avenue, Kennedy Boulevard, Pavonia Avenue or Magnolia Avenue.
 - All parking structures shall be masked along all street frontages or areas visible from a street, by usable floor are designed to contain permitted uses other than parking.



2. Parking requirements can be found in the Parking/Loading section of this plan.

D. DISTRICT 4

1. Permitted Principal Uses

District IV shall consist of the air rights above the Conrail/PATH rail cut. This area shall remain as passive open space.

The area of air rights may be utilized for a parking deck to serve the general retail, school and business district, provided the upper level is equal in height to the grade of the land to which it is adjacent and also provided that the roof top of the deck be improved for active recreational space. Design guideline for parking structures contained within this plan shall apply to t. pro to the grade office. All Design Requirements contained within this plan shall apply.

E. DISTRICT 5

1. Permitted Principal Uses

- a. Residential
- b. Offices
- c. Retail Sales
- d. Restaurants, Bars, Night Clubs
- e. Health Clubs
- f. Hotels
- g. Convention Center
- h. Cultural Facilities
- i. Theater/Studio
- j. Public/Semi-Public Uses
- k. Schools, Colleges, and Universities
- l. Appropriate mixed-use of the above
- m. Commercial parking structures provided that they are ringed by ground floor retail or a commercial function other than parking wherever the property meets the street, R-O-W, sidewalk, alley etc.

2. Accessory Uses

- a. All uses ancillary to and customarily associated with Schools and their service
- b. Off-street parking and loading
- c. Fences and low walls
- d. Landscape features
- e. Improved Open Space
- f. Signs, including Outdoor Advertising Signs when affixed to the base portion of the building as further described in paragraph 6. below.
- g. Roof top recreational opportunities, including but not limited to the



following uses:

- Amphitheaters
- Restaurants
- Video and film productions
- Passive recreation

3. Maximum Height

- a. Tower Elements – Not more than two towers may be constructed above the building base structure. The towers shall vary in height. The maximum permitted height for the lower of the tower elements within this district shall be 525 feet. The maximum permitted height for the taller of the tower elements within this district shall be 675 feet. Antennas and equipment associated with energy conservation / green building (LEED) construction techniques, such as solar arrays, shall be exempt from height calculations, but shall not exceed 25 feet above the otherwise permitted height.
- b. The maximum permitted height for the building base shall be no more than seven (7) stories above grade.
- c. The maximum height for the building located on Block 1866, Lot 27C shall be no more than four (4) stories.

4. Area, Yard, and Bulk Requirements - The building located on Block 1866, Lot 27C may not be expanded in anyway that would increase its exterior building envelope or height or decrease any of the existing setbacks. All other portions of District 5 shall comply with the following:

- a. **Minimum Lot Size**
Minimum lot size shall be 65,000 square feet. Except that the minimum lot size for any parcel used exclusively for a school, college or university use shall be 15,000 square feet.
- b. **Maximum Lot Coverage** – 100% for the building base, exclusive of required setbacks. Tower elements shall not cover more than 50% of the lot area.
- c. **Setbacks**
 - 1. The minimum required front setback along Journal Square shall be 0 feet for the base of the building.
 - 2. The minimum required setback for the base of the building along Concourse East and Sip Avenue shall be sufficient to provide the required 15 foot wide sidewalk area.
 - 3. A minimum setback of Five (5) feet shall be required for the development on Lot 25H in order to widen and enhance the required



pedestrian access way along this edge.

4. There shall be an additional required minimum setback from the façades of the base of the building for Tower elements above the base of the building. This minimum additional setback shall be an average of fifteen (15) feet from the exterior faces of the building base, with no setback being less than five (5) feet.
5. The minimum distance between tower elements shall be at least eighty (80) feet, with the average distance between the tower elements being at least one hundred (100) feet. The intent of this criteria is to allow for flexibility in the location of the towers on the base of the structure and encourage the location of the towers in an angular manner to each other in order to take advantage of scenic views, provide for greater light and air, and better organize the open space at the top of the base of the structure.

d. **Floor Area Ratio** – Maximum of Twenty-two (22)

e. **Open Space** – District 5 is located on and includes Journal Square, which is a large public pedestrian plaza. It is anticipated that Journal Square will be redesigned and improved as part of any major redevelopment within District 5. Therefore, the area constituting the Journal Square pedestrian plaza area shall be counted toward meeting the open space requirement found in Section IV.B. of this plan, provided that the redevelopment activities included in the approved site plan for the redevelopment within District 5 includes the reconstruction and improvement of Journal Square and the redeveloper undertakes and completes the approved improvements.

Any building constructed within District 5 shall also include an interior atrium within the base of the building, which shall include common areas open to the public during normal business hours.

Skylights may not be located within Journal Square over any commercial or other use areas which might extend under the pedestrian plaza areas. Additional public and/or private open space may also be provided within the building or on roof top areas.

5. Building Development and Design Standards

a. Building Base –

- The base of any building constructed with District 5 shall contain commercial uses, primarily active retail and restaurant uses.
- Commercial uses located at the grade level on the exterior of the base must be retail or restaurant uses and shall provide direct pedestrian access from each shop to the adjacent sidewalk areas along Journal



Square, Sip Avenue and Concourse East.

- Commercial uses may also be located below grade and within the upper levels of the building base. Commercial uses below grade may not have skylights which penetrate into the Journal Square pedestrian plaza. Where upper level commercial uses are provided, larger expansive glass areas are required in order to provide views into and out of the building.
- An interior 2 story atrium, (3 story if there is a basement commercial space proposed) shall also be provided which includes some access to commercial spaces immediately adjacent. The main atrium entrance shall be provided along the Journal Square Frontage. A secondary entrance shall be provided along the eastern access way. Additional entrances into the atrium space may be provided from Sip Avenue, Concourse East and the eastern façade of the building. The major entrance shall be grander in scale and provide a focal point when viewed from Kennedy Boulevard.
- Loading areas and loading circulation shall be provided below grade within the interior of the base.
- All parking levels must be completely covered from above.

b. Tower Element –

- In addition to any required setbacks or stepbacks, the facades of any tower elements shall be articulated in order to provide architectural interest. The intent of this required articulation is to create interesting and varied building facades such that the building façades do not read as uniform or continuous slabs.
- Changes in materials, forms and planes, appropriately detailed and well thought-out, shall be done in order to enhance the design and excitement of the building. Walls shall not be left blank.
- Façade designs shall include changes in angles, step-backs, bump-outs and other architectural techniques in order to create façade articulation.
- Signage is not permitted on all tower facades, or on the roof tops. All signage shall be submitted by the developer to the Planning Board of the City of Jersey City for review and approval, so that compliance of such signage plans with the intent of these regulations can be determined.

c. Parking / Loading Areas-

- All vehicular access and egress for parking and loading shall be provided from Sip Avenue only.
- Exterior parking & loading docks are prohibited.
- The universal P sign shall be provided to identify the entrances to the parking areas. All signage shall be submitted by the developer to the Planning Board of the City of Jersey City for review and approval, so that compliance of such signage plans with the intent of these regulations can be determined.



6. Additional Signage Regulations

- Outdoor advertising signs are permitted in District 5.
- Outdoor advertising signs may only be attached to the building base of a high rise structure and may not be attached to the high-rise portion of the building above the base or on any roof.
- Outdoor advertising signs may not be located on the eastern face of the building facing Lot 27C.
- At least fifty (50%) percent of all outdoor advertising signage located on any façade of the building shall be animated either electronically or mechanically in order to provide a sense of activity and vitality.
- Retail, restaurant and other commercial signage may also be animated either electronically or mechanically in order to enhance the sense of activity and vitality.
- Window signs shall be permitted for commercial uses located above the ground floor level within the building base.
- All signage must be displayed in a designed and organized manner and not present a haphazard or distasteful appearance.
- All signage shall be submitted by the developer to the Planning Board of the City of Jersey City for review and approval, so that compliance of such signage plans with the intent of these regulations can be determined.

F. DISTRICT 6

1. Permitted Principal Uses

- a. Offices
- b. Retail Sales
- c. Restaurants, Bars, Night Clubs
- d. Health Clubs
- e. Residential building provided ground floor retail is included along the frontage of Kennedy Blvd. and a minimum of 100 parking spaces primarily for Journal Square residences, employees, businesses, and shoppers are available within the District 6 building site.
- f. Appropriate mixed-use of the above
- g. A stand alone commercial parking garage provided that it is ringed by ground floor retail or a commercial function other than parking wherever the property meets the R-O-W, sidewalk of J.F. Kennedy Boulevard.

2. Accessory Uses

- a. Off-street structured parking and interior loading
- b. Fences and low walls
- c. Improved open space, on ground surface or on a roof or the top of a parking deck

3. Maximum Height



- a. The maximum height for Residential structures, as permitted within this district, shall be thirty-two (32) stories, and not more than three hundred and fifty (350) feet, inclusive of ground floor retail uses and any other non-residential floors, all mechanical equipment, penthouses, etc.
- b. Parking structures within Residential Buildings shall be a maximum of eight and one-half stories, measured from the ground and not more than ninety-five (95) feet.
- c. All other buildings in this district shall have a maximum height of 6 stories excluding, mechanical and elevator enclosures and rooftop equipment.

4. **Area, Yard and Bulk Requirements**

a. **Floor Area Ratio / Residential Density**

- The floor area ratio in this district shall not exceed 6 (inclusive of parking and area used for mechanicals). If any lot in this district does not use the entire FAR permitted by the Plan, the FAR not used may be transferred to another lot within this district.
- The above floor area ratio requirement shall not apply to residential buildings as outlined in item e. of the District 6, Permitted Principal Uses. The floor area ratio for residential buildings in this district shall not exceed 11 (exclusive of parking and areas as defined by Gross Floor Area within this plan).
- The maximum residential density in this district shall not exceed four hundred (400) units per acre.

b. **Maximum Lot Coverage - 100%**

Therefore, the Improved Open Space standards of Section IV.B. shall not apply.

c. **Setbacks**

For all new construction, front yard setbacks shall be 0 feet from the property line, except that residential portions of all structures shall be set back a minimum of:

- 80 feet from Sip Avenue;
- 0 feet from the western property line; and
- 10 feet from eastern property line

All new construction shall be integrated with existing structures. In such cases, side and rear yard setbacks may be permitted in accordance with contiguous structures and upon review by the Planning Board.

d. **Step backs**

- The step back requirements found in section IV. A. 13. of this Plan



shall not apply to residential structures constructed in District 6.

- The step back requirements found in section IV. A. 12 shall not apply to parking structures constructed in District 6.
- There shall be a minimum of one step back provided along the frontage of any building along Kennedy Boulevard. The step back shall be provided at a height between the 2nd and 5th level or story, and shall be a minimum of eight (8) feet in depth.

5. Access

The purpose of this requirement is to insure that Kennedy Boulevard is kept lively and with pedestrian activity and that service activities are limited to Sip Avenue.

- Pedestrian Access – All retail tenant spaces shall have principal access directly from the sidewalk on Kennedy Blvd. All parking facilities shall provide direct pedestrian access to Kennedy Boulevard and Sip Avenue.
- Vehicular Access – All driveway access for service purposes shall be located on Sip Avenue. Ingress and egress to any parking facilities shall be provided from Sip Avenue. In addition, a one- way, one- lane ingress only driveway curb cut is permitted from J. F. Kennedy Boulevard.

G. District 7

1. Permitted Principal Uses

- a. Offices
- b. Retail sales
- c. Restaurants Category one & two
- d. Hotel
- e. Residential uses
- f. Health Clubs
- g. Theaters
- h. Governmental uses
- i. Conference Centers
- j. Financial institutions w/out Drive thru facilities
- k. Colleges and Universities
- l. Appropriate mixes of the above

2. Area, Yard & Bulk Requirements

- a. Minimum Lot Size: 6,000 sq. ft.
- b. Maximum Building Height: 110 ft.
- c. Minimum Floor to Ceiling Height: 8 ft 8 in.
- d. Minimum Front Yard: 0
- e. Minimum Side Yard: 0



- f. Maximum building Coverage: One hundred percent (100%) with step-back and yard provisions of subsection g. below
- g. Minimum Rear Yard: 15 feet at the 3rd floor and above, 5 feet of which may contain projecting balconies
- h. Required Minimum Step-back: 5 ft. after the 8th floor or to mirror or compliment the Step-back of the adjacent Hotel

3. Parking

- a. Maximum of one space per dwelling Unit

4. Design Requirements

All of the Design requirements set forth in the redevelopment area shall apply to District 7. In addition, special consideration shall be made to insure that the building façade shall be of a design and materials that are consistent with and correspond with design cues of the adjacent Earl Hotel.

X. OTHER PROVISIONS NECESSARY TO MEET REQUIREMENTS OF STATE AND OTHER LOCAL LAWS

- A. The Redevelopment Agencies Law" NASA 40:55C-1 et. seq., specifically 40:55C-32 requires that a Redevelopment Plan shall:
 - 1. Conform to the general plan for the municipality as a whole; and
 - 2. Shall be sufficiently complete to indicate such land acquisition, demolition and removal of structures, redevelopment, improvement, conservation or rehabilitation as may be proposed to be carried out in the area of the project, zoning and planning changes, if any, land uses, maximum densities, building requirements, and the plan's relationship to definite local objectives respecting appropriate land uses, improved traffic, public transportation, public utilities, recreational and community facilities, and other public improvements".

- B. In accordance with the State requirements, the following statements are made:
 - 1. The proposals of this Plan conform with the general plan for the municipality:
 - 2. This plan provides an outline for the development of the Journal Square Area and sufficiently complete to indicate redevelopment improvements as proposed, planning changes, land uses, maximum densities, building requirements and its relationship to definite local objectives respecting appropriate land uses, improved traffic, public transportation, public utilities, community facilities and other public improvements.



3. The City of Jersey City, through the services of the Jersey City Redevelopment Agency relocation staff, will provide displaced businesses and persons with relocation assistance necessitated by State law. This office will be staffed by qualified personnel who will actively assist displaced businesses or persons in finding adequate accommodations. All businesses will be interviewed to determine their relocation requirements. The various elements of this Redevelopment Plan set forth above are in compliance with the requirements of State and local law and there are no additional requirements with respect to a Redevelopment Plan which have not been complied with.

XI. PROCEDURE FOR CHANGES IN APPROVED PLAN

This Redevelopment Plan may be amended from time to time upon compliance with the requirements of law. A fee of \$5,000.00 plus all costs for copying and transcripts shall be payable to the City of Jersey City for any request to amend this plan. If there is a designated developer, as provided for under NASA 40:55C-1 et. seq. said developer shall pay these costs. If there is no developer, the appropriate agency shall be responsible for any and all such costs.

XII. DEFINITIONS

ACCESSORY BUILDING, STRUCTURE, OR USE: A building structure or use which is customarily associated with and is subordinate and incidental to the principal building, structure or use and which is located on the same lot therewith. An accessory building attached to the principal building shall comply in all respects with the requirements applicable to the principal building.

BUILDING: Any structure, part of a structure, extension thereof, or addition thereto having a roof supported by columns, posts, piers, or walls and intended for the shelter, business, housing or enclosing of persons, animals or property.

BUILDING COVERAGE: The area of a lot that is covered with buildings. Building coverage expressed as a percentage shall mean the percentage of a lot or assemblage of lots occupied by one or more buildings. Maximum building coverage expressed as a percentage of the lot or assemblage of lots means that the lot or assemblage of lots may have that much building coverage provided the site design contains all other requirements of this Redevelopment Plan such as, but not limited to; parking, loading, buffers and circulation. If these other requirements of the ordinance are not met, the building coverage shall be reduced until all provisions of the Redevelopment Plan are satisfied.

BUILDING HEIGHT: The vertical distance measured to the highest point of the roof and measured from the mean elevation of the finished grade five (5) feet away from the foundation along the side(s) of the building facing a street, or the street line, whichever is closer to the foundation. On a corner lot, the height shall be measured from the average of the mean elevation along both streets calculated as outlined above. Subject to the height exceptions as found in the Jersey City Land Development Ordinance Section 345-60.F.



CONSTRUCTION: The act of: A) adding an addition to an existing building or structure; B) the erection of a new principal or accessory building or structure on a lot or property; C) alterations.

DENSITY: A number expressing dwelling unit(s) per gross acre.

DISTRICT: Three distinct zones within the Redevelopment Plan governed by standards and regulations contained within the Plan.

DWELLING: A building which is designed or used exclusively as the living quarters for one or more housekeeping units.

DWELLING UNIT: A room or series of connected rooms containing living, cooking, sleeping and sanitary facilities for one housekeeping unit. The dwelling unit shall be self contained and shall not require the use of outside stairs, passing through another dwelling unit, or another indirect unit to get to any portion of the dwelling unit, nor shall one dwelling unit require shared facilities with another dwelling unit.

FLOOR AREA RATIO: The ratio of gross floor area to the lot.

GARAGE PARKING: Buildings used exclusively for the parking or storing of motor vehicles and in which services limited to washing, polishing and other cleaning services may be provided.

GRADE:

1. For buildings adjoining* one street only, elevation of the established curb at the center of the wall adjoining the street.
2. For buildings adjoining more than one street, the average of the elevations of the established curbs at the center of all walls adjoining the streets.
3. For buildings having no wall adjoining the street, the average level of the ground adjacent to the exterior walls of the buildings.

*(All walls approximately parallel to and not more than 15 feet from the street line are to be considered as adjoining a street.)

GROSS FLOOR AREA: Gross floor area in residences shall be measured by using the outside dimension of the building, excluding the area of a garage, attic, open porch or patio and further excluding the area used as a cellar, basement, utility, heating and cooling rooms. Only those portions of floor areas in residential structures which are at or above grade and have a ceiling height above them of seven and one-half (7.5) feet or more, or those floor areas on the top story which meet the definition of a half story shall be included in the gross floor area.

In non-residential structure, floor areas used for storage and other purposes, regardless of the ceiling height, shall be included in the gross floor area, but floor areas used for parking, loading



docks, mechanical, utility, heating and cooling rooms shall be excluded. The gross floor area of any use sharing a common wall with another use shall be measured from the center of interior walls and the outside of exterior walls.

HISTORIC: A building, structure, object, site or landscape feature having a degree of significance or importance, over or at, a period of time.

HISTORIC DISTRICT: An area defined as a historic district by City Council, State or Federal authority and which may contain within definable geographic boundaries one or more landmarks or clusters, including their accessory buildings, fences and other appurtenance, and natural resources having historical, cultural, and archaeological significance, and which district may have within its boundaries other buildings or structures, that while not of such historical, cultural, architectural or archaeological significance as to be designated landmarks, nevertheless contribute to the overall visual characteristics of the landmark or landmarks located within the district.

HOTEL: A building designed for occupancy as a temporary place of abode for individuals and/or families who are lodged, in which:

- (a) there are ten (10) or more guest rooms or suites served by one or more common entrances; and
- (b) no provision is made for cooking in any individual room or suite; and
- (c) one or more of the following services are provided: maid, telephone, desk, or bellboy service, or the furnishing or laundering of linens; and
- (d) Restaurants, cocktail lounges, indoor swimming pools and health clubs may be provided.

LIGHT INDUSTRIAL: Uses to be permitted shall include; warehousing, wholesaling, shipping and receiving, manufacturing, processing, research, assembly, laboratory, testing, service, professional and governmental offices, public and quasi-public uses and other operations which do not include the production of petroleum into fuel, oil or other products or chemical processing and storage. Light Industrial uses shall not produce any adverse influences as outlined in Section VIII.E. provided however that existing activities not in violation of City, State or Federal law are not affected.

LOADING SPACE: An off-street space or berth on the same lot with a building or group of buildings for the temporary parking of a commercial vehicle while loading or unloading having fifteen (15) feet of vertical clearance, and lengths and widths that meet the zoning ordinance.

LOT: A tract or parcel of land which may abut a street, but not including any portion of a street, which tract or parcel of land is legally separate from any other tract or parcel of land.

LOT AREA: The area contained within the lot lines of a lot not including any portion of a street right-of-way.

LOT COVERAGE: The square footage or other area measurements by which all buildings and paved surfaces occupy a lot as measured on a horizontal plane around the periphery of the foundations and paved areas and including the area under the roof of any structure.



LOT FRONTAGE: The horizontal distance between side lot lines measured along the street line. The minimum lot frontage shall be the same as the lot width except where side lot lines are not parallel or where the lot fronts on a street with a curved alignment having an outside radius of less than five hundred (500) feet, in which case the minimum distance between the side lot lines measured at the street line shall not be less than seventy-five (75) percent of the required minimum lot width. In the case of a corner lot, any street frontage which meets the minimum frontage required for that zone may be considered the lot frontage.

LOT LINE: Any line forming a portion of the exterior boundary of a lot and the same line as the street line for the portion of a lot abutting a street. Lot lines extend vertically in both directions from ground level.

MIXED USE: A lot or structure containing more than one (1) zoning use.

MOTEL: A building or group of buildings which:

- (a) contains living or sleeping accommodations used primarily for transient occupancy; and
- (b) has individual entrances from outside the building to serve each such living or sleeping unit.

NIGHT CLUB: Any room, building or place in which any musical entertainment, singing, dancing or other similar amusement is permitted in connection with the restaurant business or business of directly or indirectly selling food or drink to the public.

NON-CONFORMING BUILDING, OR STRUCTURE: A building or structure which in its location upon a lot or in its size, does not conform to the regulations of this Redevelopment Plan for the district in which it is located.

NON-CONFORMING LOT: A lot of record which does not have the minimum width, frontage, depth, or contain the minimum area for the district in which it is located.

NON-CONFORMING USE: A use occupying a building, structure or lot which does not conform to the use regulations of the historic district in which it is located.

OFFICE: A place for the transaction of business where reports are prepared, records kept, and services rendered, but where no retail sales are offered and where no manufacturing, assembly or fabricating takes place.

OPEN SPACE: Part of a zoning lot including courts or yards which is unpaved and which a) is open and unobstructed from its lowest level to the sky, and b) conforms with lot coverage.

PARKING SPACE: An area measuring eight and one half (8.5) feet in width by eighteen (18) feet in depth, either within a structure or in the open, for the parking of motor vehicles, exclusive of driveways, access drives, fire lanes and public right-of-way.

PARKING SPACE (COMPACT): An area measuring a minimum of eight (8) feet in width by



fifteen (15) feet in depth, either within a structure or in the open, for the parking of motor vehicles, exclusive of driveways, access drives, fire lanes and public right-of-way.

PRINCIPAL USE: The main use of land or structures as distinguished from a subordinate or accessory use.

PROFESSIONAL OFFICE: The office of a licensed physician, psychiatrist, psychologist, chiropractor, lawyer, registered architect, licensed engineer, licensed planner, and licensed land surveyor.

REHABILITATION: The act or process of returning a building, object, site, structure or landscape feature to a state of utility through repair, remodeling, or alteration that makes possible an efficient contemporary use while preserving those portions or features of the building, object, site, or structure that are significant to its historical, architectural and cultural values.

RETAIL SALES: Department stores, variety stores, apparel and accessory sales, furniture and appliance stores, grocery stores, second hand and antique stores where all sales are under roof, cleaning establishments such as dry cleaning and Laundromats where an attendant is provided, but in no instance shall bars, service stations or drug rehabilitation centers be considered retail sales.

ROW HOUSE: One of an unbroken line of houses sharing one or more sidewalls with its neighbors, or one of a number of similarly constructed houses in a row; usually in a housing development.

SERVICE STATION: A place where motor fuel, lubricants and miscellaneous accessories for motor vehicles are sold and/or dispensed and where services are rendered for engine and mechanical repairs.

SETBACK LINE: A line drawn parallel to the street line or lot line and drawn through the point of a building nearest the street line or lot line. The term "required setback" means a line that is established a minimum horizontal distance from the street line and beyond which a building or part of a building is not permitted to extend toward the street line or lot line.

SIGN: Any announcement, declaration, demonstration, display, illustration or insignia used to advertise or promote the interest of any person or product when the same is placed to be seen by the general public.

SIGN, FREESTANDING: A sign which is attached to or a part of a completely self-supporting structure. The supporting structure shall be set firmly in or below the ground surface and shall not be attached to any building or any other structure whether portable or stationary.

SIGN, INTERNALLY ILLUMINATED: Any sign which has characters, letters, figures, designs or outline illuminated such that the light is directed into the eyes of the viewer from the light source.

SITE: The location of a significant event; a prehistoric or historic occupation or activity, or a building, structure, object, or landscape feature, whether standing, ruined or vanished, where the



location itself maintains historical, cultural, architectural, or archaeological value regardless of the value of any existing structure.

SITE PLAN REVIEW: The examination of the specific development plans for a lot. Whenever the term “site plan approval” is used in this Redevelopment Plan it shall be understood to mean a requirement that the site plan be approved by the Planning Board.

STORY: That portion of a building comprised between a floor and the floor or the roof next above it. A half story is a story at the top of a building, the height of which shall not be less than seven and one-half (7.5) feet above at least one-third the area of the floor when the room is used for sleeping, study or similar activity.

STREET: Any street, avenue, boulevard, road, land, parkway, viaduct, alley or other way which is an existing State, County, or Municipal roadway, or a street or way shown upon a plat heretofore approved pursuant to law or approved by official action pursuant to the Municipal Planning Act (R.S. 40:55-1.1 et. seq., as amended) or a street or way on a plat duly filed and recorded in the office of the County Recording Officer prior to the appointment of a planning board; and the grant to such board of the power to review plats, and any way shown on a plat approved by the City, and includes the land between the street lines, whether improved or unimproved, and may comprise pavement, shoulders, gutters, sidewalks, and parking areas within the street line.

STREET LINE: The edge of the street right-of-way forming the dividing line between the street and a lot.

STRUCTURE: An assembly of materials having a fixed location including but not limited to buildings, signs, fences, tanks, towers or poles, but excluding walks, walkways, driveways, streets, and roads.

TOWN HOUSE: One (1) dwelling unit in a line of three (3) or more connected dwelling units where each dwelling unit is compatibly designed in relation to all other units, but is distinct by such design features as width, setback, roof, design, color, exterior materials, and other features, singularly or in combination. Each dwelling unit may have one (1), two (2) or three (3) stories, but nothing in the definition shall be construed to allow either one (1) dwelling unit over the other or one dwelling unit behind the other.

UTILITY: Water, sewerage, telephone, gas, or electric service from a private or public utility company under the regulations of the New Jersey Public Utility Commission. On-site storage of gasoline, compressed gas, steam or use of conveyor belts, elevators, water mains, or other means of storing, servicing, or transporting goods and services on-site shall not be considered a utility.

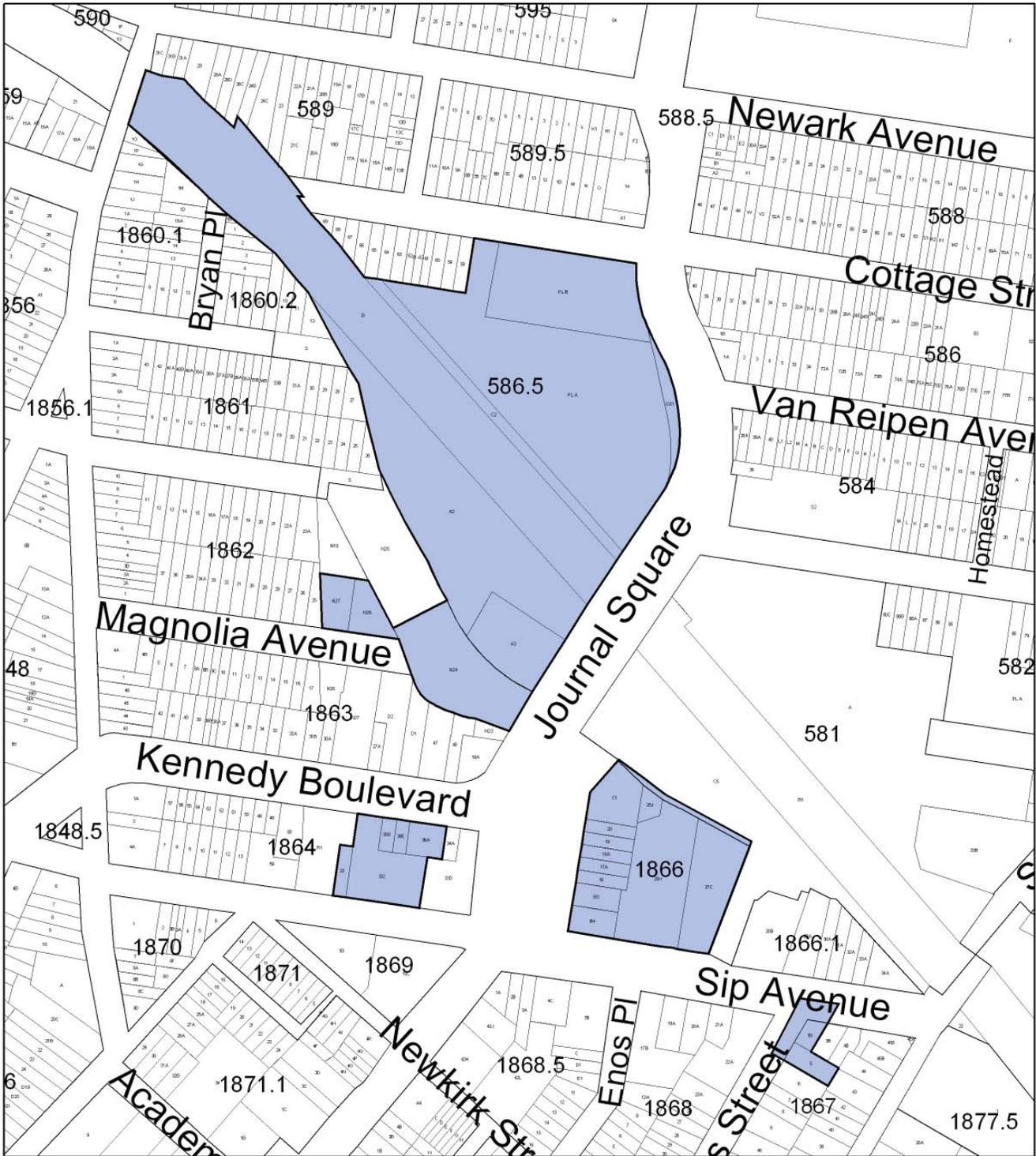
YARD, FRONT: An open space extending across the full width of the lot and lying between the street line and the closest point of any building on the lot. The depth of the front yard shall be measured horizontally and at right angles to either a straight street line or the point of tangent of curved street lines. The minimum required front yard shall be the same as the required setback.

YARD, REAR: An open space extending across the full width of the lot and lying between the



rear lot line and the closest point of the principal building on the lot. The depth of the rear yard shall be measured horizontally and at right angles to either a straight rear lot line or the point of tangent of curved rear lot lines. The minimum required rear yard shall be the same as the required setback.

YARD, SIDE: An open space extending from the front yard to the rear yard and lying between each side lot line and the closest point of the principal building on the lot. The width of the required side yard shall be measured horizontally and at right angles to either a straight side line or the point of tangent of curved side lot lines. The minimum required side yard shall be the same as the required setback.



Journal Square Redevelopment Plan Boundary Map

April 26, 2007



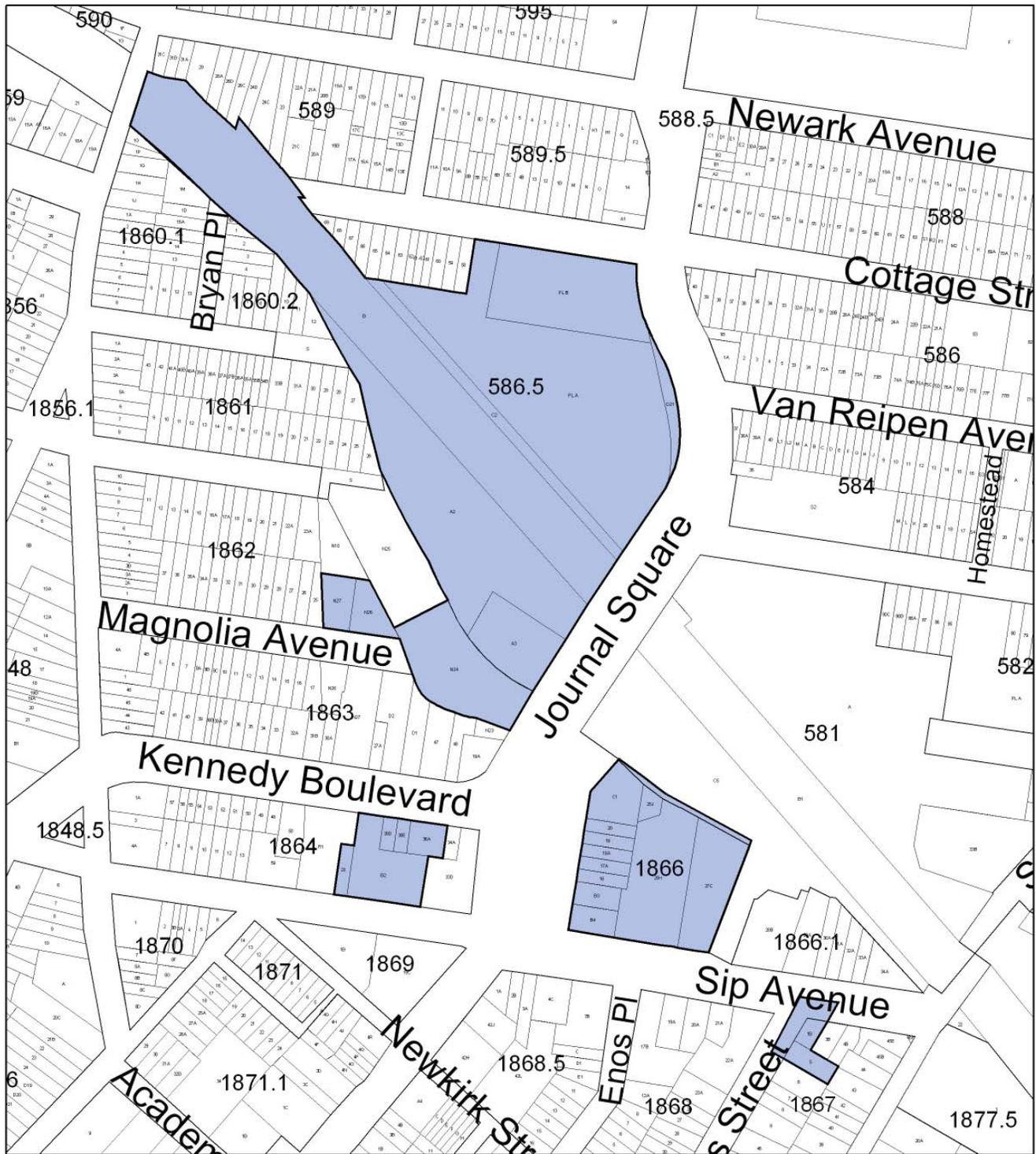
Legend

Redevelopment Plan Boundary



1 inch equals 250 feet





Journal Square Redevelopment Plan Acquisition Map

Legend

 To be Acquired

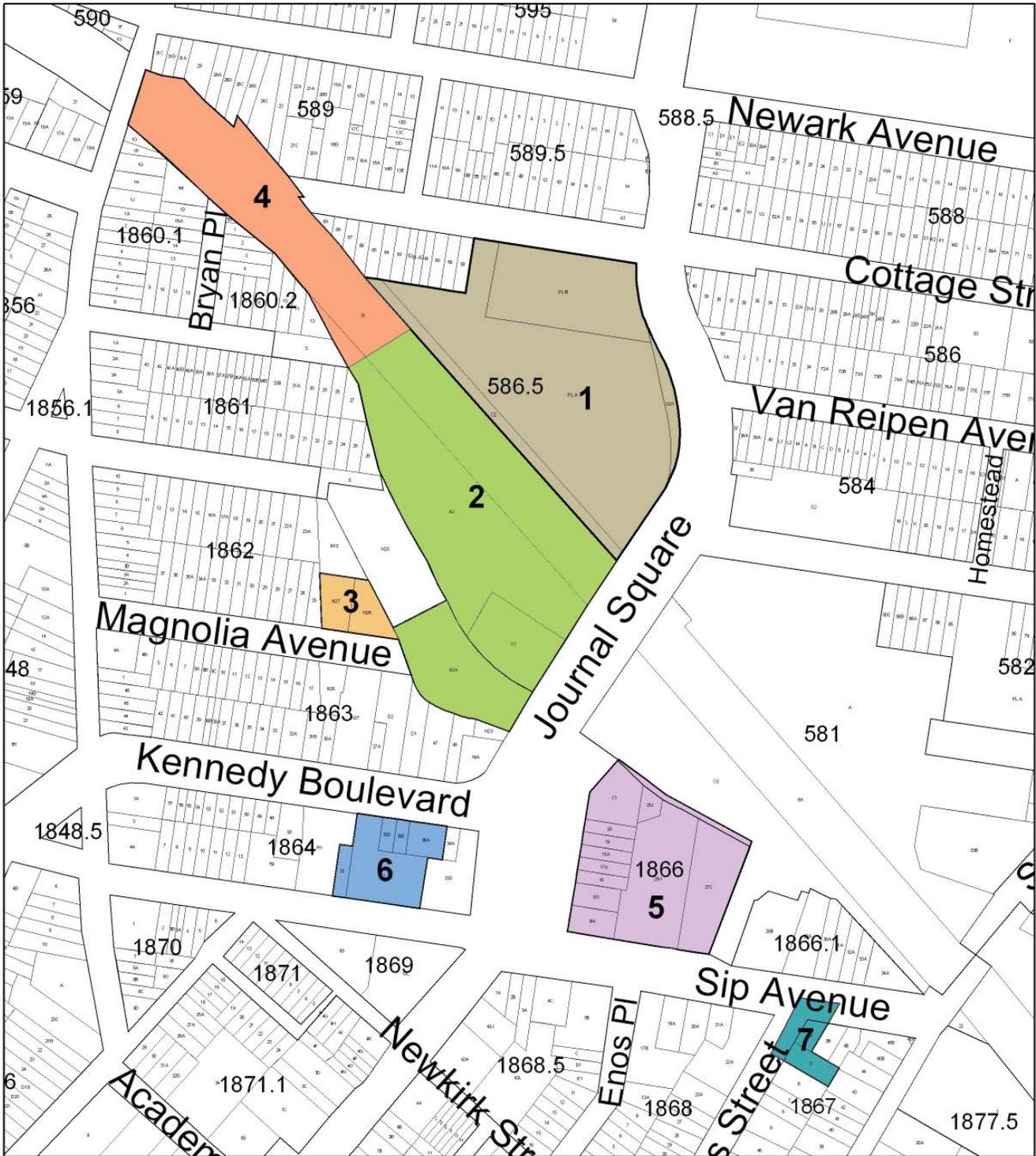


April 26, 2007



1 inch equals 250 feet





Journal Square Redevelopment Plan District Map

April 26, 2007



Legend

Districts	1	2	3	4	5	6	7

1 inch equals 250 feet



